

Chapter 7: Community Background

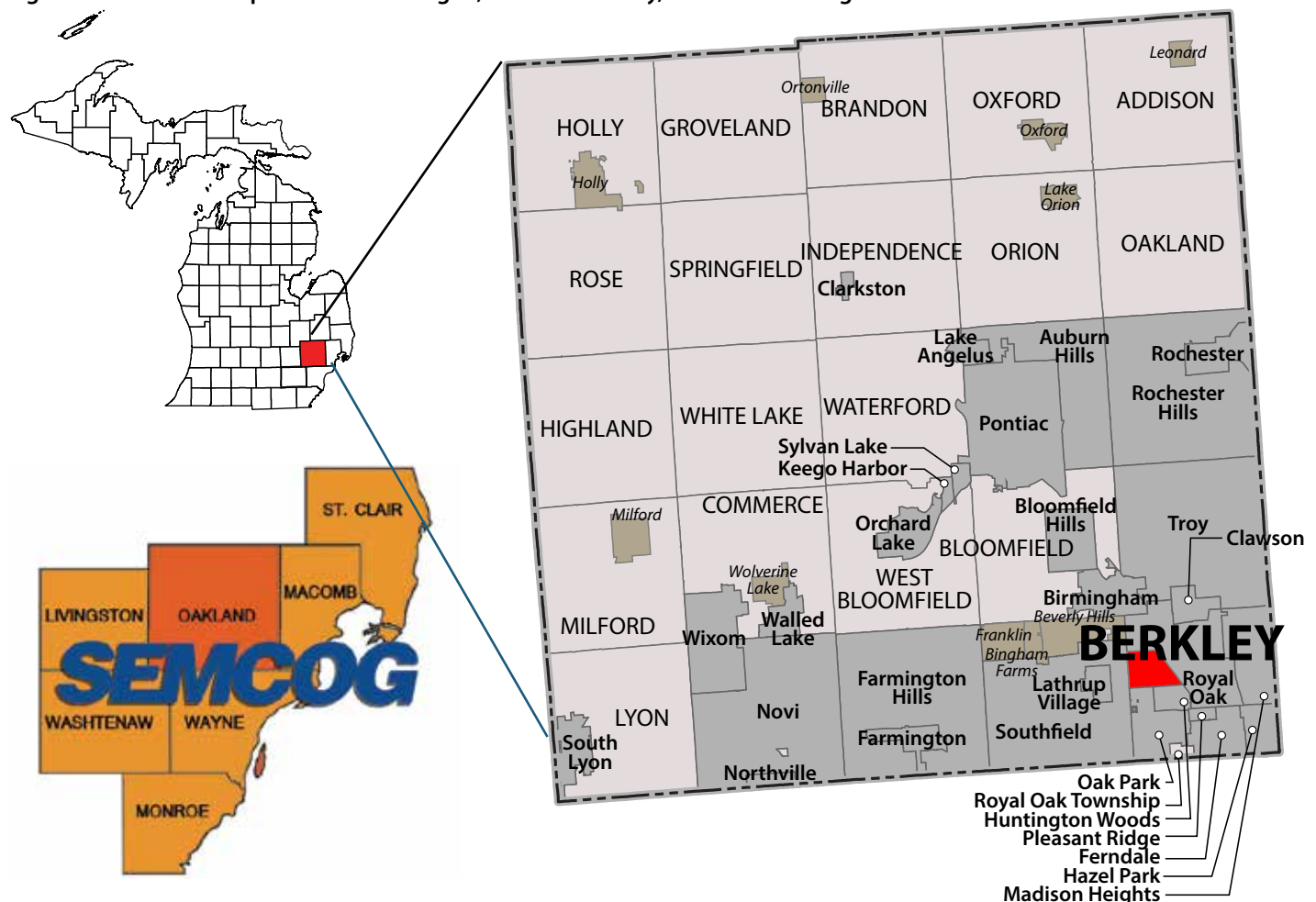
REGIONAL SETTING

This chapter provides an overview of the human and recreational resources of the City of Berkley with the goal of understanding the unique features and opportunities the community has to offer.

The City of Berkley is located in southeastern Oakland County, and is one of the original Detroit “inner-ring” suburbs. Most of Berkley we see today was developed after World War II and into the 1960s. Its eastern boundary is defined by the famous Woodward Avenue (M-1), home of the Dream Cruise and the world’s first concrete highway. Berkley’s southern boundary (Eleven Mile Road) is three miles from the limits of the City of Detroit. Greenfield Road borders Berkley to the west, and Webster Road to the north. The surrounding communities include Huntington Woods, Oak Park, Southfield, and Royal Oak.

Berkley is located in the center of southeast Michigan. Easy access to amenities available in the greater Detroit area including the arts, recreation, sports, and major transportation routes make the City a desirable home base. The community has the benefits of a City, with a small-town feel. Residents enjoy affordable and safe neighborhoods; walkable, tree-lined streets; and a high-quality school system that supports growing families. Residents also have a strong sense of community and shared pride in the City’s culture, politics and social opportunities.

Figure 10. Context Maps - State of Michigan, Oakland County, and SEMCOG Region



The population of the City of Berkley peaked in 1960 at 23,275, and declined over the years to a low of 14,592 in 2020. The Southeast Michigan Council of Governments (SEMCOG) population estimates suggests that for the first time since the 1960s, Berkley's population should begin to see slow but steady growth through 2045.

Historical population trends in the inner-ring suburbs were the result of expansive office, commercial, industrial, and residential development in the outer-ring suburbs. The more recent trend of "Millennials" moving back into urban cores is helping to reverse recent downturns, however, especially on a regional level. Factors such as the reduction in the number of people living together in each household, and the general aging of the region's population similarly will have an impact on the City's population.

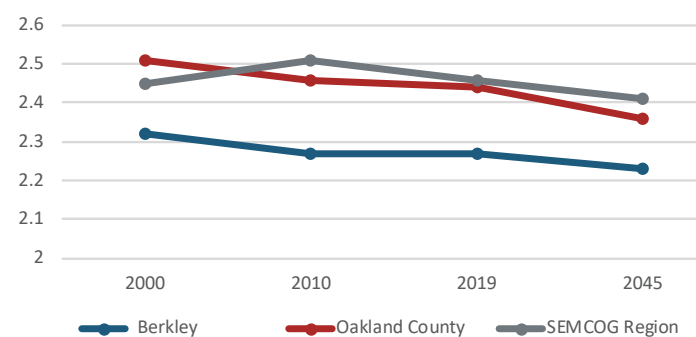
Like most of Michigan, household sizes have been decreasing. Berkley's average household size is significantly smaller than those seen across the region, and is projected to continue to fall through 2045.

Table 23. Household Size, Berkley & Region

	2000	2010	2019	2045
Berkley	2.32	2.27	2.27	2.23
Oakland County	2.51	2.46	2.44	2.36
SEMCOG Region	2.45	2.51	2.46	2.41

Source: US Census, SEMCOG

Figure 11. Household Size Trends



Source: US Census, SEMCOG

Table 25. Historical Population Data, Berkley Area

Community	1990	2000	2010	2020 (SEMCOG Projection)	Percent change 2010 - 2020
Berkley	16,960	15,531	14,970	14,592	-2.4%
Huntington Woods	6,419	6,151	6,238	6,247	0.1%
Oak Park	30,462	29,793	29,312	30,186	2.9%
Royal Oak	65,410	60,062	57,236	59,930	4.5%
Southfield	75,728	78,322	71,758	81,229	12.1%
Oakland County	1,083,592	1,194,156	1,202,362	1,261,941	5.0%
Southeast Michigan	4,590,468	4,833,368	4,704,809	4,768,427	1.3%

Source: US Census, SEMCOG

Table 22. Median Age, Berkley & Region

Year	Berkley	Oakland County	State of Michigan
2010	38.4	40.2	38.9
2011	38.4	40.6	39.2
2012	38.2	40.7	39.3
2013	37.9	40.8	39.5
2014	38.0	40.9	39.5
2015	37.7	41	39.6
2016	37.7	41	39.6
2017	37.3	41	39.7
2018	37.0	41	39.8

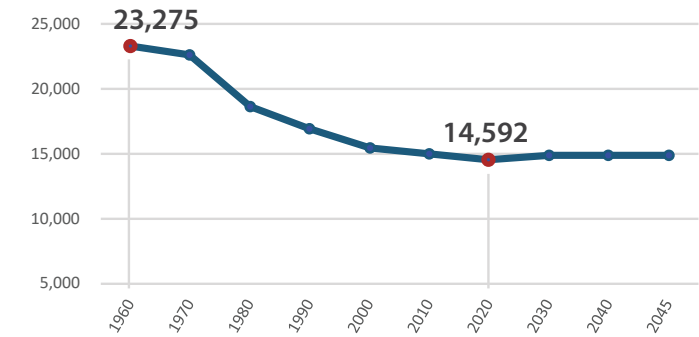
Source: American Community Survey, 2010 - 2018

Table 24. Population Projections, Berkley Area

Community	2020	2045	% change 2020-2045
Berkley	14,592	14,964	2.5%
Huntington Woods	6,247	6,247	0.0%
Oak Park	30,186	29,129	-3.5%
Royal Oak	59,930	61,612	2.8%
Southfield	81,229	83,816	3.2%
Oakland County	1,261,941	1,319,089	4.5%
Southeast Michigan	4,768,427	5,104,922	7.1%

Source: US Census, SEMCOG

Figure 12. Berkley Population Trends



Source: US Census, SEMCOG

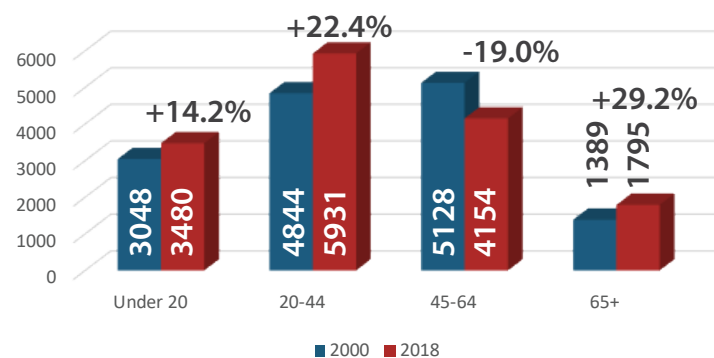
Berkley's demographics show a fairly well-balanced range of ages. Approximately two-thirds are between the ages of 20 and 64. 22.7% are under 20, while seniors 65 and older make up 11.7%. SEMCOG's 2045 population forecast shows the number of seniors increasing by nearly 72% between 2015 and 2045, while the midage-range population (24 to 64; -10%) and younger population (24 and under; -14.1%) are projected to decrease significantly.

Contradicting SEMCOG's forecast, however, is evidence that suggests Berkley is trending towards becoming a younger community. The median age for Berkley residents has been dropping since 2010. According to American Community Survey (ACS) 2018 estimates, the current median age for the City (37.0 years) is lower than that of Oakland County (41.0 years) and the State of Michigan (39.8) (Table 22 on page 66).

Table 27 shows where the changes in population are occurring. While there were significant percentage increases in the senior populations, especially in the 65 to 79 age ranges, in terms of actual numbers, those increases were offset by increases in both children and adults in young families. By combining groups into four general categories (children under twenty, young family, mature family, and seniors; see Figures 12 and 13), we see the largest increases in the young family (+1,087) and children under age 20 (+432) ranges. Seniors over 65 saw an increase of 406 individuals during this same time frame, while residents in the mature families category fell by 974. These results are somewhat surprising; the general trend across the state and the nation has been towards older and smaller households.

According to the 2018 American Community Survey, 1,398 residents or 9.1% of Berkley's population have some form of physical disability. While this is a decrease from the 10.6% rate reported in 2013, the numbers still underscore the need to accommodate individuals with disabilities when planning for parks and recreation facilities and programs.

Figure 13. Age Distribution Comparison 2000 - 2018



Source: US Census, ACS, SEMCOG

Table 26. Household Characteristics

Household Types	Census 2010	ACS 2018	% change	2045 SEMCOG
With seniors 65+	1354	1355	0.1%	2628
Without seniors	5240	5288	0.9%	4068
2 or more persons w/out children	2534	2610	3%	2650
Live alone 65+	696	644	-7.5%	1085
Live alone, under 65	1517	1602	5.6%	1221
With children	1847	1787	-3.2%	1740

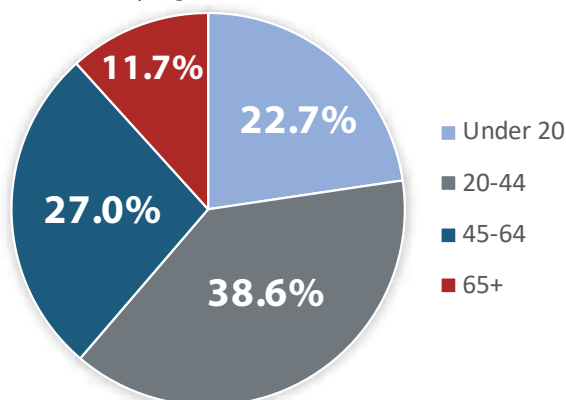
Source: US Census, ACS, SEMCOG

Table 27. Berkley Age Ranges 2000 - 2018

Age group	Census 2000	Census 2010	ACS 2018	% change 2000 - 2018
Under 5	938	988	964	2.8%
5 - 9	732	847	952	30.1%
10 - 14	703	833	828	17.8%
15 - 19	675	781	736	9.0%
20 - 24	635	661	619	-2.5%
25 - 29	1027	1232	1486	44.7%
30 - 34	1289	1473	1459	13.2%
35 - 39	932	1189	1467	57.4%
40 - 44	961	1174	900	-6.3%
45 - 49	1068	1131	962	-9.9%
50 - 54	1437	1159	919	-36.0%
55 - 59	1536	1053	1094	-28.8%
60 - 64	1087	754	1179	8.5%
65 - 69	496	452	886	78.6%
70 - 74	168	324	340	102.4%
75 - 79	84	286	181	115.5%
80 - 84	228	304	132	-42.1%
85 and up	413	329	256	-38.0%

Source: US Census, ACS

Figure 14. Berkley Age Distribution 2018



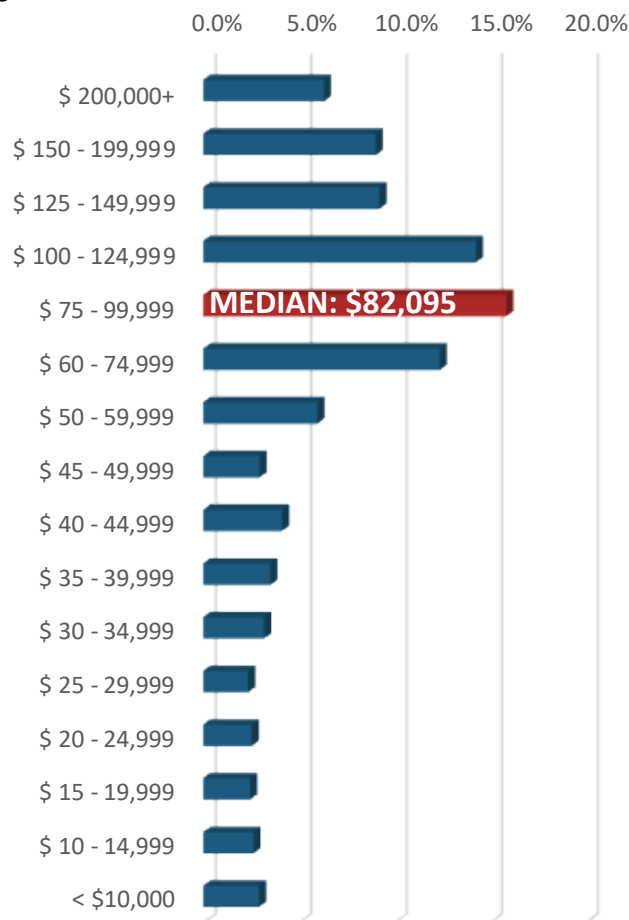
SOCIO-ECONOMIC CHARACTERISTICS

The City of Berkley's median household income rose by \$6,612 between 2010 and 2018, an 8.8% increase. According to ACS 2018 estimates, the median income for Berkley households stands at \$82,095. The number of households in poverty fell by 2.4% during the same period. This was a positive trend reflecting a rebound from the 2008 recession.

According to the SEMCOG 2045 Regional Development Forecast, the largest employment sector for Berkley residents is in the Information & Financial Activities group, followed by Healthcare Services and Leisure and Hospitality. Healthcare Service is expected to see continued growth through 2045, and will continue to remain the primary employer for Berkley residents. Per 2018 ACS estimates, 72.2% of Berkley were employed for 50 or more weeks in the previous year, a higher percentage than Oakland County at 62.9% and 58.2% for the State of Michigan.

86.1% of City workers are employed outside of the community; Detroit and Royal Oak are the most common commuter destinations. For positions inside the City borders, 25.7% of workers reside in Berkley.

Figure 15. Household Incomes 2018



Source: ACS 2018, SEMCOG

Table 30. Forecasted Jobs by Industry Sector

Sector	2015	2020	2045	% change 2020 - 45
Natural Resources, Mining, & Construction	381	438	430	12.9%
Manufacturing	176	168	126	-28.4%
Wholesale Trade	226	232	227	0.4%
Retail Trade	609	637	484	-20.5%
Transportation, Warehousing, & Utilities	117	123	128	9.4%
Information & Financial Activities	924	939	916	-0.9%
Professional and Technical Services & Corporate HQ	523	517	636	21.6%
Administrative, Support, & Waste Services	553	570	614	11.0%
Education Services	475	490	514	8.2%
Healthcare Services	810	883	1,176	45.2%
Leisure & Hospitality	785	809	865	10.2%
Other Services	569	586	548	-3.7%
Public Administration	472	477	474	0.4%

Source: SEMCOG

Table 28. Where Residents Work

Destination	Percent	Destination	Percent
Berkley	13.9%	Farmington Hills	4.9%
Detroit	10.6%	Birmingham	4.4%
Royal Oak	9.5%	Dearborn	3.4%
Troy	8.2%	Warren	2.8%
Southfield	7.6%	Auburn Hills	2.7%
		Elsewhere	31.9%

Source: SEMCOG

Table 29. Where Workers Commute From

Origin	Percent	Origin	Percent
Berkley	25.7%	Farmington Hills	3.4%
Royal Oak	9.2%	Warren	2.6%
Detroit	4.5%	Bloomfield Township	2.2%
Southfield	4.0%	Oak Park	2.0%
Troy	3.6%	Ferndale	2.0%
		Elsewhere	40.7%

Source: SEMCOG

HOUSING

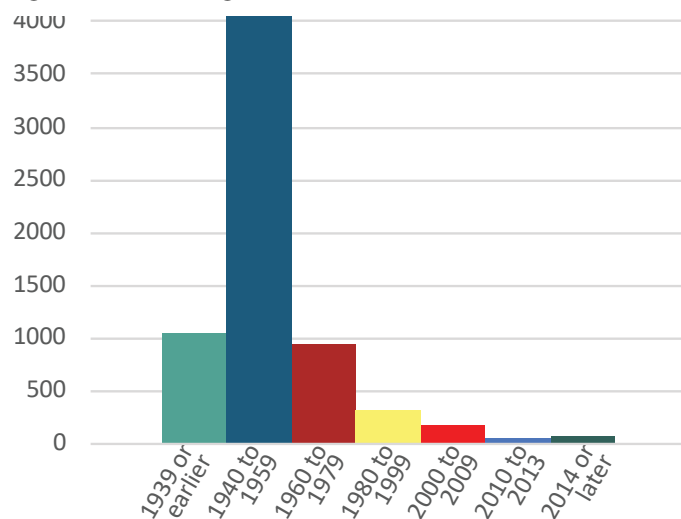
Housing availability may be a key driver for attracting younger families. Berkley's homes are largely post-WWII bungalows set on relatively small lots. 72% of homes were built between 1940 and 1959; only 5% were built after 2000. There has been a trend towards replacing the bungalows with larger homes in recent years but even so, relatively small lot sizes limit the ultimate size of the house which in turn helps keep the average home price at a more reasonable levels than those found in many nearby communities. Berkley's smaller houses tend to appeal more to young singles, couples without children, families with young children, and couples whose children are grown and have left home.

The majority of new structures built since 2000 (62%) are multi-family housing, while attached condominiums made up 21% of the new units. Even with this move towards multi-unit structures, however, single-family detached homes remain the predominant housing type (91.3%).

Despite the addition of new multi-family housing, the number of renter-occupied decreased slightly from 2010 to 2018 from 17.8% to 16.9%, while 5% of housing units were vacant in 2018. The vast majority of housing units (77.4%) in 2018 remain owner-occupied.

According to the American Community Survey, the median housing value of owner-occupied units in 2018 was \$191,100, an increase of \$25,100 (13.1%) from 2010, and significantly higher than values reported in 2015 (\$153,300). SEMCOG is reporting that the median gross rent decreased by 9% between 2010 and 2018, dropping from \$1,201 to \$1,093 per month.

Figure 18. Housing Structures - Year Built



Source: ACS, SEMCOG

Table 31. Housing Permits

Year	Single Family	Attach Condo	Multi Family	Total Units	Total Demos	Net Total
2000	5	0	0	5	3	2
2001	8	0	0	8	6	2
2002	2	0	0	2	0	2
2003	0	0	0	0	0	0
2004	2	3	0	5	3	2
2005	0	43	0	43	0	43
2006	3	16	0	19	4	15
2007	1	0	0	1	1	0
2008	0	0	0	0	0	0
2009	0	0	0	0	0	0
2010	0	0	0	0	0	0
2011	0	0	0	0	0	0
2012	3	0	0	3	1	2
2013	6	0	0	6	4	2
2014	1	0	0	1	3	-2
2015	3	0	79	82	0	82
2016	2	0	31	33	1	32
2017	2	0	52	54	1	53
2018	6	0	16	22	3	19
2019	2	0	0	2	3	-1
Totals	46	62	178	286	33	253

Source: SEMCOG

Figure 16. Housing Types 2018

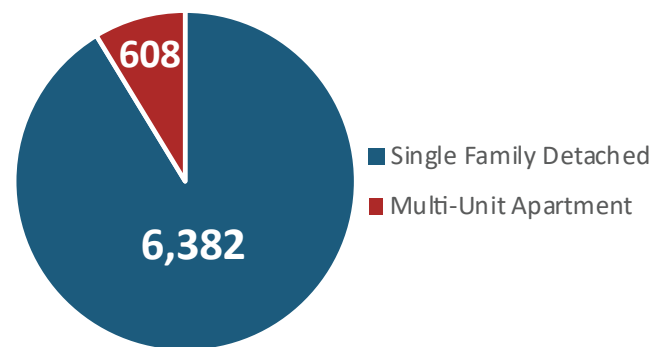
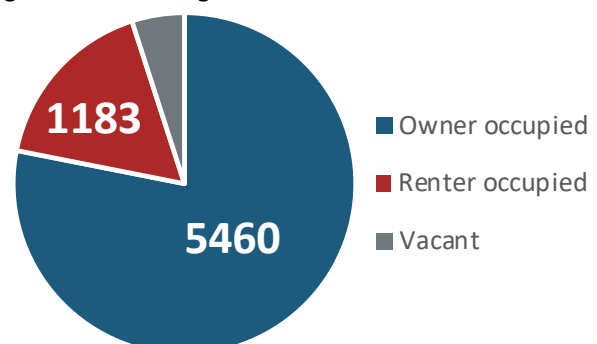


Figure 17. Housing Tenure 2018

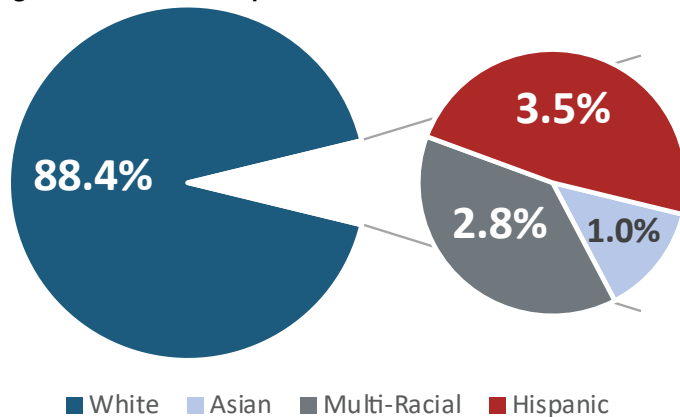


Source: ACS, SEMCOG

ETHNICITY

Racially, Berkley's population is largely homogeneous, although there has been an increase in diversity in recent years. Since 2010, minority populations have grown by 3.4% and now make up almost 12% of the total population. Hispanics made the largest gains, growing from 1.8% in 2010 to 3.5% in 2018. Multi-racial (1.1%) and African American (0.9%) populations also saw modest gains..

Figure 19. Racial Composition 2018



EDUCATION

Berkley residents are well educated, with 60% of residents reporting an Associates degree or higher in 2018. The City saw an 3.3% increase in residents with Bachelor's degrees and a 5.1% increase in graduate and professional degrees between 2010 and 2018. Education levels in the City are comparable with those seen across Oakland County, although Berkley has a higher percentage of residents with Bachelor's and graduate level degrees than the County as a whole.

Figure 20. Berkley Educational Levels, 2010 - 2018

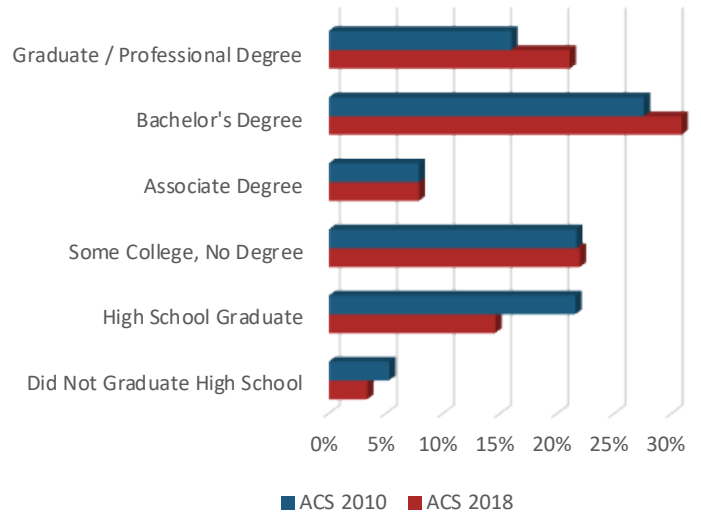
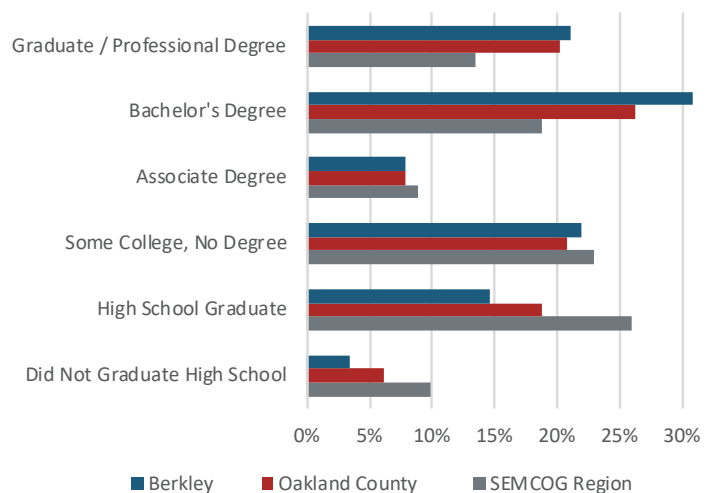


Figure 21. Comparison of Educational Levels - Berkley, Oakland County, and State of Michigan



LAND USE PATTERNS AND DEVELOPMENT TRENDS

Berkley, like most of the communities in southeast Oakland County, is essentially built out. According to Oakland County GIS, only a little over 11 acres remain undeveloped within the city borders. The City's predominant land use is single-family residential, making up over 73% of the total land area. Scattered throughout the residential areas are public uses (schools, community facilities), and recreation/conservation areas (parks). Office, commercial and industrial uses are concentrated along major corridors including Eleven Mile, Coolidge Highway, Twelve Mile, and Woodward Avenue. See the Existing Land Use map on the following page.

TRANSPORTATION

Bus service through Berkley is provided by Suburban Mobility Authority for Regional Transportation (SMART). A number of SMART bus lines travel Woodward Avenue (Berkley's eastern border), providing access between downtown Detroit, Pontiac, Birmingham, and the Chrysler Tech Center in Auburn Hills. The #415 bus line along Greenfield (Berkley's western border) services Greenfield, Fourteen Mile Road, and Southfield to the Oakland County Community College campus at Southfield. The #740 bus line services many shopping and employment centers between Roseville and Farmington Hills including Macomb, Tel-Twelve and Universal malls; Oakland County Community College campus at Farmington Hills; General Motors Technical Center and Macomb Community College south campus.

Berkley also offers van service for residents who are disabled or are 50 years of age or older. The service picks up anywhere within the city limits with transportation to Meijer, doctor appointments, shopping centers, and more. The service is funded and operated through the joint efforts of the City of Berkley and SMART.

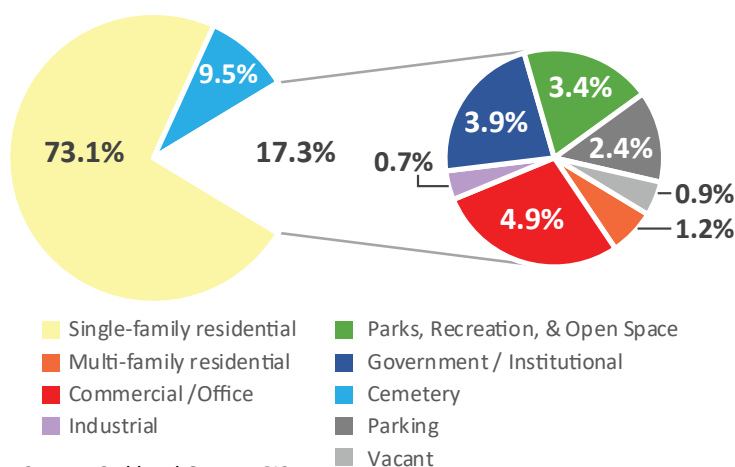
The City passed a resolution in support of "complete street" policies in 2010, and adopted a Multi-Modal Transportation Plan in May 2012. Berkley has made concerted efforts to make the city more pedestrian and biker friendly, and continues to pursue opportunities to improve the street level experience for non-motorized users.

Table 32. Existing Land Use 2019

Land Use	Acres	Percent
Single family residential	938.7	73.1%
Multi family residential	15.4	1.2%
Commercial / Office	62.7	4.9%
Industrial	9.6	0.7%
Government / Institutional	50	3.9%
Parks & Recreation	43.3	3.4%
Cemetery	122.4	9.5%
Parking	30.2	2.4%
Vacant	11.3	0.9%
TOTAL AREA	1283.6	

Source: Oakland County GIS

Figure 22. Existing Land Use 2019



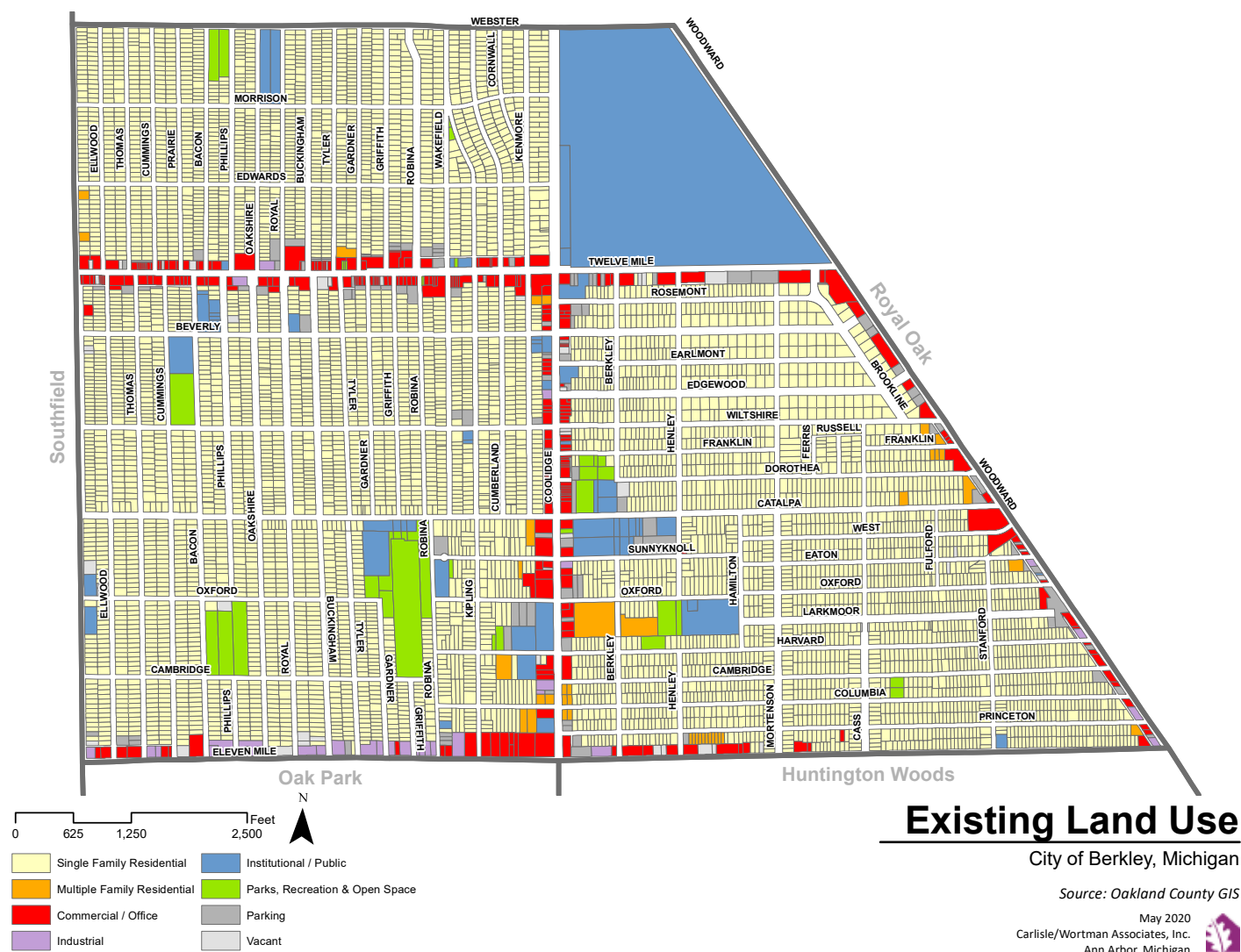
Source: Oakland County GIS

Table 33. Land Use Percentage Comparison 2007 - 2019

	2007	2013	2019
Single Family	42.9%	42.8%	35.0%
Multi-Family	3.5%	5.9%	7.1%
Commercial	1.5%	1.5%	1.1%
Public / Institutional	0.9%	0.9%	4.0%
Recreation/Conservation	5.6%	5.6%	15.0%
Transportation / Communication / Utility	-	-	0.2%
Road Right-of-Way	13.3%	13.3%	13.1%
Vacant	5.5%	3.1%	2.8%
Water	26.8%	26.9%	21.8%

Source: Oakland County GIS, Berkley 2014 Recreation Plan

Map 6. Existing Land Use 2019



NATURAL FEATURES

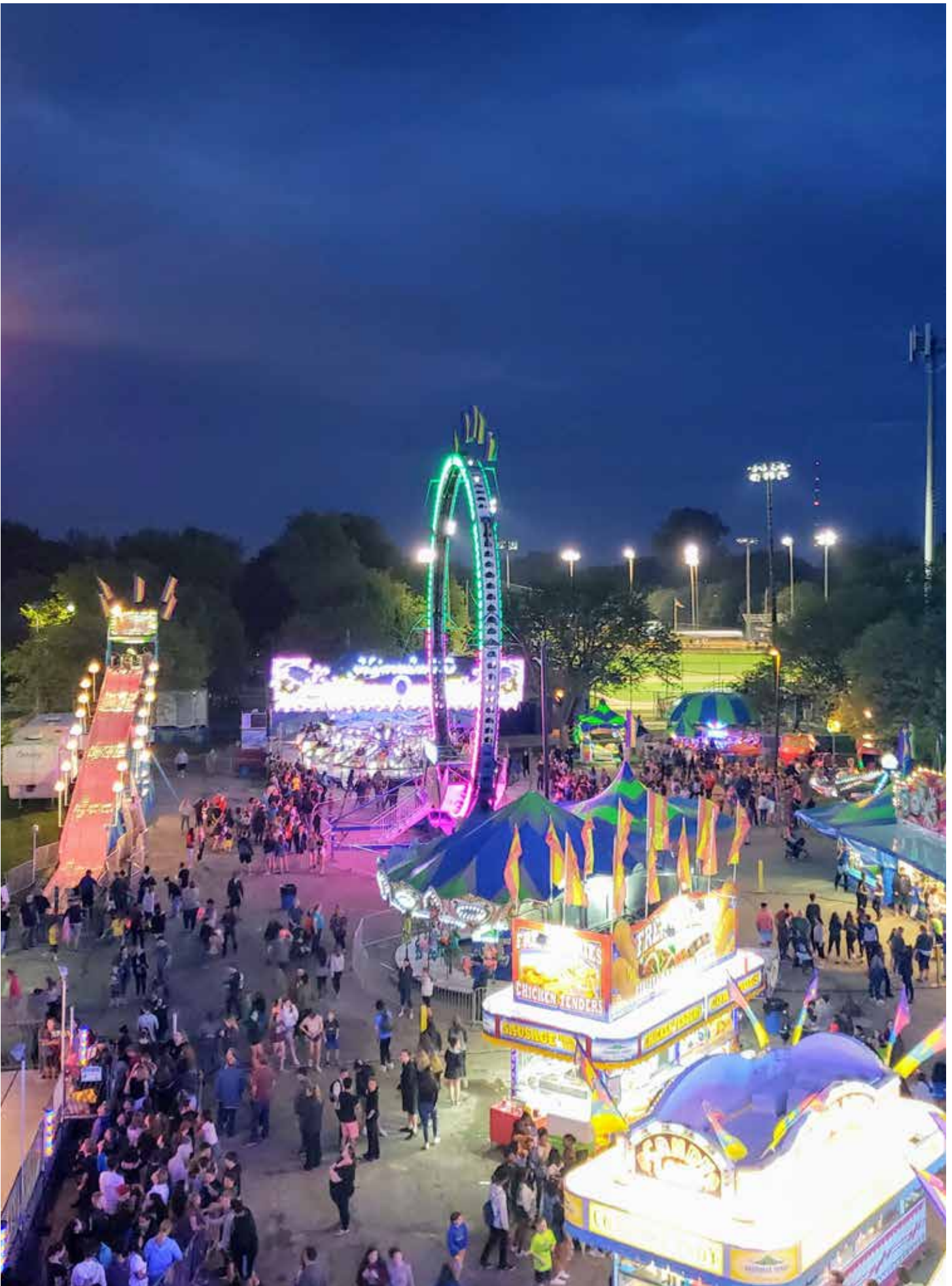
Before the City of Berkley was developed in the 1940s, the predominant vegetation of the area was hardwood swamp and bogs, being within the large, prehistoric lake plain of the Detroit area. The clay and poorly-drained soils underlying these wetland plant communities are still present today, which when combined with the large amount of impervious surfaces, present the City with flooding problems. The stormwater system in the City is a combined stormwater / sanitary system, most of which was installed during the 1940's. The City's parks and Roseland Park Cemetery, in the northeast corner of the City, provide some stormwater detention capability.

The City's topography is essentially flat. The land pitches gently from the northwest to the southeast, ranging in elevation from 710 to 660 feet.

Wildlife within Berkley is typical of urban areas with large, mature trees, and includes squirrels, chipmunks, raccoons, opossums, skunks, birds, and other small urban wildlife.

The City is within the Clinton River watershed and the Red Run sub-watershed. Stormwater runoff from Berkley makes its way into the Clinton River, and eventually to Lake St. Clair. Through watershed planning efforts, the Red Run sub-watershed communities have identified the following impacts from stormwater runoff on rivers and lakes:

- Pesticides and fertilizers from lawns;
- Oil and grease from roads and parking lots; and
- Sediment from construction sites and other land disturbances.



PLANNING INITIATIVES

City of Berkley Master Plan

The City of Berkley is in the process of updating their Master Plan update. This update is being prepared concurrently with the Recreation Plan efforts.

Prior to this update, Berkley last updated its master plan in 2007. Several land use goals and objectives were identified that could affect its role in providing parks, open space, and recreation uses. Due in part to the recent economic downturn, the projects have not yet been implemented, but remain items of discussion in the community. The goals or objectives relevant to parks and recreation include:

- Adding a Veteran's Park in the southwest corner of Roseland Park Cemetery. This park concept is undersigned and calls for the dedication of approximately 1.5 acres at the northeast corner of Coolidge / Twelve Mile intersection to honor the area's war veterans. Discussion with Cemetery representatives is needed to initiate this proposal.
- Addition of a mini-park at the terminus of Edgewood at Woodward Avenue. This park will be located within the current right-of-way, thereby closing this access point to Woodward.
- Addition of a linear park along Coolidge north of Twelve Mile, intended to provide a recreation opportunity to residents and also create a connection from Beaumont Hospital to Downtown Berkley.

Berkley Complete Streets Ordinance

The Berkley City Council passed a Complete Streets ordinance in March of 2018. The ordinance built off of years of effort following adoption of a resolution of support for Complete Streets in 2010. The ordinance enables the City to require adherence to complete principals during future developments, ensuring that pedestrian, cyclist, and mass transit uses are incorporated into future street designs.

The Ordinance directly lead to the Coolidge Highway Complete Streets Initiative, a 24-month evaluation period to determine if re-striping Coolidge between 11 Mile and 12 Mile from four lanes to three will improve traffic flow and make the road safer and friendlier for pedestrians and bicyclists.

An important element of the Coolidge Highway Road Diet are temporary bike lanes which will run in both directions from 11 Mile to 12 Mile. These lanes are intended to be used for evaluation purposes, with the long-term goal of becoming part of a proposed multi-community bike system that includes Huntington Woods and Oak Park. Ultimately, the results of the evaluation will determine the feasibility of such a project.

Berkley Bicycle Pathway System

The Berkley Bicycle Pathway System takes advantage of the existing sidewalk and traffic systems to create a safe route for riders of all abilities to enjoy. The 5.7 mile route runs primarily on residential roads with low (25 miles per hour) speed limits, allowing inexperienced cyclists the option to ride on sidewalks or while more experienced cyclists are able to ride directly on the road. The pathway system directly connects to four parks, Angell and Rogers Elementary Schools, and Anderson Middle School. It also brings users to commercial properties, and close to Berkley High School, the Berkley Public Library, and the downtown district.

A map of existing and proposed routes through the immediate region is provided on page 57.

Figure 23. Berkley Pathway System Flyer


Rules of the Road

- Drive on the right side of the road, in the direction of traffic.
- Obey all traffic lights and signals
- Start using hand signals about 100 feet from where you'll stop or turn.
- Never hitch a ride on moving vehicles.
- Don't carry passengers.
- Don't weave in and out of traffic.
- Pass cars only on the left where the driver can see you.


Dress for the Ride

- Wear a hard shell helmet.
- Wear bright-colored clothes at night; they are easier to see.
- Wear ankle clips or elastic bands around your pant cuffs so they don't get caught in the chain.
- Put reflectorized tape on your clothes at night.
- Wear gloves to protect your hands in case you fall.
- Wear shoes that tie or fasten so they don't fall off.

Berkley's Bicycle Pathway System



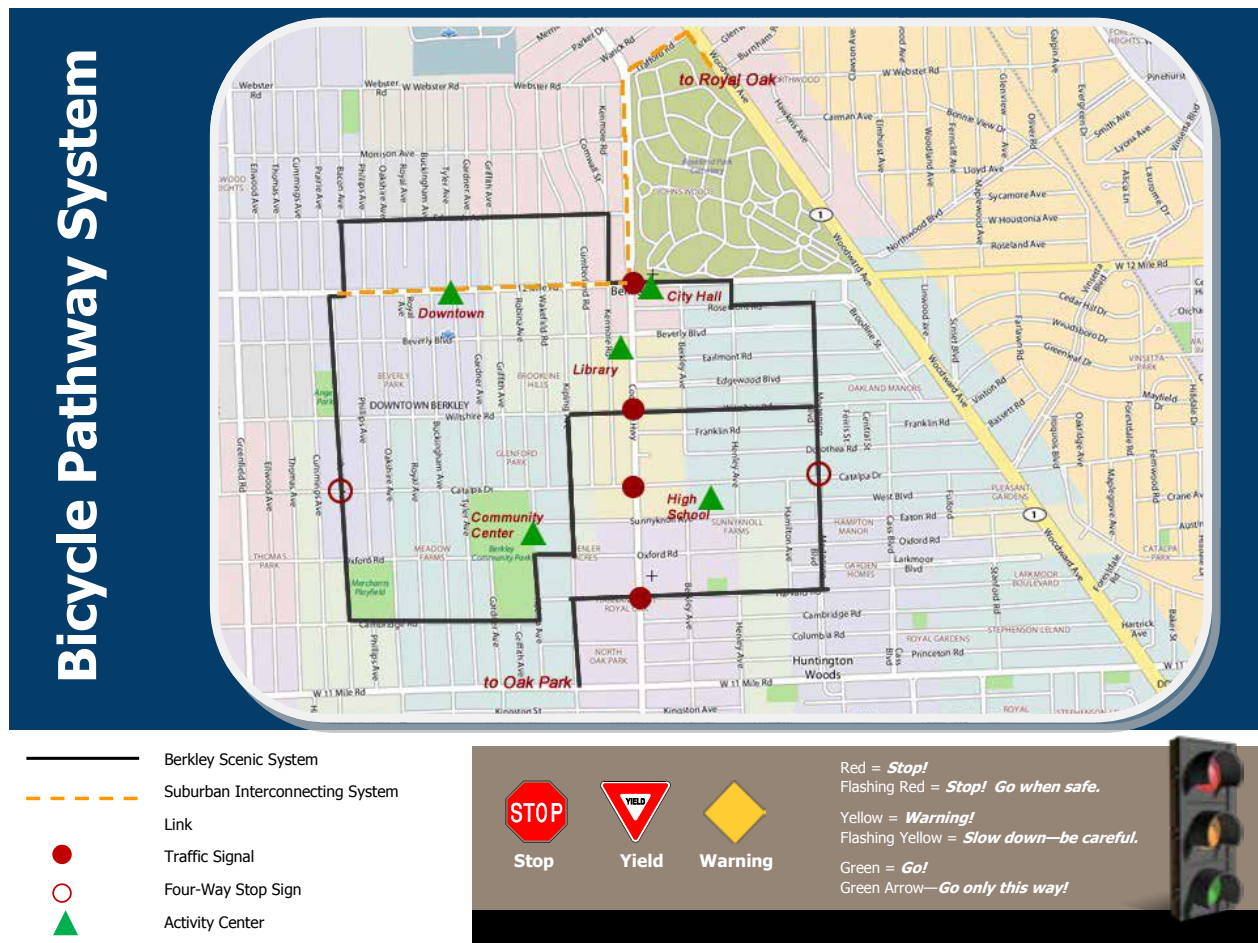
Bicycle Hand Signals



Left Turn
Right Turn
Stop

City of Berkley
3338 Coolidge Highway
Berkley, MI 48072
Phone: 248.658.3300
Fax: 248.658.3301
www.berkleymich.org





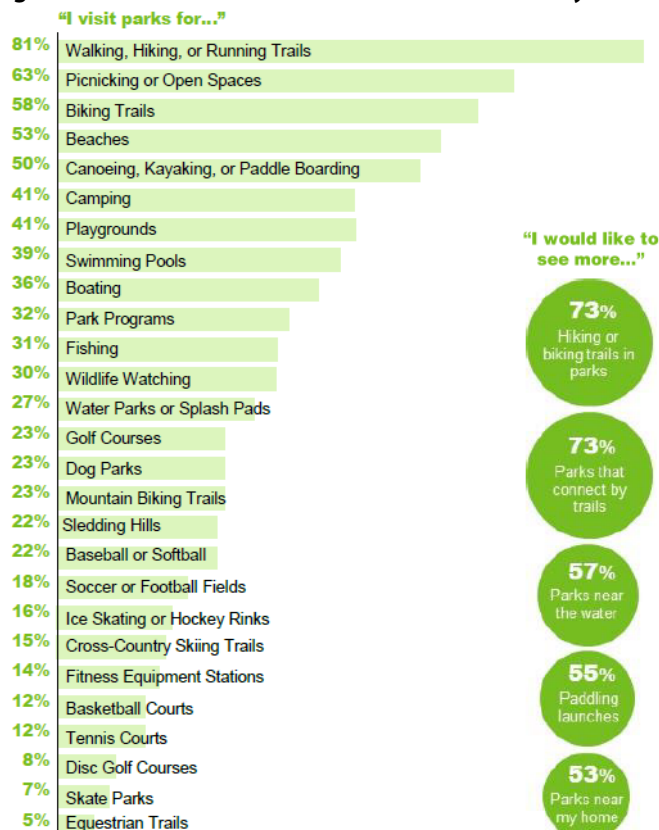
Oakland County Parks Five-Year Parks & Recreation Master Plan: 2018-2022

The 10-member Oakland County Parks & Recreation Commission (OCPRC) oversees 13 facilities totaling just over 6,700 acres. Catalpa Oaks, located in Southfield, is the closest County facility to Berkley. This facility offer a wide variety of recreation opportunities for area residents, including baseball/softball, cross-country ski trails, geocaching, picnic pavilions, playgrounds, sledding, snowshoes, and soccer facilities.

The OCPRC adopted their 5-Year Parks & Recreation Master Plan in February, 2018. A number of projects are proposed for Catalpa Oaks that will provide even more recreation opportunities for Berkley residents. Installation of electric pedestals will enable large special events, a new pavilion, a zero-depth water feature, sports fields rehabilitation, and development of a perimeter pathway are all planned.

Oakland County Parks help to fill the void for communities across the region by providing access to large scale, active use facilities that may be prohibitively expensive to construct and operate.

Figure 24. SEMCOG Parks & Recreation Plan Survey Results



Recreational trails
are among the most popular reasons to visit parks, and the amenity that most people would like to see more

SEMCOG Parks & Recreation Plan for Southeast Michigan

The SEMCOG plan, adopted in 2019, was developed to highlight available recreation resources and ensure that the region's recreation system meets the needs of residents and visitors. The Plan notes that with over 2,300 parks covering 214,000 acres, over 400 miles of trails and greenways, and 450 miles of designated water trails, the region is well served by local, state, regional, and federal recreation authorities.

With almost 67,000 acres of parkland, Oakland County offers 56 acres of park per 1,000 residents. This level is higher than the region's average of 43 acres per resident. In terms of pure park availability, Oakland's acreage is almost double that of the next highest county (Washtenaw at 38,695).

The plan provides several regional policy recommendations to address recreation development. Among the recommendations, prioritization of investment in land acquisition is viewed as necessary to address gaps in access to outdoor recreation facilities. Fostering collaboration among outdoor recreation stakeholders speaks to the need for ongoing cooperation between local and regional recreation authorities. Utilization of universal design principles ensures that facilities are accessible to residents of all abilities. Supporting conservation and stewardship of natural resources recognizes the value of open space and the desire of the region's residents to have access to natural areas in reasonable proximity to their homes.

SEMOG 2040 Regional Transportation Plan / 2014 Bicycle and Pedestrian Plan for SE Michigan

The 2045 Regional Transportation Plan provides broad transportation recommendations for projects across the seven-county region, with a budget of over \$5 billion proposed for transportation infrastructure improvements. While this plan focuses on motorized infrastructure, it does provide a chance to look at upcoming projects and plan accordingly for other related infrastructure improvements. No projects are specifically mentioned for Berkley, but reconstruction of Coolidge Highway is planned beginning in 2025. Generally speaking, road infrastructure improvements represent opportunities for adding to bicycle networks and implementing complete street programs.

The 2014 SEMCOG plan is a joint effort between Southeast Michigan Council of Governments (SEMCOG) and the Michigan Department of Transportation (MDOT). The plan provides recommendations for bicycle and pedestrian facilities across 10 regions and incorporating 60 different action items. SEMCOG’s larger regional focus places more emphasis on existing non-motorized transportation options.

Maps of existing and planned facilities highlight a number of existing routes running through the City. Woodward Avenue is called out as a potential regional corridor, although the majority of the route is currently shown as a gap in the network. As local and regional planning agencies move forward with non-motorized facility development efforts, it will be critical for Berkley officials to provide input to ensure that SEMCOG’s plans are consistent with the City’s own vision.

Figure 25. Detail: SEMCOG 2014 Bicycle & Pedestrian Plan

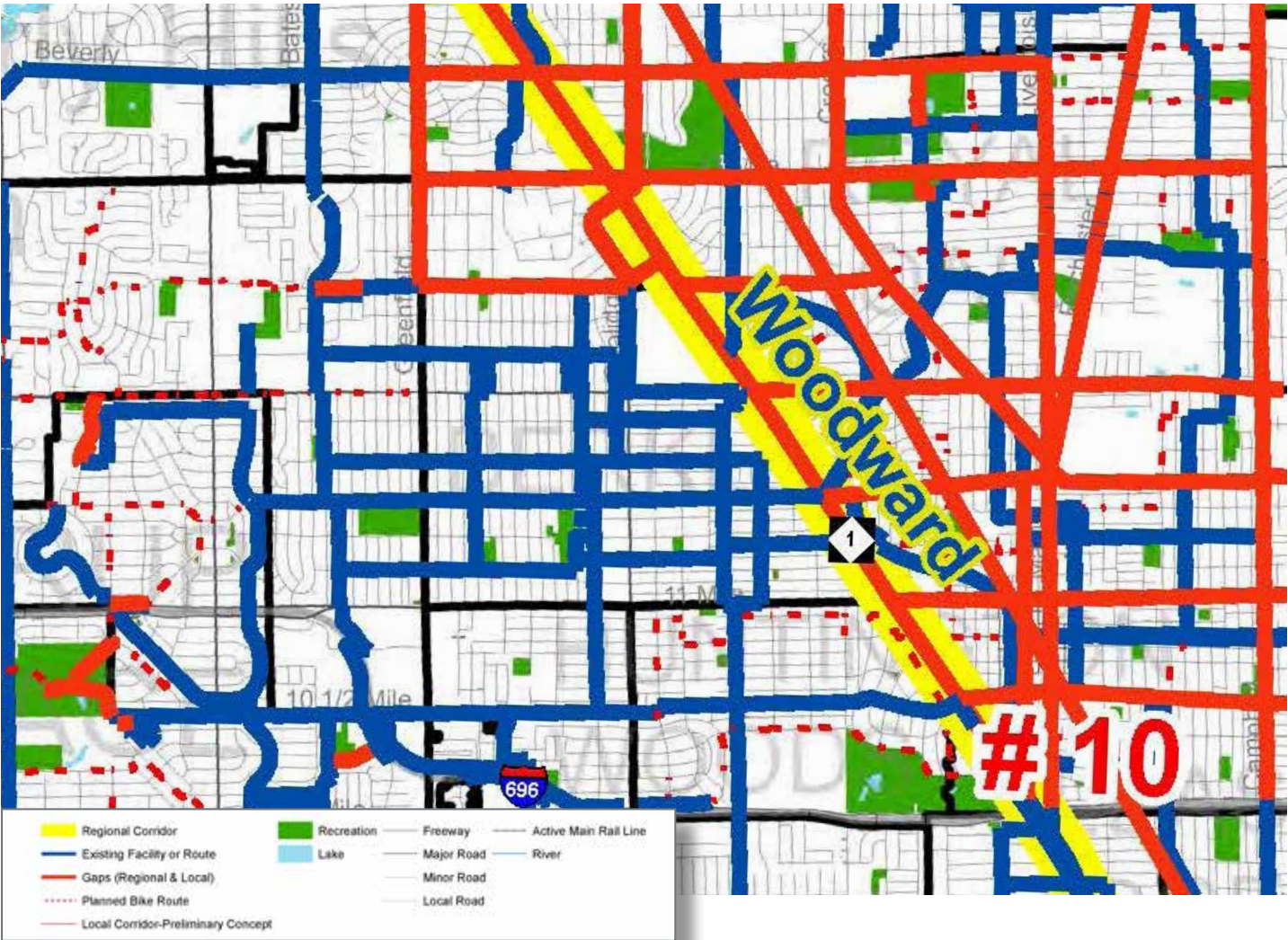


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Woodward Avenue Action Association (WA3) Planning & Development Studies

The Woodward Avenue Action Association (WA3) represents eleven communities (Oakland and Wayne Counties, the cities of Berkley, Birmingham, Bloomfield Hills, Ferndale, Highland Park, Huntington Woods, Pontiac, and Royal Oak, and Bloomfield Township) situated along Woodward Avenue. The Association advocates for promotion and development of the corridor, and has successfully obtained close to \$7 million in grants, memberships, and partnerships to benefit improvements and planning along the corridor.

Key accomplishments and studies completed by WA3 include:

- Completion of the Woodward Avenue Non-Motorized Plan;
- In 2002, obtained the designation of “National Scenic Byway” for Woodward Avenue;
- Obtained the designation “All-American Road” in 2009, one of only 30 such designations in the country;
- Prepared a Transit Oriented Design study used to secure a \$2 million grant for SEMCOG;
- Prepared an Alternatives Analysis Plan for Bus Rapid Transit;
- Prepared a Complete Streets Master Plan for Woodward, the largest such plan in the world; and
- Provided \$200,000 to the M-1 Rail / Woodward reconstruction effort to include pedestrian accommodations.

The WA3 Complete Streets Plan recommends protected bike lanes between Six and Eight Mile Roads, and painted bike lanes north to I-696. Transit stops, parking, and pedestrian improvements would also be accommodated.

WA3 and its member organizations will be key players in any future non-motorized developments along the Woodward corridor, and can provide valuable knowledge and expertise for connecting proposed routes along Eleven and Twelve Mile Roads.

Figure 26. Rendering of Bike Lanes Along Woodward Ave.



Adjacent Community Recreation and Non-Motorized Plans

Royal Oak, Southfield, Oak Park, and Huntington Woods all offer recreation resources that may be utilized by Berkley residents. Each community has developed their own plans to help guide ongoing recreation development efforts.

ROYAL OAK

Royal Oak has completed a number of studies and plans that relate to recreation and/or non-motorized route development in the city, including a Corridor Plan for Eleven Mile, a Non-Motorized plan in 2011, and a Recreation Plan in 2017. The city also passed a Complete Streets Ordinance in 2012, paving the way for a variety of trail and pathway related projects.

The 2011 Non-Motorized Plan in particular offers suggestions which directly impact Berkley. The proposed network map recommends a road diet and new bike lanes for Twelve Mile Road, shared lane markings for Webster Road, Catalpa Drive, and Eleven Mile Road, and connection points at Catalpa, Griffith, and Tyler Avenues. Woodward Avenue is shown as a “future improvement zone”, and refers to the Woodward Avenue Non-Motorized Plan for project guidance.

HUNTINGTON WOODS

Huntington Woods completed a Recreation Plan update in 2019. The Action plan calls for a number of improvements to park facilities and the city’s recreation center, and calling for improvements to pedestrian improvements at problem intersections such as Eleven Mile/Woodward and Lincoln/Woodward. The plan also recommends working with the Planning Commission to develop a complete streets plan, and to work with surrounding communities to ensure that planned non-motorized facilities are consistent with the values of Huntington Woods residents.

Figure 27. Royal Oak Bicycle Network Map

CITY OF ROYAL OAK Non-Motorized Transportation Plan Bicycle Network Map August 2011

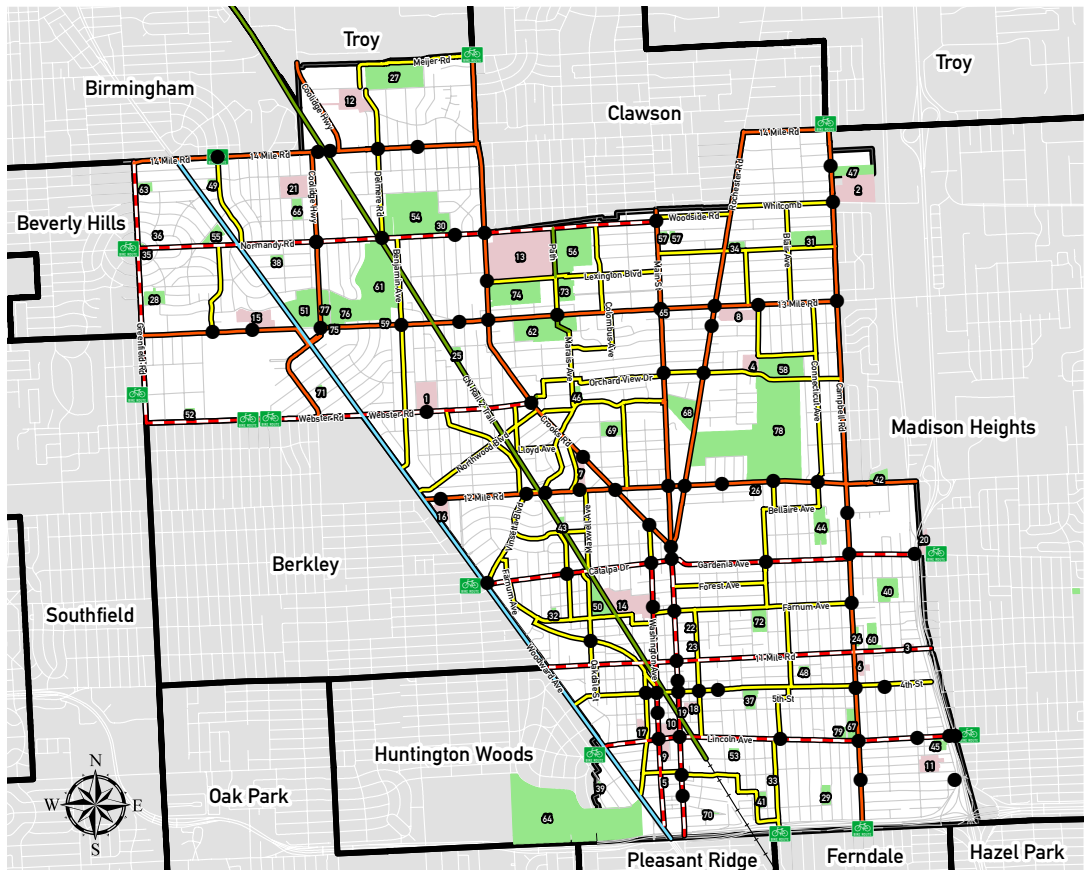
Bicycle Facility Recommendations

- Future Improvement Area
- Bike Route
- Bike Lane with Road Diet
- Shared Lane Marking
- Trail or Path

- Connection to Adjacent City
- Roads
- Railroad
- Schools
- Parks and Open Space
- Traffic Signal



Prepared By:
Active Transportation Alliance August 2011
Data Source:
Active Transportation Alliance, Royal Oak, SEMCOG



OAK PARK

Oak Park passed a resolution in support of a Complete Streets ordinance in February 2018. The city's Complete Streets Master Plan, also adopted in 2018, focuses on constructing non-motorized facilities along major thoroughfares such as Nine Mile, Eleven Mile, and Greenfield Roads and Coolidge Highway. Proposed bike routes are shown running the length of Coolidge and Eleven Mile Roads, although detailed design treatments for each corridor are not specified. Similarly, Oak Park's 2018 Recreation Plan calls for continuous pathway/trail system throughout the city.

SOUTHFIELD

The City of Southfield owns and operates 34 separate recreation facilities and has actively worked to create non-motorized, multi-modal transit opportunities for their residents. Southfield has developed a number of planning and policy documents related to recreation and non-motorized transportation in recent years, including the 2017 - 2021 Parks & Recreation Master Plan, the Non-Motorized Pathway and Public Transportation Plan, the Southfield City Centre Non-Motorized and Transit Sub-Area Plan, the Southfield DDA Non-Motorized and Transit Sub-Area Plan, the Non-Motorized Pathways and Pedestrian Amenities Plan, the 2016 Sustainable Southfield Plan, the Southfield City Centre Vision and Redevelopment Plan, and the Northland Redevelopment Plan.

The neighborhoods adjacent to Berkley have well developed sidewalk systems and Southfield's plans do not show any further development planned for the region. Proposed shared-use paths are indicated south of I-696 from Ten Mile to Nine Mile Road, with a crossing to the north side at Nine Mile, consistent with the routes indicated on Berkley's multi-community bike maps.

Figure 28. Southfield Non-Motorized Transit Plan

