

5.0 Design & Character

This chapter contains design guidelines and development standards that will guide and define the character of new development in the Downtown Specific Plan area. The design guidelines are intended to implement the vision and goals of the Specific Plan, as presented in the other chapters. The design guidelines are not directive, but are intended to establish the guiding principles for ensuring good design that effectively implement the goals and policies, as well as land use decisions, of the Downtown Specific Plan.

The Design Guidelines are crafted to:

- Provide property owners and developers with a clear vision of the type and quality of development the city desires and expects in Downtown.
- Serve as a set of guiding design principles for public officials, developers, designers and the community to use, which are sensitive to the conditions of each subarea of the planning area.
- Give the City of Burlingame tools to evaluate and guide project design.
- Supplement the Commercial Design Guidebook with guidelines and standards specific to Downtown.

The guidelines and standards that have been developed for the Downtown Specific Plan area are based on the land uses and character of each Downtown subarea. The Subareas are described in the Land Use Chapter (Chapter 3) of this plan.

Within the Specific Plan area, any actions proposing substantial physical changes to any parcel of land or existing structure, or the proposed construction of new structures, shall be subject to Design Review as outlined in Section 25.57 of the Burlingame Municipal Code. Applications shall be reviewed for consistency with all applicable Downtown Specific Plan and General Plan provisions, and applicable City ordinances and standards. Design guidelines and standards in both the *Downtown Specific Plan* and the *Commercial Design Guidebook* apply to all downtown projects and provide the basis for design review.

5.1 DOWNTOWN ARCHITECTURAL SETTING

Downtown Burlingame was the focal point of the City during its early development, and over the years it has continued to be the symbolic center of the community. It continues to be a defined, identifiable place with distinct boundaries and a unique urban scale. A range of architectural styles and periods are represented within Downtown and serve to create a distinctive character for the area, one that is highly valued by the City's residents and that leaves a lasting, positive impression upon visitors. New buildings and rehabilitation projects should draw from and build upon this character.





FIGURE 5-1: The core commercial areas centered on Burlingame and Howard Avenues features a range of architectural styles and periods.



FIGURE 5-2: Commercial and mixed use development projects in the Downtown Specific Plan area are subject to the City of Burlingame's *Commercial Design Guidebook*.

In the commercial areas, there is a consistency and cohesion of architectural styles. Many buildings utilize classical proportions, and are enriched with detailing such as pilasters, wood detailing, and embossed relief. There are also some fine modern buildings, which overall are compatible in scale and detail with more historical examples. The "core" area centered around Burlingame and Howard Avenues functions as a defined retail center.

In the residential neighborhoods, styles are more varied. Cohesion is achieved by compatibility in building scale and massing, along with consistently lush landscaping.

All buildings within each area of Downtown Burlingame should contribute to the area's identity as a part of Downtown Burlingame. The core commercial areas centered on Burlingame and Howard Avenues should have a lively mix of buildings at different heights and styles. Ground floor retail should relate to Downtown's traditional storefronts by using large display windows, kickplates, and clerestory and transom windows. In the California Drive commercial areas, development may be lower in intensity but should continue to build on the Downtown core's classic, restrained styling. In the residential areas, new projects should reinforce the fine-grained scale and quiet amenity that exists.

The variety of architectural styles is an asset to Downtown, and both historically inspired and modern styles should be accommodated. Regardless of architectural style and approach, new buildings should exhibit fine-grained, pedestrian-friendly scale and details.

5.2 DESIGN STANDARDS FOR COMMERCIAL AND MIXED USE AREAS

The commercial areas of Downtown Burlingame have historically been the most active, public places in the community. New commercial and mixed use buildings should contribute to the existing "Main Street" character. They should enhance the pedestrian nature of Downtown, defining the street as a public place, with active storefronts, windows,

and doors at ground level. Architecture should include the type of well-crafted architectural details that are common to Burlingame, and convey that architectural heritage in terms of material, color, proportion, window type, and overall composition.

Commercial and mixed use development projects in the Downtown Specific Plan area are subject to the City of Burlingame's *Commercial Design Guidebook*. In addition, the following recommendations apply specifically to Downtown development:

5.2.1 PEDESTRIAN USE AND CHARACTER

5.2.1.1 Entrances

Commercial entrances should be recessed from the façade, creating a small alcove. This establishes a more definitive sense of entry and affords an alternative view of merchandise in the display windows. Existing recessed entries should be retained.

The doors of a commercial storefront typically contain large glass panels with vertical proportions that present a visual connection to the streetscape. Storefronts should continue to exhibit this pattern, whether a new project or the re-use of an existing space.

5.2.1.2 Ground-Level Corner Uses

High activity-generating uses are especially encouraged at the Burlingame Avenue and Howard Avenue intersections with side streets. Store façades along side streets should be designed to help entice pedestrians onto the side streets. To achieve this, the façades should include windows and continuation of the architectural details from the main storefront extending across the sidestreet façade. Entries to elevator lobbies should not be located at these intersections where they would serve to diminish pedestrian activity at these highly visible locations.



FIGURE 5-3: Commercial entrances should be recessed from the façade, creating a small alcove.



FIGURE 5-4: Corner parcels are encouraged to incorporate special features such as rounded or cut corners, special corner entrances, display windows, corner roof features, etc. but should avoid monumentally-scaled elements such as towers.



FIGURE 5-5: Particular attention should be given to craftsmanship and detailing within the pedestrian's range of touch and view.



FIGURE 5-6: Downtown Burlingame is characterized by relatively narrow building increments, predominantly 15 to 50 feet in width.

5.2.1.3 Ground Level Treatment

The unique community character created by the mixture of building ages and architectural styles should be maintained. All street-frontage establishments should provide primary access directly to the street.

Particular attention should be given to craftsmanship and detailing within the pedestrian's range of touch and view. For instance, the use of special storefront detailing and façade ornamentation such as planters, flower boxes, and special materials can reinforce the pedestrian nature of the street.

To ensure ease in caring for landscaping, major remodels and new projects should provide outdoor water spigots and electric sockets. When businesses have access to water, they can more easily care for their plants and trees, and keep the streets cleaned as well.

5.2.1.4 Site Access

Curb cuts are prohibited on Burlingame Avenue and should be avoided to the extent feasible on Howard Avenue and California Drive. Any on-site parking garage should be accessed in a safe, attractive manner and should not significantly detract from pedestrian flow, nor interfere with the orderly flow of traffic on public streets and within parking lots. Where possible, parking garage access should be from the side streets or alleys. In some cases, access to on-site parking could be provided from city-owned parking lots.

5.2.2 ARCHITECTURAL COMPATIBILITY

5.2.2.1 Building Scale

Table 3-2 in Chapter 3 specifies basic building standards such as setbacks and height. Beyond conforming to the basic building mass, new development should preserve the rhythm and fine-grained pedestrian scale of existing buildings within the commercial districts by respecting the relatively narrow building increments, which typically range from 15 feet to no more than 50 feet in width. To be consistent with the existing character of Downtown Burlingame, to provide a welcoming retail environment, and to accommodate a range of potential uses over the lifetime of the building, first floors should have a floor to finished ceiling height of at least 15 feet.

New development should also be sensitive to the human scale of Downtown with sensitivity to building height. Buildings should not overwhelm the pedestrian experience on the street and should account for the relationship between building height and street width. Where building mass and height might overwhelm the pedestrian experience on the street, design strategies such as upper floor setbacks and articulated building mass should be considered to ensure comfortable human scale.

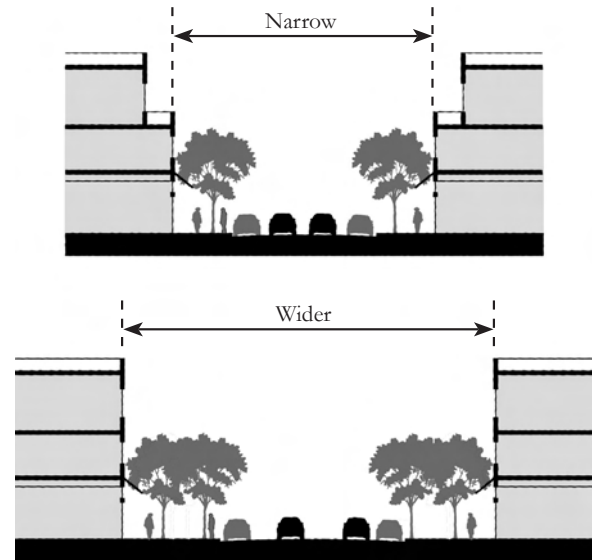


FIGURE 5-7: Buildings should not overwhelm the pedestrian experience on the street and should account for the relationship between building height and street width.

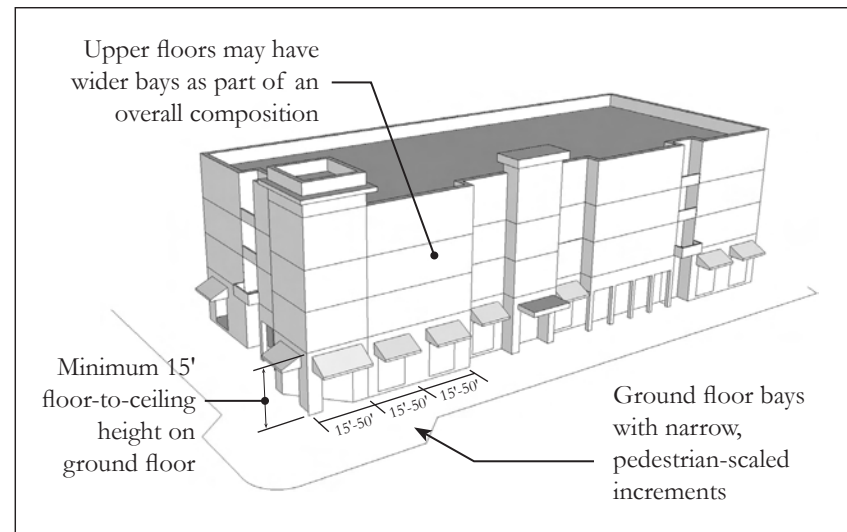
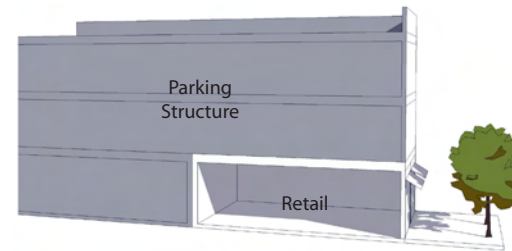


FIGURE 5-8: Building scale should preserve the rhythm and fine-grained pedestrian character of downtown, particularly at the pedestrian level.

FIGURE 5-9: ON-SITE STRUCTURED PARKING IN COMMERCIAL AND MIXED USE AREAS**A. Wrapped on Ground Level**

An above-ground parking structure where non-parking uses such as retail spaces are integrated into the ground level of the building along the street frontage of the parcel. The parking structure may be exposed to the building street frontage on upper levels, with appropriate design and screening.

Application: Municipal parking structure.

**B. Wrapped on All Levels**

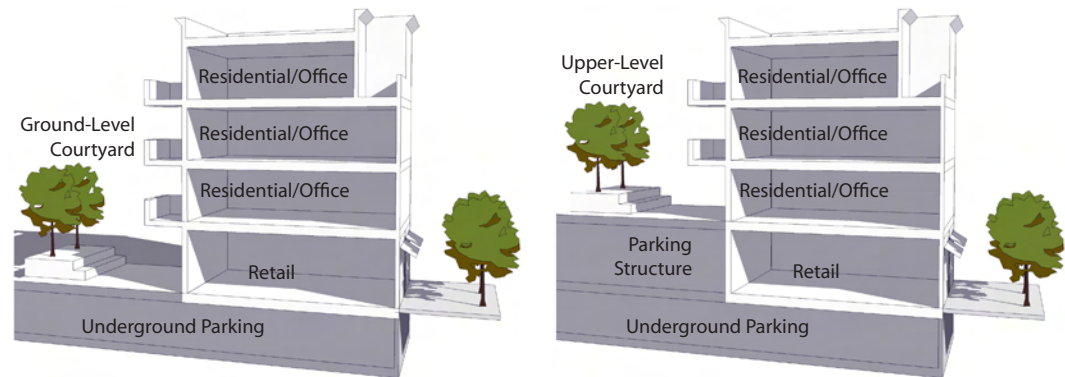
An above-ground parking structure where non-parking uses are integrated into the building along the entire street frontage of the parcel on all levels of the building. The parking structure is totally hidden behind a "liner building" of non-parking uses.

Application: Projects with relatively large amount of parking provided on-site. Typically requires a relatively large site to accommodate the parking structure and liner building.

**C. Underground**

A parking structure that is fully submerged underground and is not visible from the street. Depending on amount of parking provided, may also include a level of at-grade parking hidden behind non-parking uses such as retail.

Application: Can be suitable for projects on relatively small sites, as well as larger sites. Could also be combined with in-lieu arrangement, where some parking is provided on-site (such as for residential uses) and other parking is provided off-site in a municipal facility through in-lieu fees.



5.2.2.2 On-Site Structured Parking

Given the density and premium land values Downtown, new projects will likely provide on-site parking in enclosed garage structures or underground. However, the parking should not overwhelm the character of the project or detract from the pedestrian environment. Ground level enclosed parking should be fronted or wrapped with actively occupied spaces such as storefronts and lobbies. Access to parking shall be designed so that it is not prominent and ties into the adjacent architectural style.

5.2.2.3 Upper-Story Setbacks – Burlingame Avenue Frontages

While the height limit allowed by conditional use permit is 55 feet on Burlingame Avenue, many existing buildings and in particular, many buildings with historic character, have façades of a smaller scale. New buildings and building additions should reinforce the historic pattern with heights and setbacks oriented to the many two- and three-story buildings. Where neighboring buildings are three stories or lower in height, newer taller buildings should consider matching lower façades to those of adjoining lower buildings and setting upper floors back at least 10 feet from the lower façade.

5.2.2.4 Myrtle Road Mixed Use Area

The unique mix of residential and commercial uses in the Myrtle Road Mixed Use area offers an opportunity to create a niche district with its own style distinct from other parts of downtown. Recognizing the varied auto-related commercial character of the area, new development and redevelopment projects within the Myrtle Road Mixed Use Area should be encouraged to feature a blend of both commercial and residential design features. Design features could include corrugated metal roofs and sidings, simple multi-paned metal rimmed windows, and recycled "green" building materials. Buildings may even draw inspiration from the style of utilitarian buildings found in such mixed use districts such as sheds and quonset huts. The creation of this commercial, live/work identity for the Myrtle Road area will allow it to be a unique subarea of Downtown Burlingame that accommodates infill while respecting existing uses.

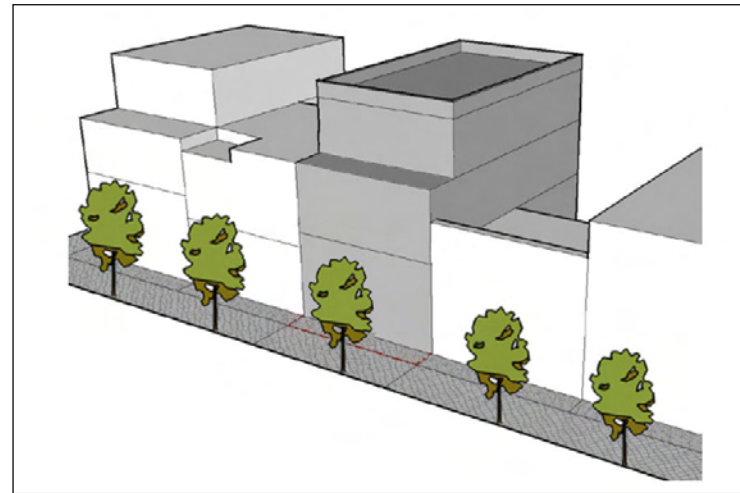


FIGURE 5-10: Where neighboring buildings are three stories or lower in height, newer taller buildings should consider matching lower façades to those of adjoining lower buildings with upper floors set back.



FIGURE 5-11: Design features such as corrugated metal roofs and sidings, simple multi-paned metal rimmed windows, and recycled "green" building materials can maintain the existing varied character of the Myrtle Road Mixed Use Area.



FIGURE 5-12: Facades on both new and rehabilitated buildings should include the elements that make up a complete storefront including doors, display windows, bulkheads, signage areas and awnings.

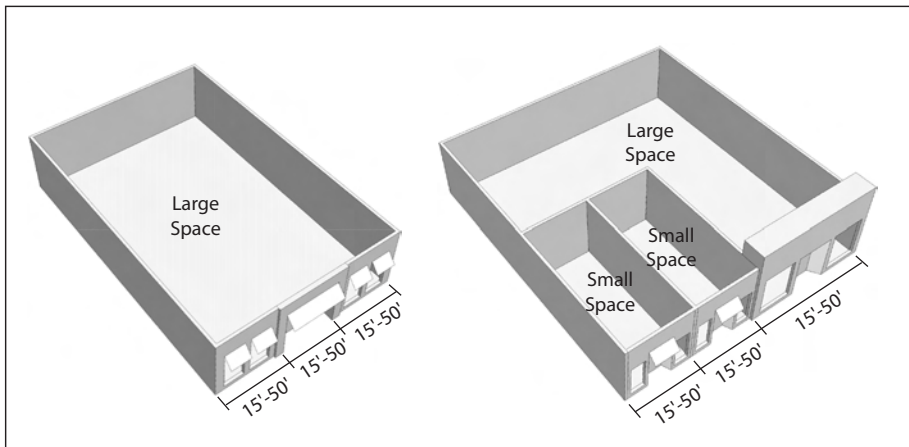


FIGURE 5-13: Even if separate businesses function within the same building, the overall design of the façade should be consistent. Individual businesses should not break the basic lines, material and concept of the facade.

5.2.3 ARCHITECTURAL DESIGN CONSISTENCY

5.2.3.1 Facade Design

To maintain the present scale and character of buildings in Downtown, large uninterrupted expanses of horizontal and vertical wall surface should be avoided. Building façades should respond to the relatively narrow increments of development (15 to 50 feet) with variation in fenestration, building materials and/or building planes. Facades should have generous reveals such as inset doorways and windows. Doors, windows, and details should be in keeping with pedestrian scale, as opposed to a monumental scale that is out of proportion to the surrounding context. Design details should be authentic and have purpose, rather than being applied or strictly decorative. Facades should have a variation of both positive space (massing) and negative space (plazas, inset doorways and windows).

Facades on both new and rehabilitated buildings should include the elements that make up a complete storefront including doors, display windows, bulkheads, signage areas and awnings. New buildings need not mimic an “historic” architectural style (and in fact should avoid imitation that results in caricatures) but should include a level of architectural detailing and quality of materials that complements existing buildings. Where older existing buildings are renovated, preservation of existing architectural details and materials is encouraged.

Even if separate businesses function within the same building, the overall design of the façade should be consistent. Individual businesses should not break the basic lines, material and concept of the façade. Storefronts can be demarcated from each other within the same building by subtle variations in the color or pattern of surfaces of doors, tiling, signage or entries. Corner parcels are encouraged to incorporate features such as rounded or cut corners, corner entrances, display windows, corner roof features, wrap-around awnings/overhangs, blade signs, etc.

5.2.3.2 Windows

General

Windows are important for providing "eyes on the street" and enlivening streetscapes. Building walls should be punctuated by well-proportioned openings that provide relief, detail and variation on the façade. Windows should be inset from the building wall to create shade and shadow detail. The use of high-quality window products that contribute to the richness and detail of the façade is encouraged. Reflective glass is considered an undesirable material because of its tendency to create uncomfortable glare conditions and a forbidding appearance. The use of materials that are reflected in the historic architecture present in the Downtown area is encouraged.

Display Windows

Display windows should be designed to enliven the street and provide pedestrian views into the interior of the storefront. Size, division and shape of display windows should maintain the established rhythm of the streetscape. Glass used in the display windows should be clear so it is possible to see inside, and display cases that block views into stores are strongly discouraged. Noticably tinted glazing is discouraged and mirrored/reflective glass is not permitted.

5.2.3.3 Awnings

Awnings should be designed to be decorative, complimentary to the overall facade design, and provide effective weather and sun protection. The placement of awnings should relate to the major architectural elements of the facade, avoiding covering any transom windows or architectural elements such as belt courses, decorative trim and similar features. The position of awnings should also relate to the pedestrian and provide a sense of shelter, with awnings situated to correspond to the tops of doorways and scale of pedestrians rather than high up on the facade with a monumental scale. Separate awnings should be used over individual storefront bays as defined by the columns or pilasters rather than placing a continuous awning across the



FIGURE 5-14: Size, division and shape of display windows should maintain the established rhythm of the streetscape



FIGURE 5-15: Awnings should be designed to be decorative, complimentary to the overall facade design, and provide effective weather and sun protection.



FIGURE 5-16: Rear and side facades that are visible from the public realm should exhibit sophisticated levels of design and materials of a quality similar to front facades. Buildings facing public parking lots are strongly encouraged to have rear entrances in addition to their principal street entrances.



FIGURE 5-17: Service facilities such as trash enclosures and mechanical equipment should be screened with enclosures and devices consistent with the building architecture in form, material and detail.

building frontage. Backlit awnings that visually appear as large light sources will not be permitted.

5.2.3.3 Materials

Building materials should be richly detailed to provide visual interest; reference should be made to materials used in notable examples of historic Downtown architecture. Metal siding and large expanses of stucco or wood siding are also to be avoided, except in the Myrtle Mixed Use area. Roofing materials and accenting features such as canopies, cornices, and tile accents should also offer color variation.

Character and richness in Downtown can be enhanced from the incorporation of details and ornamentation into the design of the buildings. These elements can include elements that have been traditionally used such as cornices, brackets or moldings.

5.2.3.4 Rear and Side Facades

Because the side streets and alleys in Downtown are highly visible and are used for both pedestrian access and vehicular access, rear and side façades that are visible from the public realm should exhibit sophisticated levels of design and materials. Rear and side façades of existing buildings should be improved with design features and quality materials where possible. Buildings should have windows and doors oriented to the alleys and side streets. Entry doors, garage doors and windows should be attractive and durable. Where buildings abut public parking lots, they are strongly encouraged to have rear entrances in addition to their principal street entrances. Rear facades may look like the back of a building, but still be pleasant and inviting.

Service facilities such as trash enclosures and mechanical equipment should be screened with enclosures and devices consistent with the building architecture in form, material and detail. Roofs and trellises are recommended for screening views from above. Whenever possible, trash and recycling enclosures should be consolidated and designed to serve several adjacent businesses provided they do not become over-

sized or too ungainly. Care should be taken to ensure refuse areas do not become noxious or smelly.

Where security devices are desired or warranted, designs should be artful with decorative grillwork that enhances the overall building design. Alley areas should be well lit but should be designed so they are attractive and do not adversely impact adjacent properties and detract from the ambiance of Downtown.

5.2.4 SITE DESIGN AND AMENITIES

5.2.4.1 Building Coverage

In order to create well-defined street spaces consistent with the scale of Downtown Burlingame, side yards are generally discouraged in favor of contiguous building façades along the street. However, narrow mid-block pedestrian passages that encourage through-block pedestrian circulation and/or arcaded spaces that create wider sidewalk areas for cafés, etc. are encouraged.

5.2.4.2 Open Space

Private open space within Downtown is not intended to provide recreational or large landscaped areas, since this is a more urban environment. However, open space is an important element and should be used to articulate building forms, promote access to light and fresh air, and maintain privacy for Downtown residents.

In residential mixed-use developments, most open space should be used to provide attractive amenities for residents, including interior courtyards and perimeter landscaping. Balconies and rooftop terraces are encouraged. Commercial development should typically have less open space in order to maintain a direct pedestrian relationship and continuous storefront streetscape. Entry alcoves, courtyards, and employee open space are examples. Open space for nonresidential projects should provide a visual amenity for the development and an attractive buffer to adjacent residential uses where applicable.



FIGURE 5-18: Open spaces such as retail plazas and outdoor seating areas should be located at building entries, or along or near well-traveled pedestrian routes to encourage frequent and spontaneous use.



FIGURE 5-19: In residential mixed-use developments, most open space should be used to provide attractive amenities for residents, including interior courtyards and perimeter landscaping.



FIGURE 5-20: To reinforce the Downtown commercial character of Downtown Burlingame, mixed-use buildings with a residential component shall conform to the setback standards for commercial projects.

Open spaces such as retail plazas and outdoor seating areas should be located at building entries, or along or near well-traveled pedestrian routes to encourage frequent and spontaneous use. Amenities should be functional as well as visually appealing, with seating, tables, canopies and covering trellises. Plazas and open spaces should be generously landscaped with trees, planters and vines. Permeable paving and/or creative site planning elements such as rain gardens are encouraged to alleviate the impacts of paved areas on drainage.

Low walls may be used to screen service and mechanical areas, create spatial definition and to provide seating. Low walls should be designed of quality materials that are complementary to the architecture of the primary structure(s) on the property.

5.2.5 RESIDENTIAL MIXED-USE DEVELOPMENTS WITHIN COMMERCIAL AREAS

5.2.5.1 Setbacks

To reinforce the Downtown commercial character of Downtown Burlingame, mixed-use buildings with a residential component shall conform to the setback standards for commercial projects (outlined in Table 3-1 in Chapter 3). The Community Development Director may allow increased side and rear setbacks to enhance the residential portion of a mixed-use project provided the setbacks do not detract from the commercial storefront character of the Downtown district. Setbacks and overall building form should maintain the human scale of Downtown and be in keeping with the character of the surroundings, with emphasis on maintaining an active street edge and sidewalk boundary.

5.2.5.2 Noise and Ground Vibrations

Projects with a residential component on California Drive should be designed to minimize noise impacts on residents from the Caltrain

line. A noise analysis prepared by a qualified acoustical engineer shall be required for all residential projects fronting California Drive. The acoustical engineer's report shall identify any noise impacts and measures to reduce these impacts to acceptable levels.

5.2.5.3 Parking Design

Parking for residential uses shall be provided on-site per Downtown Specific Plan and zoning code requirements. Parking garage access should be integrated into the overall design of the building façade, should minimize disruptions along the street frontage, and impact should be softened with choice of materials and design details. Wherever possible, access should be provided from rear alleyways, or the least conspicuous location. Conveniently located and accessed bicycle parking is encouraged.

5.2.5.4 Service Areas

Design of service areas shall be consistent with the general guidelines for rears of buildings in the commercial districts. On-site trash and recycling receptacles should be consolidated in an enclosure that is easily accessible for garbage pickup from a street or alley, and should be designed to serve multiple buildings whenever possible.

5.2.5.5 Ground-Level Treatment

Commercial frontages should meet the general guidelines for ground-level treatment in the commercial districts. Commercial spaces should have a depth of at least 40 feet to ensure viability for a range of potential commercial tenants. Residential and commercial entrances should be separate and distinct. Common residential entries shall be designed in a manner to minimize their appearance at street level, so as not to adversely impact pedestrian character at street level. Stoops for residential units may also be appropriate in some instances.

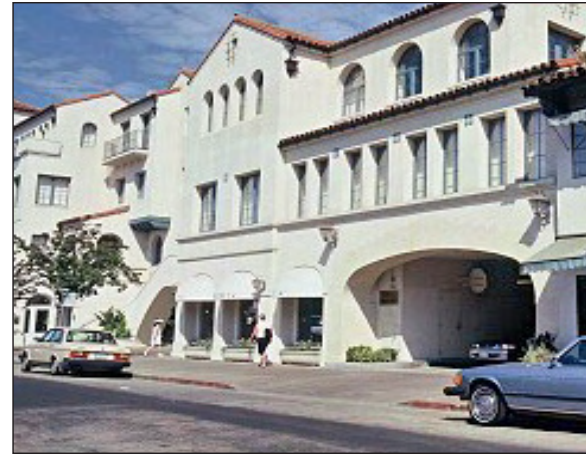


FIGURE 5-21: Parking garage access should be integrated into the overall design of the building façade and should minimize disruptions along the street frontage.



FIGURE 5-22: New residential development on larger parcels should echo the narrow parcel increments that characterize Downtown, with sensitivity to the traditional building size and storefronts.

5.2.5.6 Development Massing

Mixed use buildings will typically be taller than many of the existing buildings in Downtown. This additional height requires particular attention to the massing of the buildings to ensure an appropriate transition with the surrounding development. New residential development on larger parcels should echo the narrow parcel increments that characterize Downtown, with sensitivity to the traditional building size and storefronts.

5.2.5.7 Facade Treatment

To maintain the scale and character of the Downtown district, large, uninterrupted expanses of horizontal and vertical wall surface should be avoided. Building façades should respond to the relatively narrow patterns of development (15 to 50 feet) with variation in fenestration, building materials and/or building planes. Stoops and balconies can enliven façades and allow "eyes on the street."

Corner parcels are encouraged to incorporate special features such as rounded or cut corners, special corner entrances, display windows, corner roof features, etc. but should avoid monumentally-scaled elements such as towers. Mixed use buildings should continue architectural treatments from the front around to exposed side and rear façades, and should include windows on any exposed wall.

5.2.5.8 Roof Treatment

Mixed-use buildings with a residential component should exhibit rooflines and architectural character consistent with the Downtown commercial character. Rooftop equipment shall be concealed from view and/or integrated within the architecture of the building and screened for noise. Roof terraces are encouraged for enjoyment by residents, and green roofs and cool/white roofs are encouraged to mitigate heat transmission.

5.2.5.9 Lighting

Exterior lighting features shall be of an intensity and design to maintain the small town ambiance of Downtown. Exterior lighting shall be designed and located so that the cone of light and/or glare from the lighting element is kept entirely on the property or below the top of any fence, edge or wall.

5.2.5.10 Open Space

Open Space in mixed use projects falls into three categories: public-oriented open space, semi-public open space, and private open space. Design and landscaping should respond to the particular use and nature of each type of open space:

- *Public-oriented open space* designed to be accessed by the general public, such as entry plazas. Public-oriented open space should be welcoming and include pedestrian-scaled amenities that invite social interaction such as benches and planters.
- *Semi-public open space* such as outdoor dining areas and residential courtyards. These spaces have a more limited access, defined by elements such as low walls, landscaping elements, and decorative gates. For mixed use projects with a residential component, open space should include evergreen trees for screening, specimen trees for visual color, and attractive shrubs and ground cover. Low walls and planters may be used to provide privacy between open space areas and residences. Semi-public open spaces should also be designed with pedestrian-scaled amenities that invite informal social interaction such as seating areas, clustered mailboxes, and inset doorways.
- *Private open space* such as balconies, patios, and stoops. Private open space can be defined with elements such as railings, low walls, but should be encouraged to maintain views and provide interaction with the street and other more publicly-oriented open spaces to provide "eyes on the street."

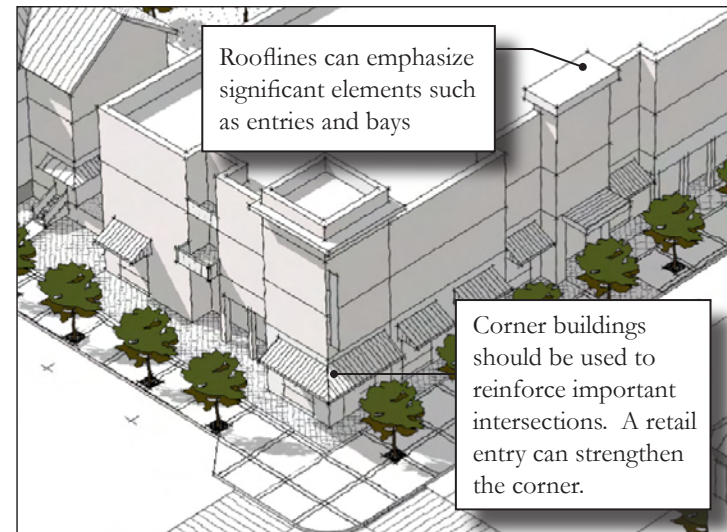


FIGURE 5-23: Corner parcels are encouraged to incorporate special features such as rounded or cut corners, special corner entrances, display windows, corner roof features



FIGURE 5-24: Mixed-use buildings with a residential component should exhibit an architectural character consistent with the Downtown commercial character.



FIGURE 5-25: An above-grade parking structure may be considered when it can be located on the rear of the lot, with appropriate commercial uses along the front and sides.



FIGURE 5-26: Passageways connecting the parking lot development with nearby commercial streets should be carefully detailed to enhance the pedestrian experience by leading pedestrians to the active shopping areas.

5.2.6 MIXED-USE DEVELOPMENT ON PUBLIC PARKING LOTS

For infill development on City-owned public parking lots, all development regulations and guidelines for the respective district apply. In addition, all required parking for the new development as well as parking to replace all existing spaces shall be provided on-site in a parking structure, ideally with access from an alley and one of the adjacent side or cross-streets. Where possible, parking structures should be provided underground. An above-grade parking structure may be considered when it can be located on the rear of the lot, with appropriate commercial uses along the front and sides. Rooftop parking may also be possible in certain situations.

Infill buildings should be carefully designed and detailed so that scale and massing responds to the traditional small Downtown parcel scale and provides a sensitive transition to adjacent residential neighborhoods.

Passageways connecting the parking lot development with nearby commercial streets should be carefully detailed to enhance the pedestrian experience by leading pedestrians to the active shopping areas. Passageways should include windows and doors from the adjacent building, seating, planting and attractive lighting. Where the passageway meets the new building, a carefully designed transition such as a plaza open space should occur and a continuation of the passageway to the side street should be considered. Bicycle parking should be incorporated into the passageway design whenever possible.

5.3 DESIGN STANDARDS FOR RESIDENTIAL AREAS

Residential buildings in Downtown Burlingame offer higher density development than elsewhere in the City, providing a lifestyle for those who want to live within walking distance of the Downtown commercial areas and transit opportunities. New buildings will mediate this density with thoughtful design and details that create attractive, livable residential environments. Buildings should contribute to an appealing neighborhood character and should employ recognizable residential design details such as visible residential entries, porches, bay windows and roof overhangs, and balconies and small outdoor areas.

Below are recommendations for the architectural treatment and organization of buildings and open space, and the suggested criteria for reviewing projects during the design review process.

5.3.1 ARCHITECTURAL DIVERSITY

Residential projects should respect the diversity of building types and styles in the residential areas Downtown and seek to support it by applying the following principles:

- Design buildings to maintain general compatibility with the neighborhood.
- Respect the mass and fine scale of adjacent buildings even when using differing architectural styles.
- Maintains the tradition of architectural diversity, but with human scale regardless of the architectural style used.
- Create buildings with quality materials and thoughtful design to last into the future.

5.3.2 PEDESTRIAN USE AND CHARACTER

5.3.2.1 Entrances

Primary pedestrian access to all ground-level uses should be from the sidewalk along the public street. Entries should be clearly defined features of front façades. Common entrances for multiple units are



FIGURE 5-27: Buildings should contribute to an appealing neighborhood character and should employ recognizable residential design details such as visible residential entries, porches, bay windows and roof overhangs, and balconies and small outdoor areas.



FIGURE 5-28: Entries should be clearly defined features of front façades, and are encouraged to have appropriately-scaled, usable gathering spaces that invite informal social interaction with neighbors.

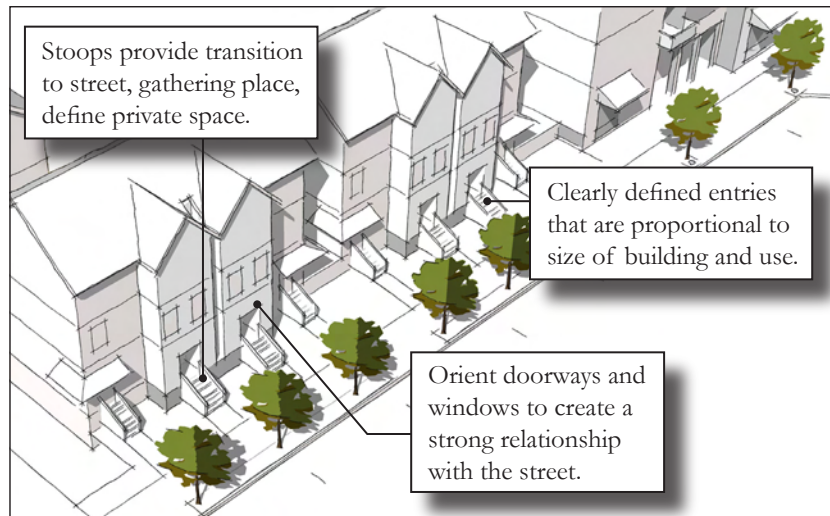


FIGURE 5-29: The street-level frontage should be visually interesting with frequent unit entrances and strong orientation to the street.



FIGURE 5-30: Articulation, setbacks, and materials should minimize massing, break down the scale of buildings, and provide visual interest.

encouraged to have appropriately-scaled, usable gathering spaces at or adjacent to entrances that invite informal social interaction with neighbors.

5.3.2.2 Ground Level Treatment

Residential development may have a finished floor elevation up to 5 feet above sidewalk level to provide more interior privacy for residents. Entry porches or stoops along the street are encouraged to bridge this change in elevation and connect these units to the sidewalk to minimize any physical separation from the street level. The street-level frontage should be visually interesting with frequent unit entrances and clear orientation to the street.

5.3.2.3 Site Access

Curb cuts should be minimized to promote traffic and pedestrian safety and create cohesive landscaping and building façades. A maximum of two curb cuts should be provided for projects requiring 30 parking spaces or more; for projects with less than 30 spaces, only one curb cut should be provided. One-way driveways should have curb cuts with a fully depressed width no greater than 12 feet; two-way curb cuts should be no greater than 22 feet. On-site bicycle parking for residents is encouraged.

5.3.3 ARCHITECTURAL COMPATIBILITY

5.3.3.1 Development Massing

The residential areas within Downtown Burlingame have a range of building heights, and so particular attention must be paid to the massing of new buildings to ensure an appropriate transition with surrounding development. Massing and street façades shall be designed to create a residential scale in keeping with Burlingame neighborhoods.

Articulation, setbacks, and materials should minimize massing, break down the scale of buildings, and provide visual interest.

5.3.3.2 On-Site Structured Parking

Given the density and premium land values Downtown, new projects will likely provide on-site parking in enclosed garage structures, underground, or in “semi-depressed” garages that are partially underground and partially above ground.

Parking should not be allowed to dominate the character of the project. Where enclosed parking is at ground level, it should be fronted or wrapped with habitable uses when possible. If it is not possible to fully wrap the parking, it should be incorporated into the design of the facade. Semi-depressed parking (partly below ground and partly exposed above ground) should be screened with architectural elements that enhance the streetscape such as stoops, porches, or balcony overhangs.

5.3.3.3 Roof Treatment

Interesting and varied roof forms are encouraged. Rooflines should emphasize and accentuate significant elements of the building such as entries, bays, and balconies. Rooftop equipment shall be concealed from view and/or integrated within the architecture of the building.

5.3.4 ARCHITECTURAL DESIGN CONSISTENCY

5.3.4.1 Facade Design

Facades should include projecting eaves and overhangs, porches, and other architectural elements that provide human scale and help break up building mass. All exposed sides of a building should be designed with the same level of care and integrity. Facades should have a variation of both positive space (massing) and negative space (plazas, inset doorways and windows).

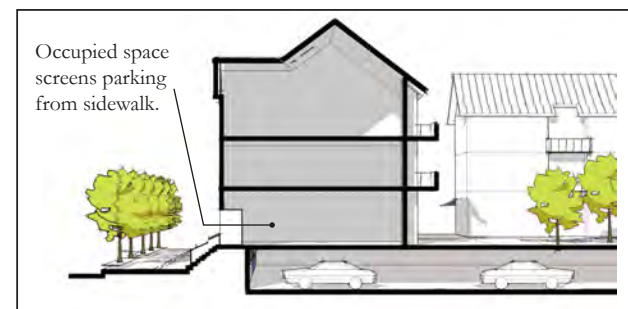
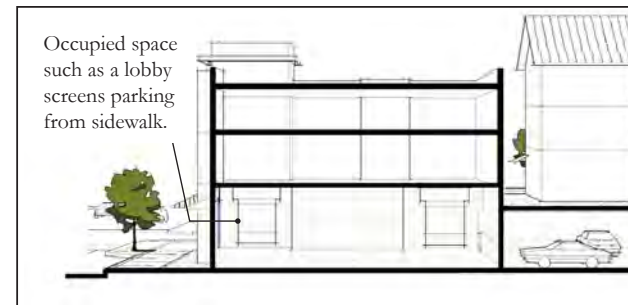


FIGURE 5-31: Where enclosed parking is at ground level, it should be fronted or wrapped with uses that can be occupied such as lobbies and living space when possible.

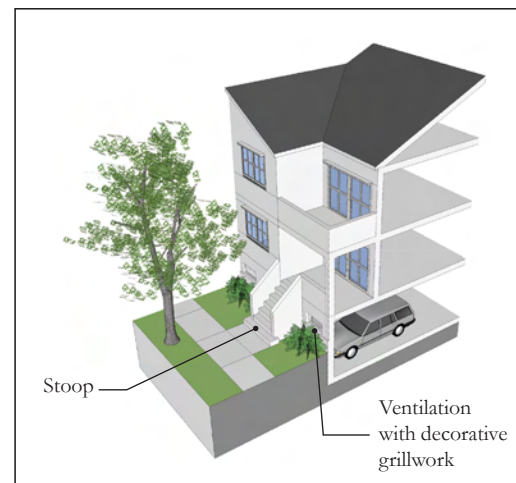


FIGURE 5-32: Semi-depressed parking should be screened with architectural elements that enhance the streetscape such as stoops, porches, or balcony overhangs.

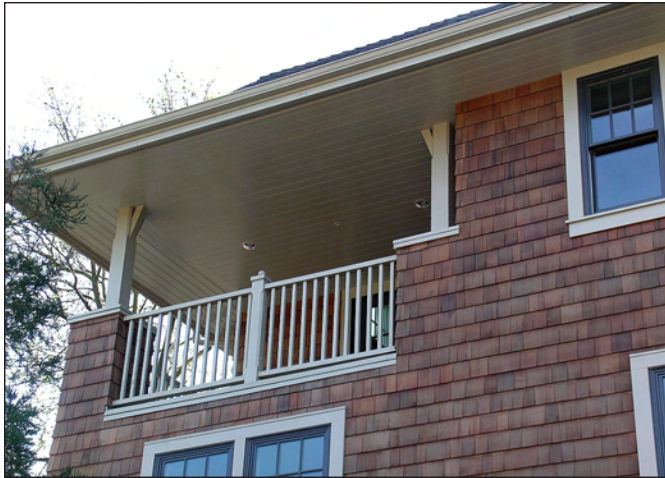


FIGURE 5-33: Residential facades should include projecting eaves and overhangs, porches, and other architectural elements that provide human scale and help break up building mass.



FIGURE 5-34: Windows should be inset generously from the building wall to create shade and shadow detail.

Elements such as entrances, stairs, porches, bays and balconies should be visible to people on the street. Corner parcels are encouraged to incorporate features such as corner entrances, bay windows, and corner roof features, but should avoid monumentally-scaled elements such as towers.

5.3.4.2 Windows

Building walls should be accented by well-proportioned openings that provide relief, detail and variation on the façade. Windows should be inset generously from the building wall to create shade and shadow detail. The use of high-quality window products that contribute to the richness, detail, and depth of the façade is encouraged. Windows with mullions should have individual window lights, rather than applied "snap-in" mullions that lack depth and are not integral to the window structure. Reflective glass is undesirable because of its tendency to create uncomfortable glare conditions and a visual barrier. Where residential uses are adjacent to each other, windows should be placed with regard to any open spaces or windows on neighboring buildings so as to protect the privacy of residents.

5.3.4.3 Materials

Building materials should be richly detailed to provide visual interest. The use of materials that are reflected in the historic architecture present in the neighborhood is encouraged. Metal siding and large expanses of stucco or wood siding are also to be avoided. Roofing materials and accenting features such as canopies, cornices, tile accents, etc. should also offer color variation. Residential building materials should include quality details such as wrought iron, wood-framed windows, wood brackets and tile roofs.

5.3.5 SITE AMENITIES

5.3.5.1 Setbacks

Table 3-2 in Chapter 3 specifies basic building standards such as setbacks and height. Building setbacks are intended to create

a transition between the hardscape, urban environment of the commercial areas and the suburban setting in the surrounding neighborhoods. Setbacks have multiple purposes, including providing sunlight, places for landscaping, and areas for activity and recreation.

Building setbacks should be appropriately landscaped to provide screening and introduce trees and plantings in this area. Landscaped setback areas should be integrated with buildings by providing openings in the building walls that connect the perimeter landscaping with interior courtyards and landscape pathways. Landscaping should be planned in relation to surrounding vegetative types with special consideration being given to native species where possible. Pathways and courtyards should be made of pervious materials to allow groundwater absorption.

5.3.5.2 Open Space

Private on-site open space within the Downtown area is not intended to provide recreational space or large landscaped areas, since this is a more urban environment. However, open space is an important element for residential buildings and should be used to effectively articulate building forms, promote access to light and fresh air, and maintain privacy for Downtown residents. In residential development, most open space should be used to provide attractive amenities for residents, including interior courtyards, outdoor seating options and perimeter landscaping. Balconies and rooftop terraces are encouraged.

Where open space is situated over a structural slab, podium or rooftop it should have a combination of landscaping and high quality paving materials, including elements such as planters, medium-sized trees, and use of textured and/or colored paved surfaces. Planters may be designed to not only accommodate colorful ornamental landscaping, but could also accommodate garden plots for "urban agriculture." Trees should be selected from the City's tree list.



FIGURE 5-35: Where open space is situated over a structural slab, podium or rooftop it should have a combination of landscaping and high quality paving materials, including elements such as planters, mature trees, and urban agriculture.



FIGURE 5-36: Transitions of development intensity from higher density development building types to lower can be done through building types or treatments that are compatible with the lower intensity surrounding uses. Boundaries can be established by providing pedestrian paseos and mews to create separation, rather than walls or fences.

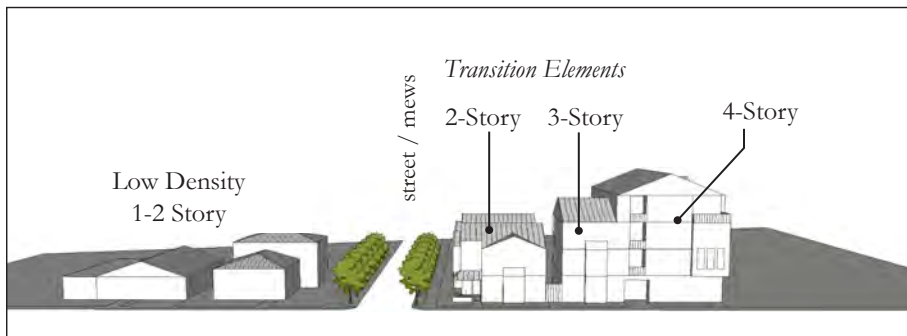


FIGURE 5-37: Transitions can also be made by stepping massing down within a project, with lower building elements providing a buffer between taller elements and adjacent lower-density development.

5.4 ADDITIONAL DESIGN STANDARDS FOR ALL AREAS OF DOWNTOWN

5.4.1 LAND USE TRANSITIONS

Where appropriate, when new projects are built adjacent to existing lower-scale residential development, care shall be taken to respect the scale and privacy of adjacent properties.

5.4.1.1 Massing and Scale Transitions

Transitions of development intensity from higher density development building types to lower can be done through different building sizes or massing treatments that are compatible with the lower intensity surrounding uses. Massing and orientation of new buildings should respect the massing of neighboring structures by varying the massing within a project, stepping back upper stories, reducing mass by composition of solids and voids, and varying sizes of elements to transition to smaller scale buildings.

5.4.1.2 Privacy

Privacy of neighboring structures should be maintained with windows and upper floor balconies positioned so they minimize views into neighboring properties, minimizing sight lines into and from neighboring properties, and limiting sun and shade impacts on abutting properties.

5.4.1.3 Boundaries

Where appropriate, when different land uses or building scales are adjacent, boundaries should be established by providing pedestrian paseos and mews to create separation, rather than walls or fences.

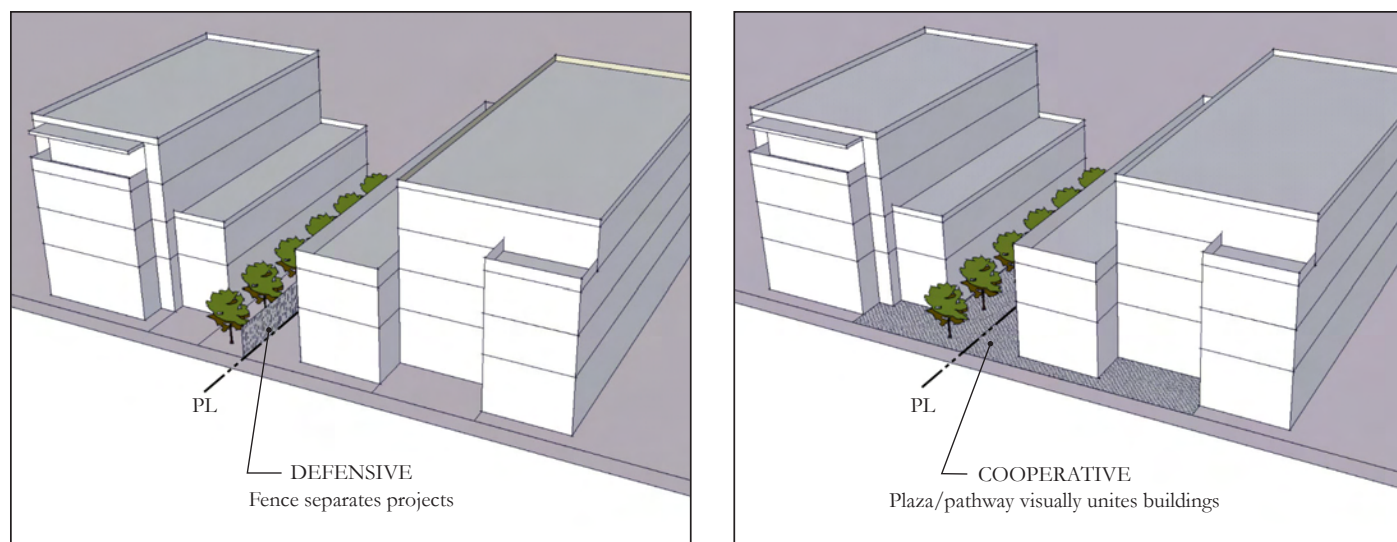


FIGURE 5-38: Following a cooperative, rather than defensive design approach for the spaces between buildings results in a more coherent downtown feel, as opposed to a collection of unrelated projects.



FIGURE 5-39: Example of two different land use intensities joined with a common paseo pathway.

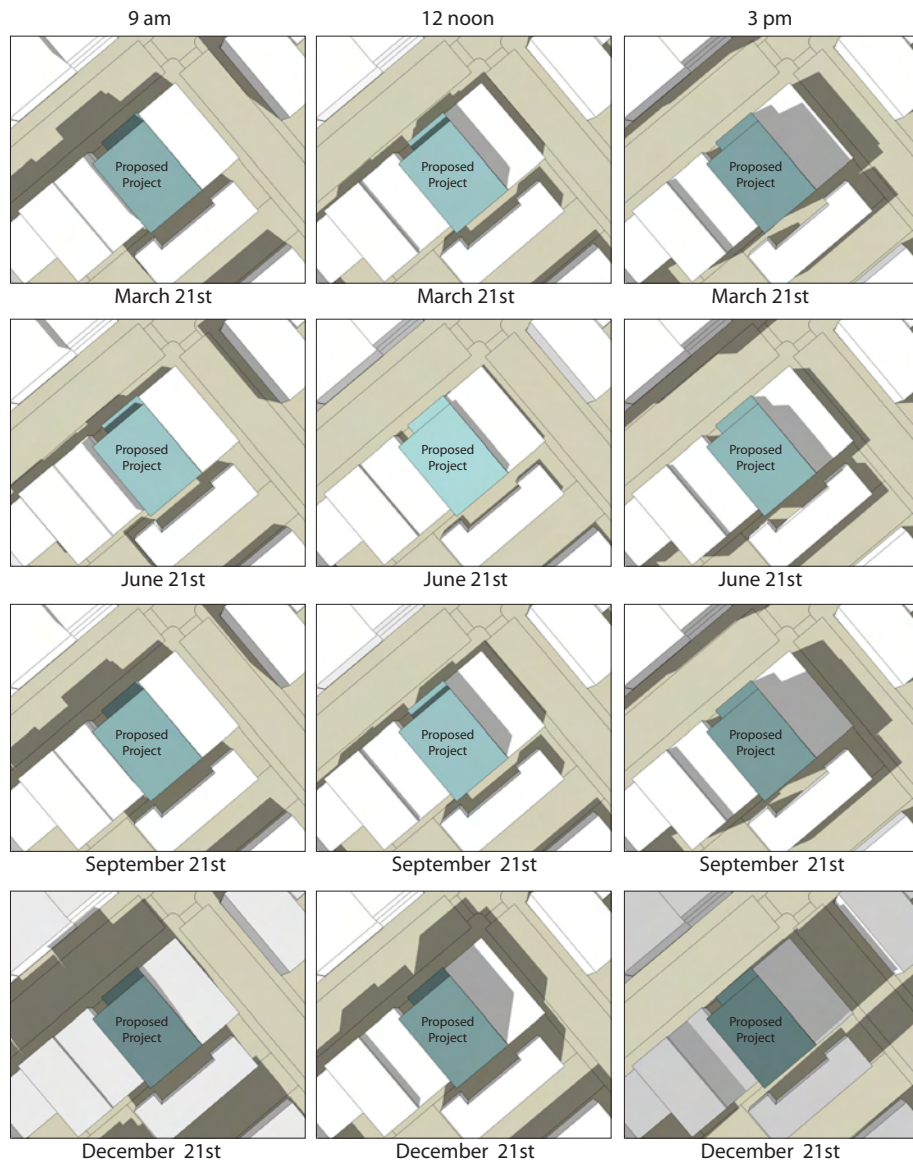


FIGURE 5-40: Sample shadow analysis shows the range of shading conditions through the year.

5.4.2 SHADOW IMPACTS

Every building invariably casts some shadows on adjoining parcels, public streets, and/or open spaces. However, as the design of a project is developed, consideration should be given to the potential shading impacts on surroundings. Site plans, massing, and building design should respond to potential shading issues, minimizing shading impacts where they would be undesirable, or conversely maximizing shading where it is desired.

As part of the design review process, development in the Specific Plan Area that is proposed to be taller than existing surrounding structures should be evaluated for potential to create new shadows/shade on public and/or quasi-public open spaces and major pedestrian routes. At a minimum, shadow diagrams should be prepared for 9 AM, 12 noon, and 3 PM on March 21st, June 21st, September 21st, and December 21st (approximately corresponding to the solstices and equinoxes) to identify extreme conditions and trends. If warranted, diagrams could also be prepared for key dates or times of day — for example, whether a sidewalk or public space would be shaded at lunchtime during warmer months.

5.4.3 SUSTAINABILITY AND GREEN BUILDING DESIGN

Project design and materials to achieve sustainability and green building design should be incorporated into projects. Green building design considers the environment during design and construction and aims for compatibility with the local environment: to protect, respect and benefit from it. In general, sustainable buildings are energy efficient, water conserving, durable and nontoxic, with high-quality spaces and high recycled content materials. The following considerations should be included in site and building design:

- Resilient, durable, sustainable materials and finishes.
- Flexibility over time, to allow for re-use and adaptation.
- Optimize building orientation for heat gain, shading, daylighting, and natural ventilation.
- Design landscaping to create comfortable micro-climates and reduce heat island effects.
- Design for easy pedestrian, bicycle, and transit access, and provide on-site bicycle parking.
- Maximize on-site stormwater management through landscaping and permeable pavement.
- On flat roofs, utilize cool/white roofs to minimize heat gain.
- Design lighting, plumbing, and equipment for efficient energy use.
- Create healthy indoor environments.
- Pursue adaptive re-use of an existing building or portion of a building as an alternative to demolition and rebuilding.
- Use creativity and innovation to build more sustainable environments. One example is establishing gardens with edible fruits, vegetables or other plants as part of project open space, or providing garden plots to residents for urban agriculture.

To reduce carbon footprint, new projects are encouraged to follow the standards and guidelines of the Leadership in Energy and Environmental Design (LEED) Green Building Rating System, developed by the U.S. Green Building Council (USGBC), and pursue LEED certification if appropriate.

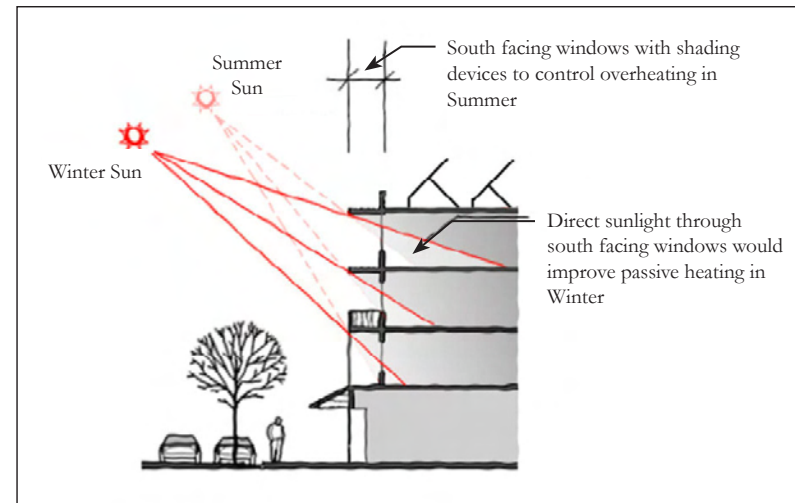


FIGURE 5-41: Use of shading devices to control solar loads in summer and gain passive heat in winter.



FIGURE 5-42: Minimize stormwater runoff to impermeable areas with landscaping, green roofs, and rain gardens when possible.



FIGURE 5-43: Consistent with Burlingame's status as "Tree City USA," new projects are required to incorporate trees into landscape and private open space plans.

5.4.4 LANDSCAPE TREES

The City of Burlingame has a long history of proactive tree planting and proper tree care. From the late 1800's when trees were planted along El Camino Real and Easton Drive to the current day, Burlingame has enjoyed the many benefits trees provide to an urban area. Burlingame's longtime commitment to trees is evidenced by recognition as a "Tree City USA" for 30 consecutive years. This is the longest streak in the County, 5th longest in the State and one of the longest in the Country for receiving this award.

In Downtown Burlingame, trees include street trees lining sidewalks and roadways (typically within the public right-of-way), as well as trees on private property in settings such as landscaped setback areas, courtyards, and roof gardens.

Chapter 4: Streetscapes & Open Space) provides guidance for street trees within the public right-of-way. Landscape trees on private property have equal importance as part of the "urban forest," in contributing environmental and aesthetic benefits to downtown. Trees are important for their beauty, shade and coolness, economic benefits, and role in reducing energy use, pollution, and noise.

The City of Burlingame has an Urban Forest Management Plan that includes policies and management practices for both city and private trees. Maintaining existing trees is a priority, and large trees on private property are protected by City Ordinance. Any tree with a circumference of 48 inches or more when measured 54 inches above the ground is a "Protected Tree." A permit is required to remove or heavily prune a protected tree.

Consistent with Burlingame's status as "Tree City USA," new projects are required to incorporate trees into landscape and private open space plans. Property owners should consult the Burlingame Urban Forest Management Plan for design considerations, planting techniques, and maintenance guidance.

5.4.5 PRESERVATION OF HISTORIC BUILDINGS

Downtown Burlingame is the symbolic and historic center of the City. The vision for Downtown is to preserve the mix of buildings, the pedestrian-scaled environment and the carefully designed public spaces that contribute to its special community character. Downtown's flexible and timeless late 19th and early 20th Century buildings contribute historic character and distinctiveness to this desirable pattern and mix of buildings. New buildings should be sensitive to the historic scale and architecture of Downtown.

Historic preservation and adaptive re-use is encouraged both to maintain the unique ambience of Downtown Burlingame but also for ecological benefits. Preservation maximizes the use of existing materials and infrastructure, reduces waste, and preserves historic character. Historic buildings were often traditionally designed with many sustainable features that responded to climate and site, and when effectively restored and reused, these features can bring about substantial energy savings.

The guidelines in this chapter, together with the *Commercial Design Guidebook* for commercial and mixed use developments and the *Inventory of Historic Resources* are intended to ensure that both new development and improvements to existing properties are compatible with the historical character of Downtown and will be the basis of design review.

Where a building is described in the *Inventory of Historic Resources*, the inventory should be consulted as part of the design review. Building characteristics described in the inventory should be a consideration in project design and review, together with other design considerations described in this chapter and in the *Commercial Design Guidebook*.



FIGURE 5-44: Downtown's late 19th and early 20th Century buildings contribute historic character and distinctiveness to this desirable pattern and mix of buildings.

