



Burlingame Broadway Specific Plan

Community Workshop #1 | January 17, 2024

I. Welcome and Introductions

II. Project Updates

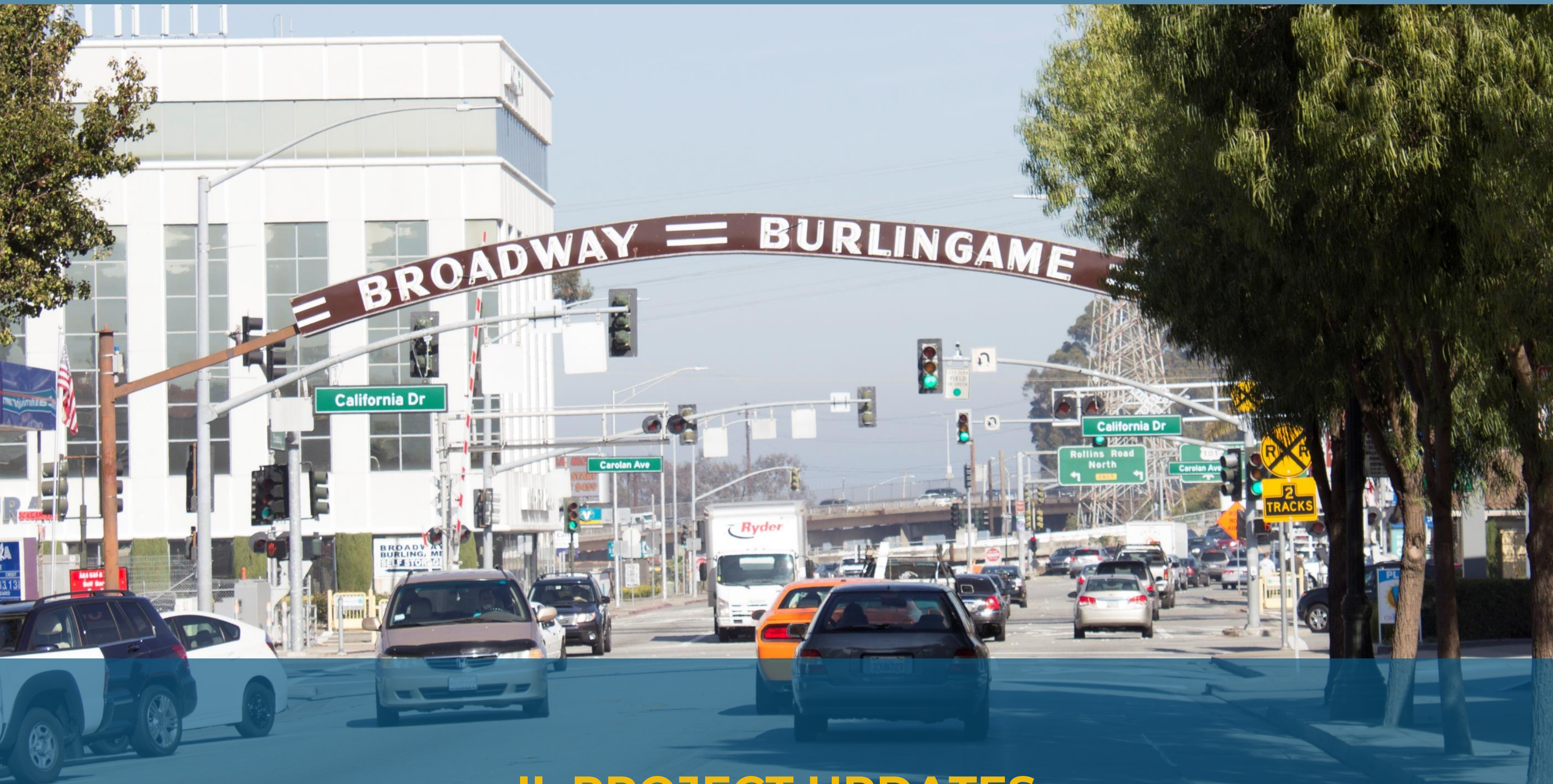
- TAC and CAC Meetings #1 Feedback
- Project Schedule and Deliverables

III. Draft Existing Conditions Analysis

IV. Group Discussion: Emerging Vision Elements

V. Group Discussion: Emerging Design Concepts

VI. Next Steps



II. PROJECT UPDATES

POLICY



INFRASTRUCTURE

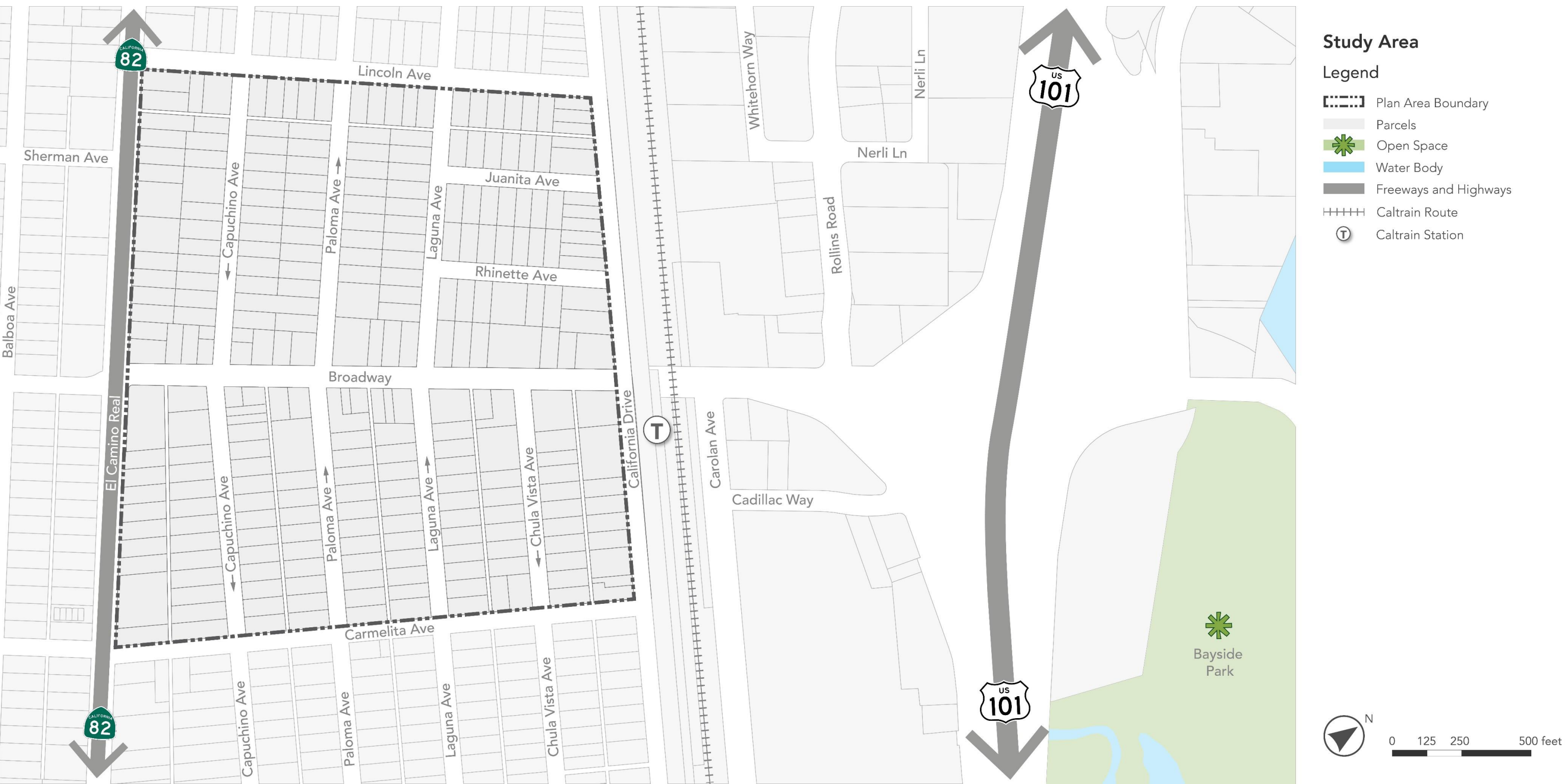


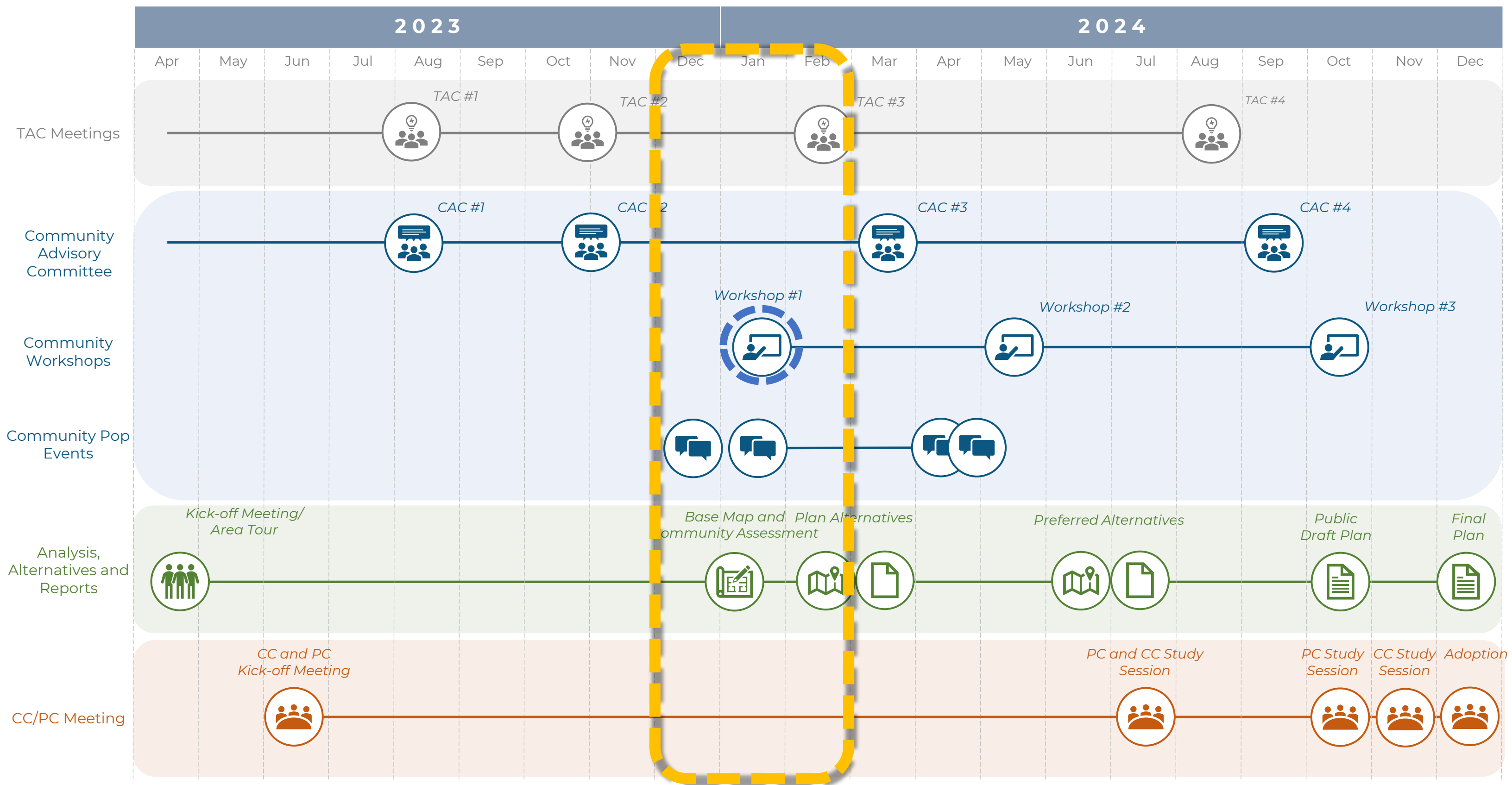
MOBILITY



1. Encourage Transit-Oriented Development
2. Promote Affordable Housing
3. Evaluate Parking Exemptions
4. Support Economic Development
5. Study Historic Preservation
6. Align with General Plan Policies
7. Improve Safety
8. Expand Bicycle and Pedestrian Connections
9. Increase Transit Access
10. Improve Regional Connections







Broadway Assets

- Historic role of Broadway as the “Heart of Burlingame”
- Locally-owned, small businesses
- Adjacency to the new Caltrain Station
- Gade Separation project
- Unique character
- Pedestrian scale

Challenges/Opportunities

- Broadway is constrained due to its narrow right-of-way
- Parklets and outdoor restaurants conflict with existing vehicle movements
- Signs are not very visible
- Economic Development
Balance the need for new and more intense residential development with parking, mobility, and other needs along Broadway.
- Housing
Support higher-density affordable housing, identify ways to create workforce housing that supports the needs of residents and local workers, and incentivize workforce housing.
- Streetscape Improvements
Create more pedestrian pathways, add more landscaping, promote sustainability, reduce maintenance, consider movable bollards to create a safe space for weekly activities like farmer’s market, and identify new strategies for supporting outdoor seating and dining.
- Pedestrian and Bicycle Improvements
Improve the street so it can become more multi-modal and support a range of users, make sidewalks more vibrant and livelier, and consider bike lanes along Broadway.

Broadway Assets

- Local businesses
- Proximity to the San Francisco Airport and ideal location to attract tourists
- Underutilized alleyways and mews
- Pedestrian scale

Challenges/Opportunities

- **Pedestrian Safety**
Create wider sidewalks to encourage more people to walk and explore, and improve crosswalks and intersections.
- **Vehicle Speeds and Movements**
Introduce better signs and wayfinding, lower speed limits, install, and flashing lights/mid-block crossings.
- **Parking**
Develop parking in-lieu standards for businesses, limit parking times in front of active ground uses, consider a parking structure, and expand parking hours on lots.
- **Bicycle Facilities**
Introduce bike lanes because currently bikers use the sidewalks due to high-speed traffic on Broadway.
- **Landscape Improvements**
Prepare a landscape and planting plan, develop pocket parks along sidewalk corners/vacant public lands, and accommodate public outdoor seating.
- **Active Ground Floor Uses**
Improve visibility of storefronts, attract a variety of business, limit non-retail businesses like offices and showrooms on the ground floor, increase the vibrancy along Broadway.
- **Community Events**
Create space for weekly community activities and support cultural events that connect people with their neighbors.



III. DRAFT EXISTING CONDITIONS ANALYSIS



Economic Analysis

Equity Assessment
Affordable Housing
Assessment
Jobs Assessment



Physical Analysis

Historic Resources
Assessment
Urban Design Analysis
Facilities Analysis



Mobility Analysis

Trip Patterns
Vehicle Miles Traveled
Safety
Pedestrian and Bicycle
Access and Circulation
Carshare Access
Transit
Parking and Curb Use
Local Policies and
Constraints

Most housing units in the Study Area are in **two-to-nine-unit multifamily** buildings (approximately 81 percent).

The housing stock in the Study Area is **much older** than the housing stock in the City and San Mateo County.

There have been few new units built in the Study Area since 1980 – units built after 1979 make up just **5.5 percent** of the housing inventory in the Study Area.



The multifamily rental inventories in the Study Area and City of Burlingame are **heavily dominated by studios and one-bedroom units** (87 percent of the multifamily rental units).

According to data from CoStar, just three percent of multifamily rental units in the Study Area have three or more bedrooms.



Single-family homes and condominiums are **generally much more expensive** in Burlingame than in San Mateo County.

In 2022, the median sale price of homes sold in Burlingame was approximately \$2.9 million, compared to just over \$2.0 million in San Mateo County.

The median sale price for condominiums was \$1.1 million in Burlingame and \$980,000 in San Mateo County.



The Study Area supports an **active commercial sector** comprised primarily of small and independently owned businesses:

- Restaurants and Food Services (31)
- Retail (26)
- Other Services (39)

Combined, the 96 businesses employ an estimated **529 workers**, or roughly 82 percent of all workers in the Study Area.



Broadway has been a commercial district for Burlingame for **more than 100 years**.

Many commercial buildings were **constructed in the 1920s** in Mission Revival or Classical Revival styles.

Some buildings within the study area **could be individually eligible** for the California or National Registers (due to events, activities, or architectural styles).

One **known historic resource** is located at the southern end of the study area.

29 **potential historic resources** are located along Broadway within the commercial area.

7 **non-historic resources** are located within the commercial area.



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Concentrations of collisions occur on Broadway, California Drive and El Camino Real.

Broadside, rear-end and sideswipe are the most common type of collisions observed within the study area.

Automobile right-of-way, improper turning and unsafe speed are most common violation types.

A fatal crash happened at the intersection of Broadway and El Camino Real.



Collisions in the Broadway Study Area

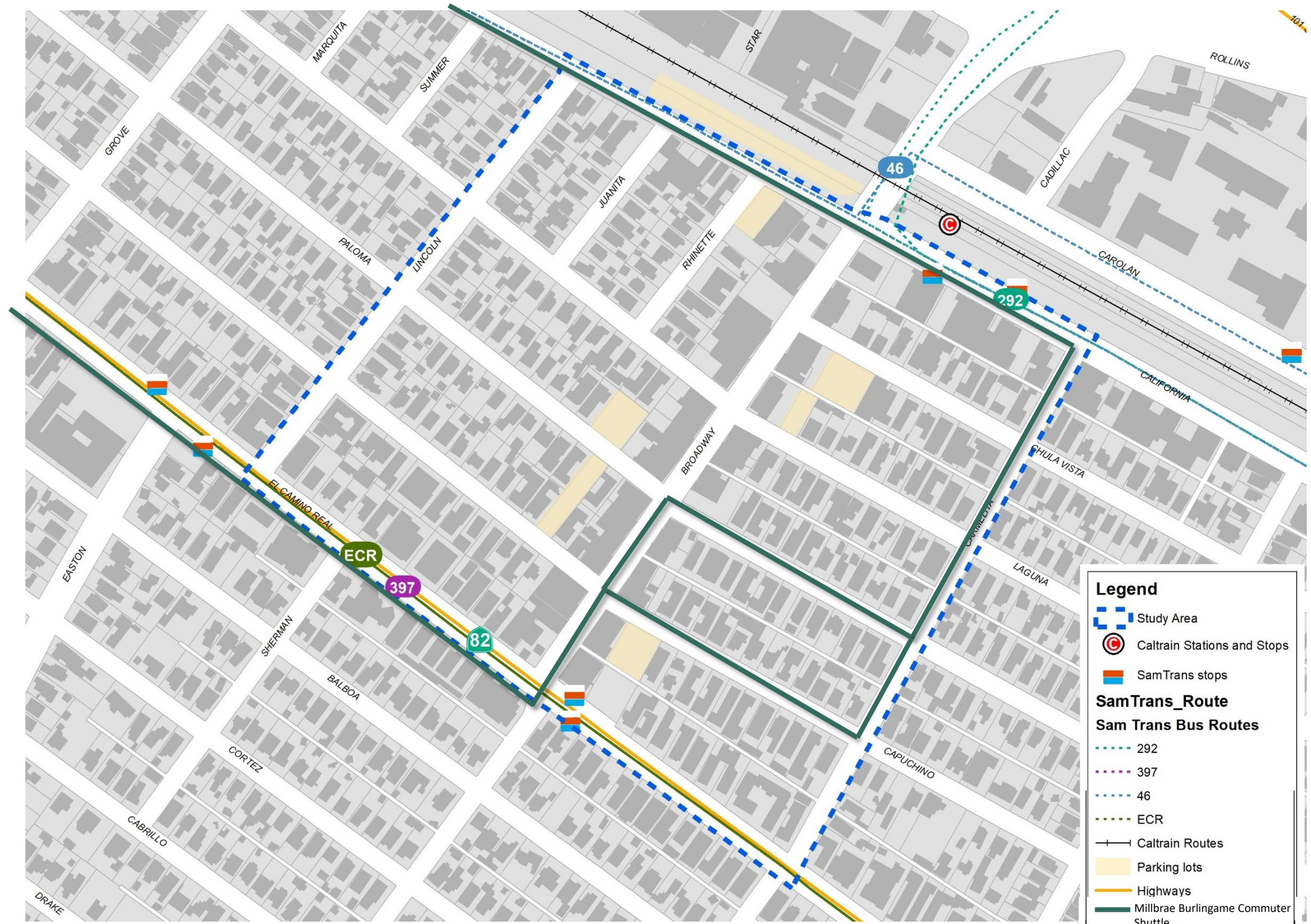
SamTrans Stops

(Bus Routes)

- Route 292
- Route 397
- Route 46

CalTrain Station and Stops

- Millbrae Burlingame Commuter Shuttle



Existing Transit Network

The study area contains sidewalks on all streets, lined with trees.

The business district along Broadway has **pedestrian safety enhancements** including curb bulb outs and crosswalks at intersection.

Narrow pedestrian zone in front of the shops and cafe.

Use of **textured pavement** and different materials to demarcate pedestrian zones.



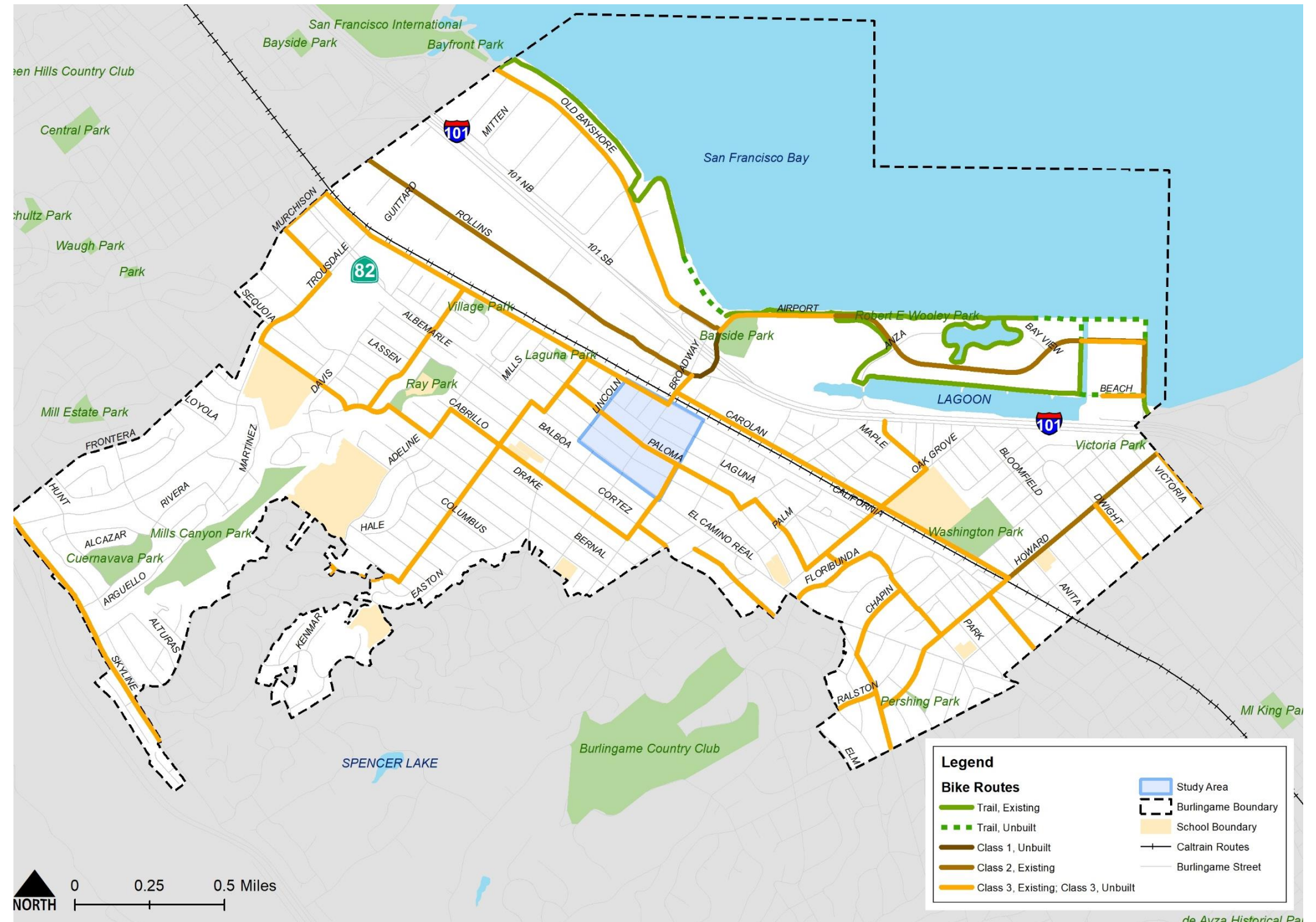
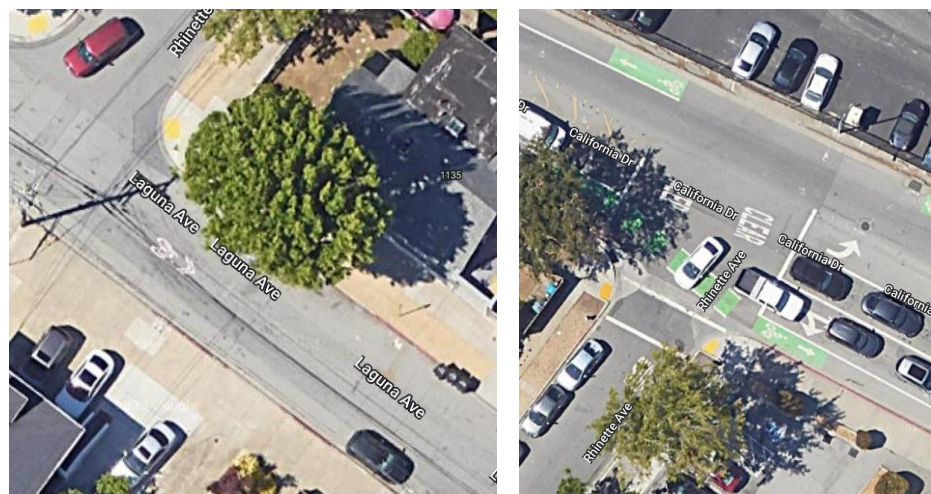
The Study Area includes:

- Class I bicycle lane on Lincoln Ave, Laguna Ave
- Class IV bicycle lane on California Drive

The existing bikeways provide **limited connectivity** to transit.

Class levels **insufficient** to induce mode shift.

Conflict points occur between bikeways and moving/parking vehicles.



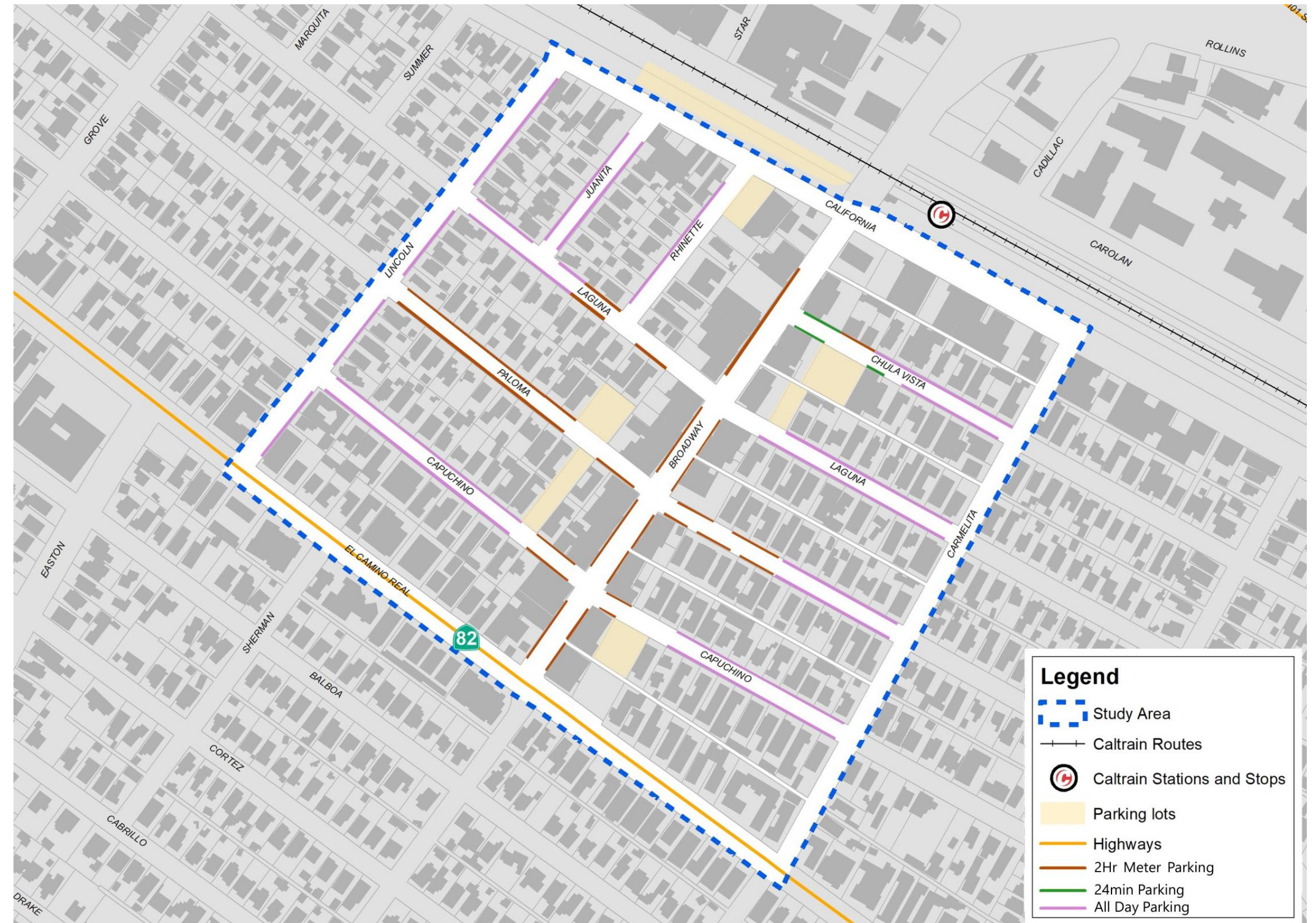
Existing Bicycle Network

Street Parking

Street parking is available throughout the Study Area, including on Broadway. On-street parking includes 24-minutes meters, 2-hour meters and All-Day parking areas.

Parking Lots

Surface lots use valuable real estate, and many existing parking lot are largely unused.



Incorporate **routine accommodation for pedestrian and bicycle** facilities when roadway or other construction work disrupts normal uses of sidewalks, trails, and on-street bicycle facilities. *(Policy 9.1)*

Create vibrant and inviting **places to walk and bike** that include street furniture, wayfinding, lighting, and place making elements (including public art) *(Goal 6)*

Develop and implement an appropriate minimum paving surface standard for bicycle boulevards *(Policy 10.3)*

Prioritize **walking and bicycle improvements** near schools, parks, Caltrain, commercial areas, and other community destinations *(Goal 6)*

Routinely evaluate locations for **enhancing crosswalks**. *(Policy 4.5)*

Develop/improve a consistent **citywide wayfinding program** focused on signage for active transportation users. *(Policy 5.1)*

Install **leading pedestrian interval phases** in traffic signal timing, as warranted, to encourage walking and facilitate crossing busy streets. *(Policy 4.3)*

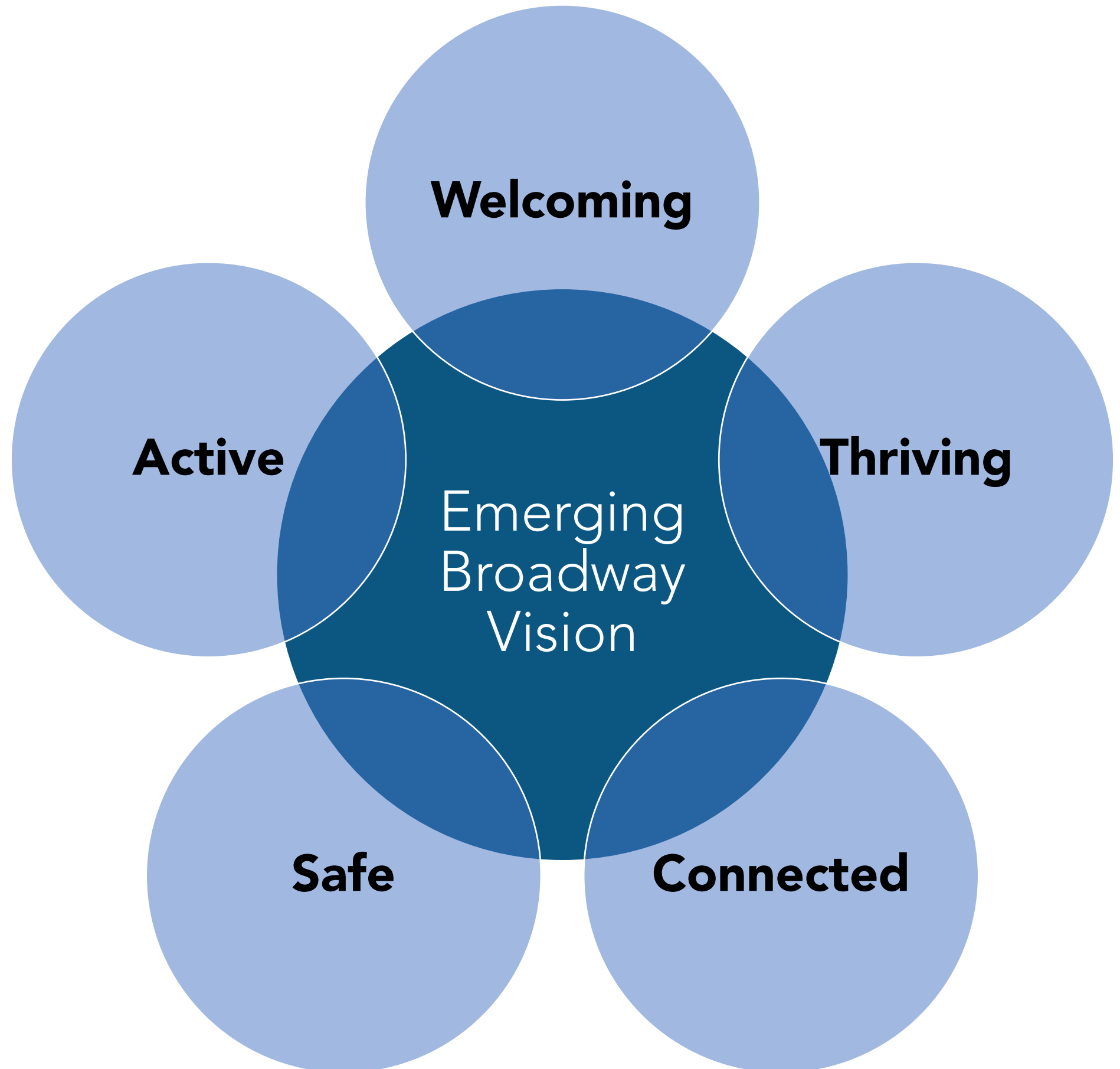
Identify and **construct sidewalks** in areas where they are incomplete *(Policy 4.2)*



IV. EMERGING VISION ELEMENTS

The **Emerging Vision Elements** build upon a foundation of TAC and CAC comments and ideas received to-date, and the existing conditions analysis.

They begin to frame **common themes** and **desires** for what is needed to create a successful Broadway area in the years and decades to come ...



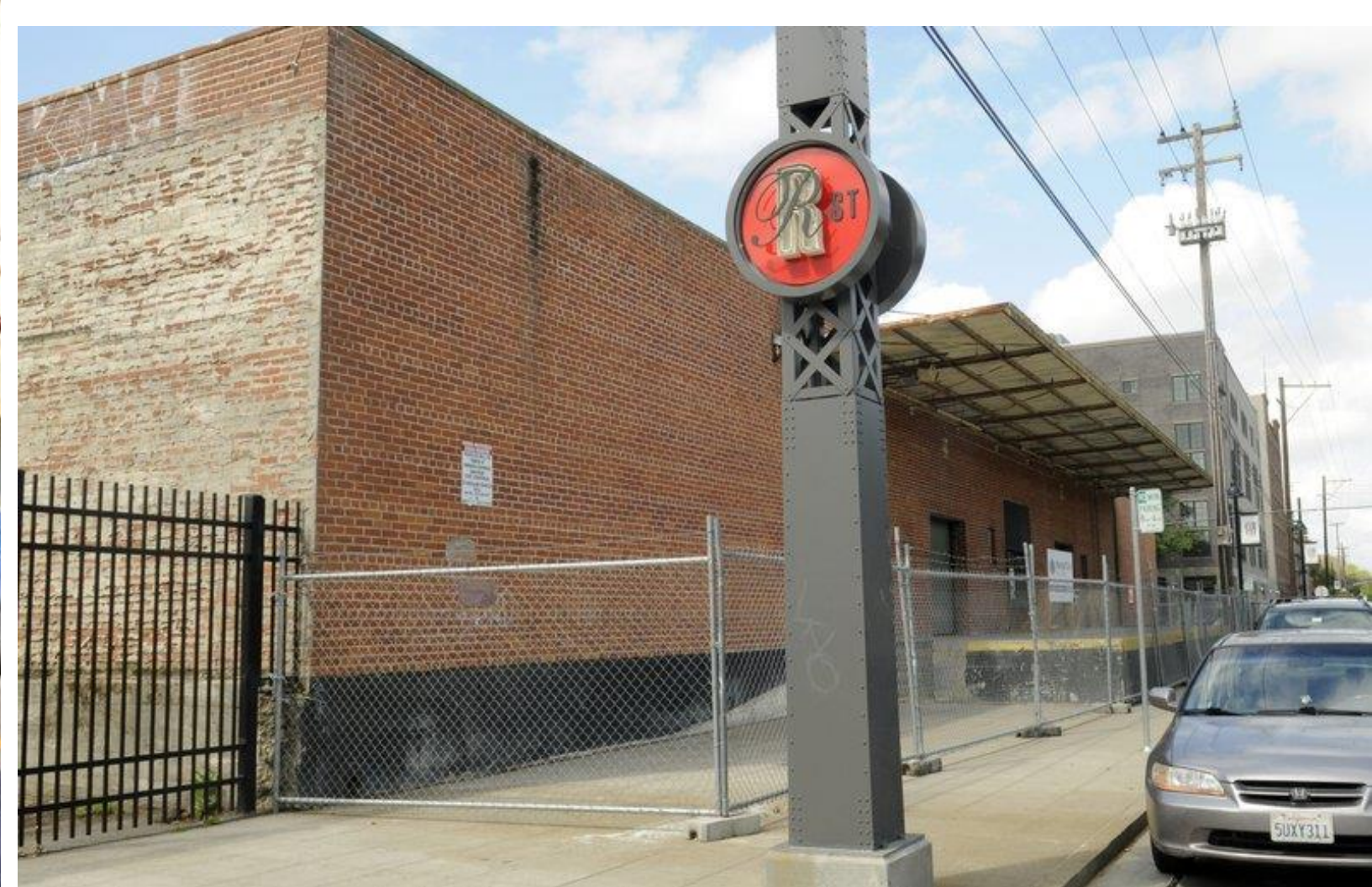




Legend:

Parcels Buildings Major Roads Project Study Area



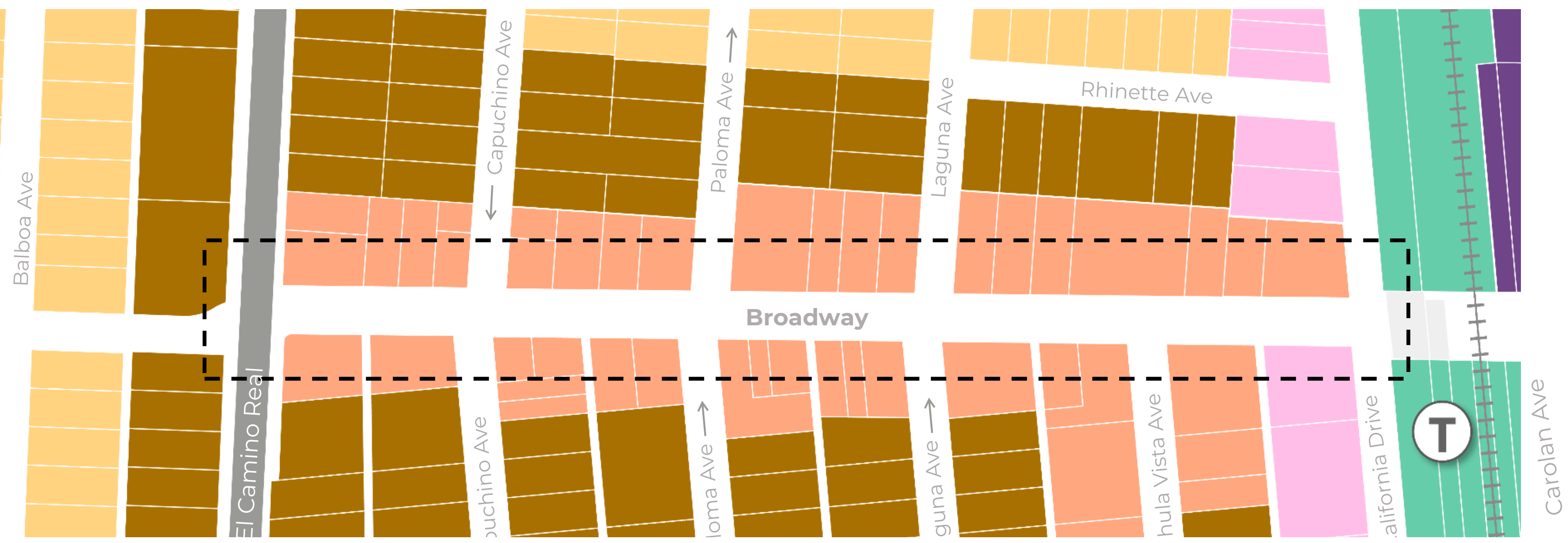




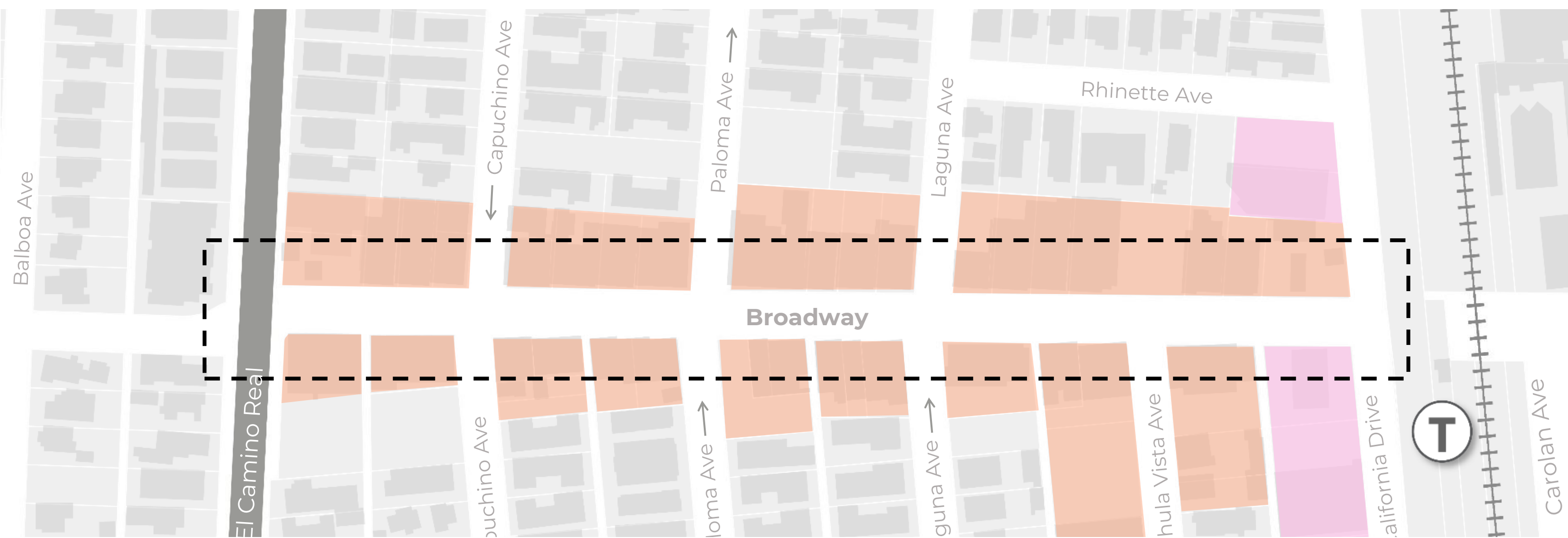
entry monument vehicular directional pedestrian wayfinding public parking street banner identification







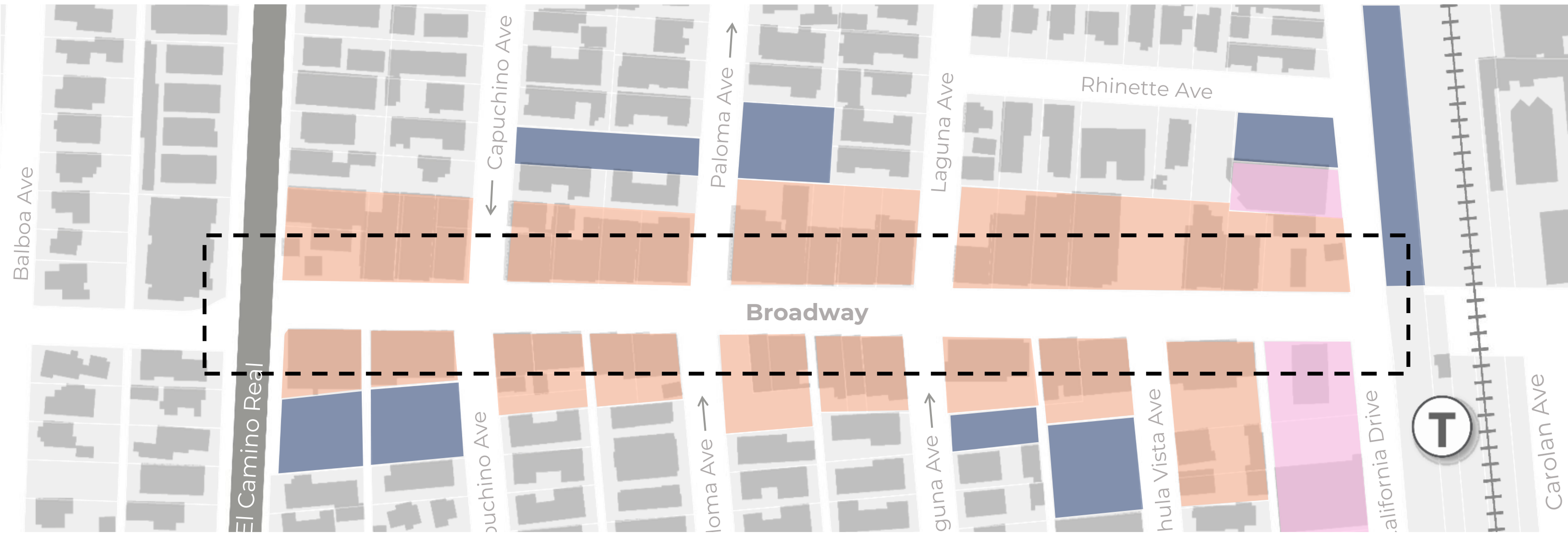
Legend: Major Roads, R1 – Low Density Residential, R3 – Medium/High Density Resi, BRMU – Broadway Mixed Use, I/I – Innovation Industrial, Parcels, Buildings, Project Study Area, CMU – California Drive Mixed Use, California Drive Mixed Use



Legend:

	Parcels		Buildings		Major Roads		Project Study Area		Broadway Mixed Use		California Drive Mixed Use
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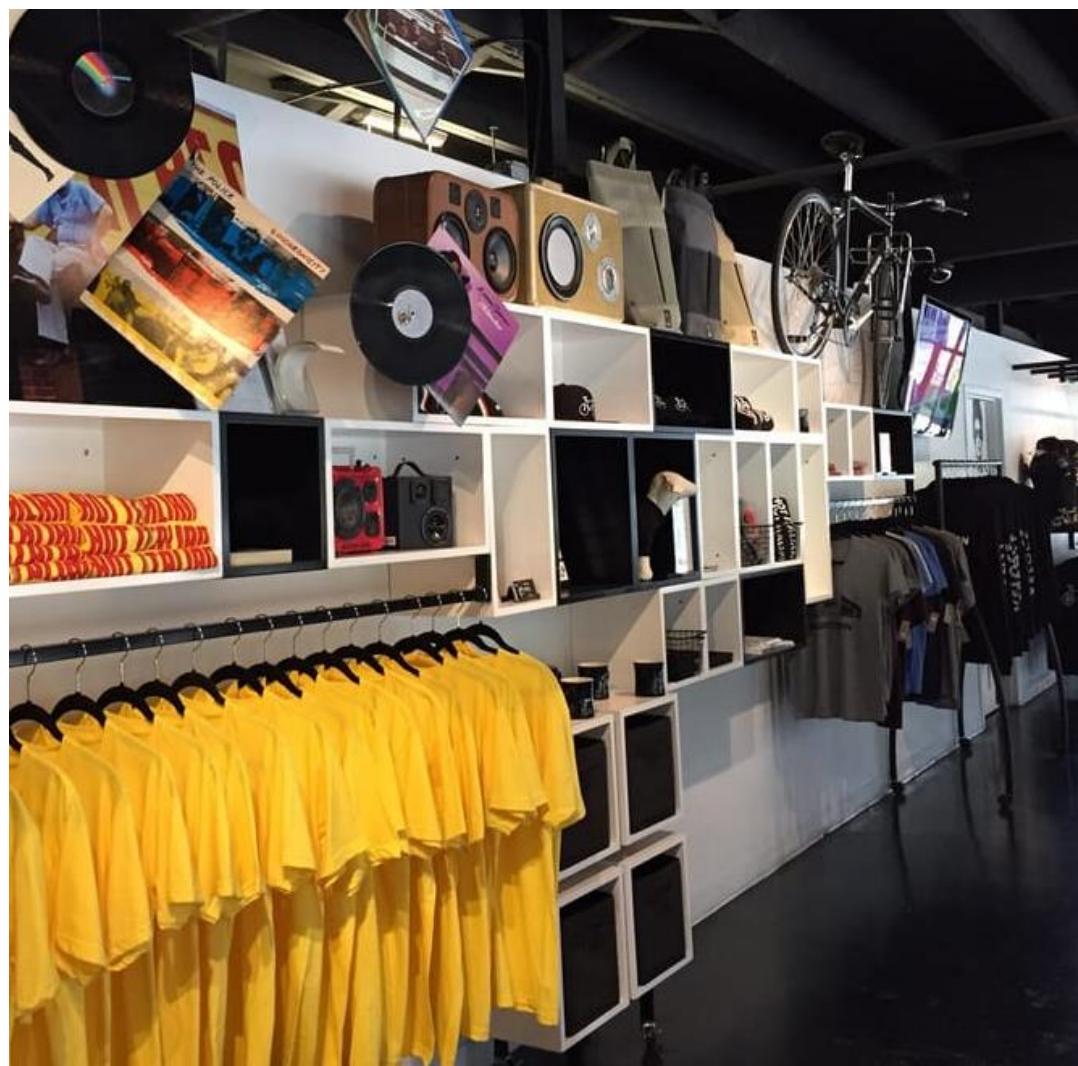
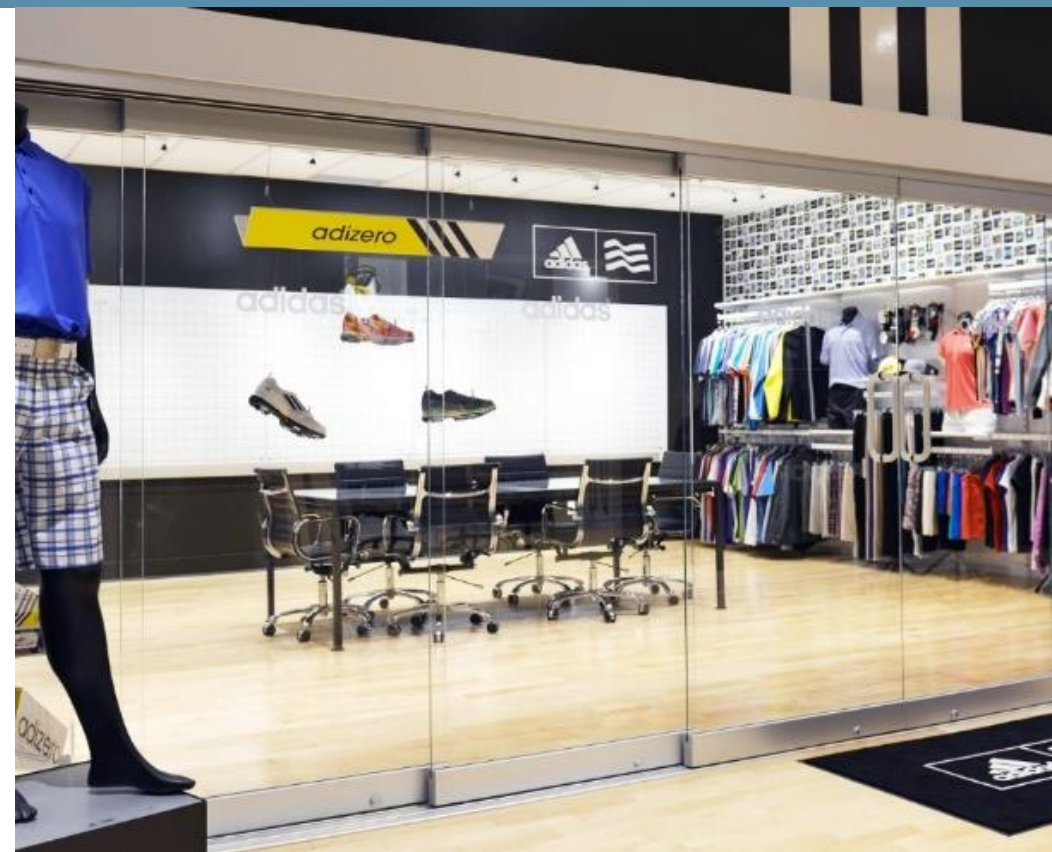




Legend:

Parcels	Buildings	Major Roads	Project Study Area	Broadway Mixed Use	California Drive Mixed Use	Parking Lots
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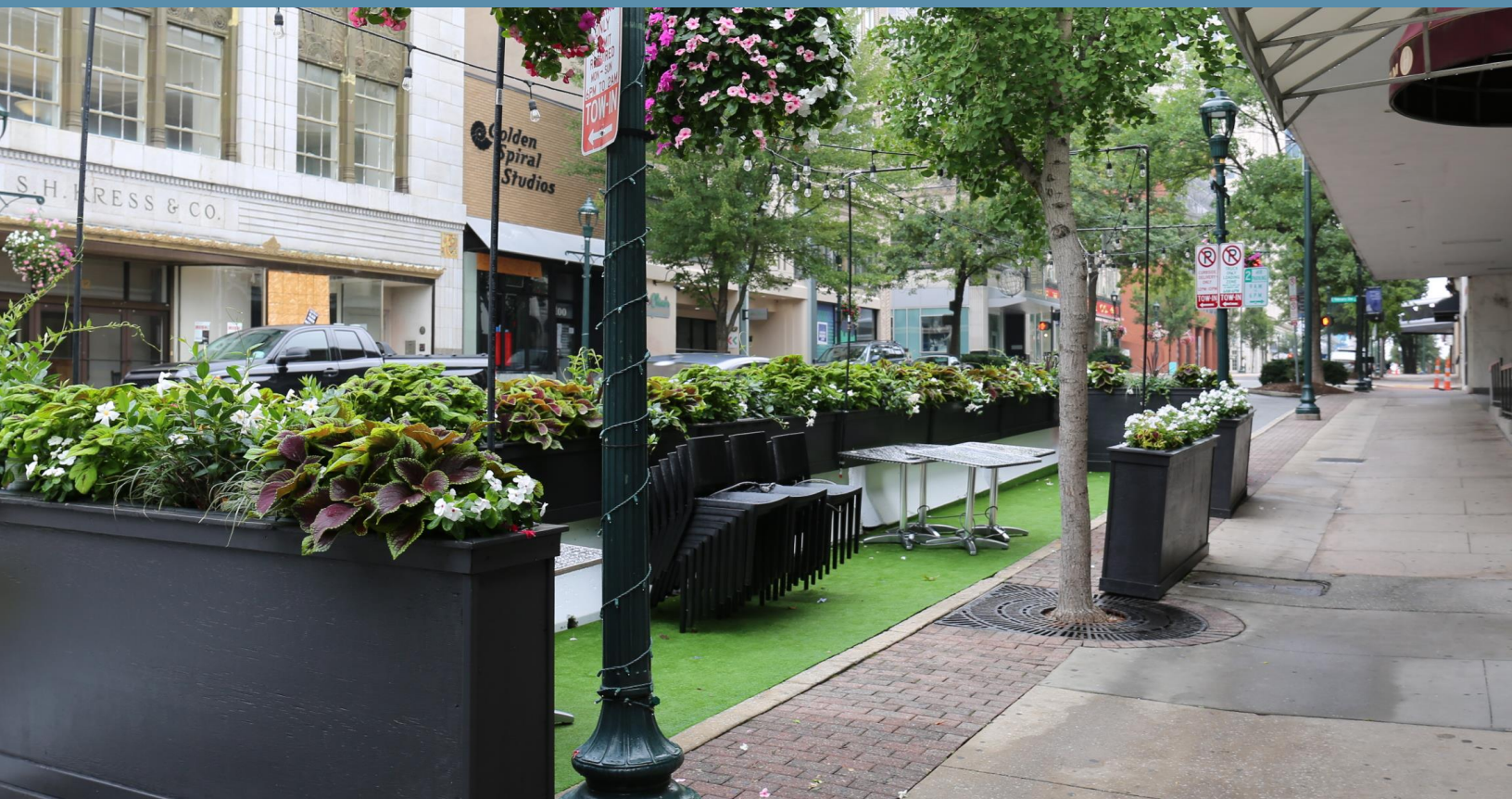


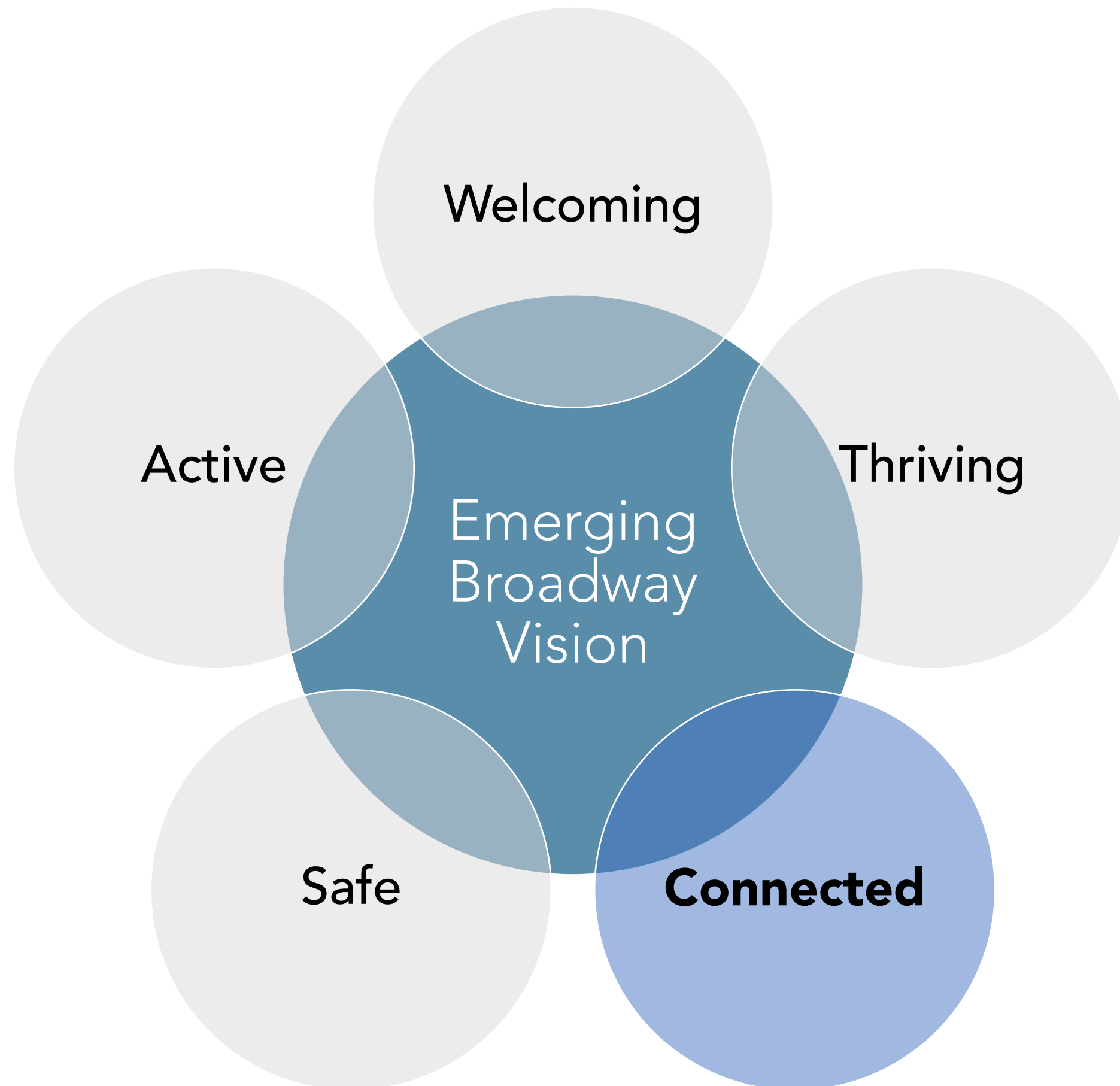


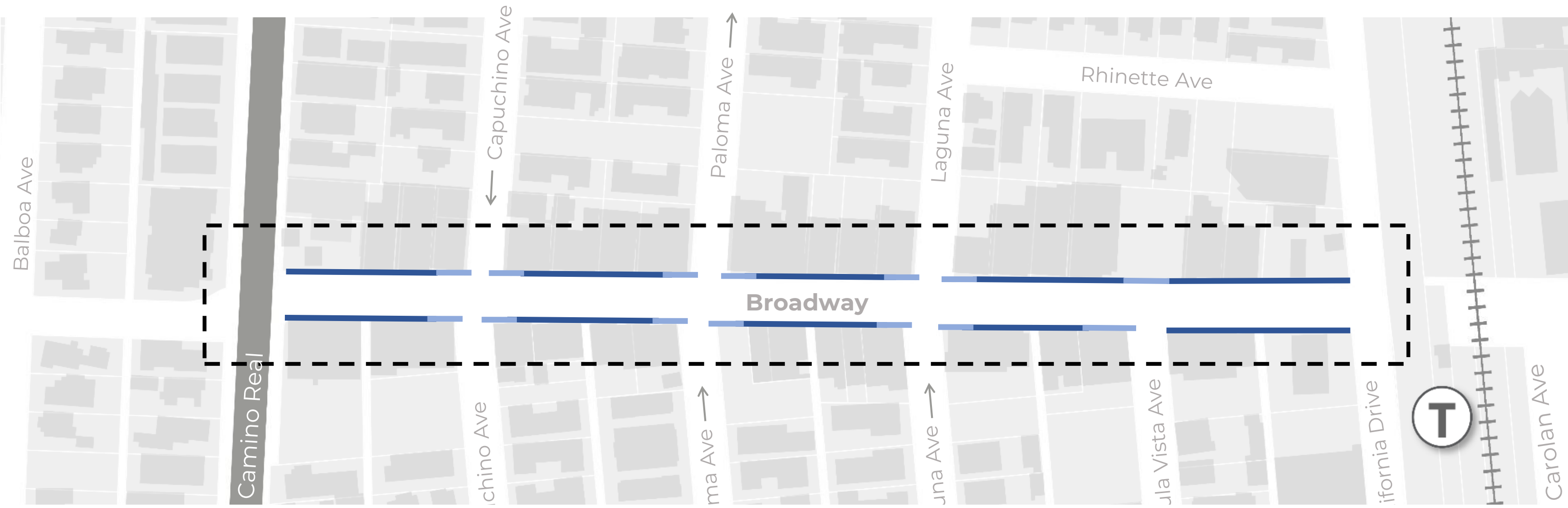






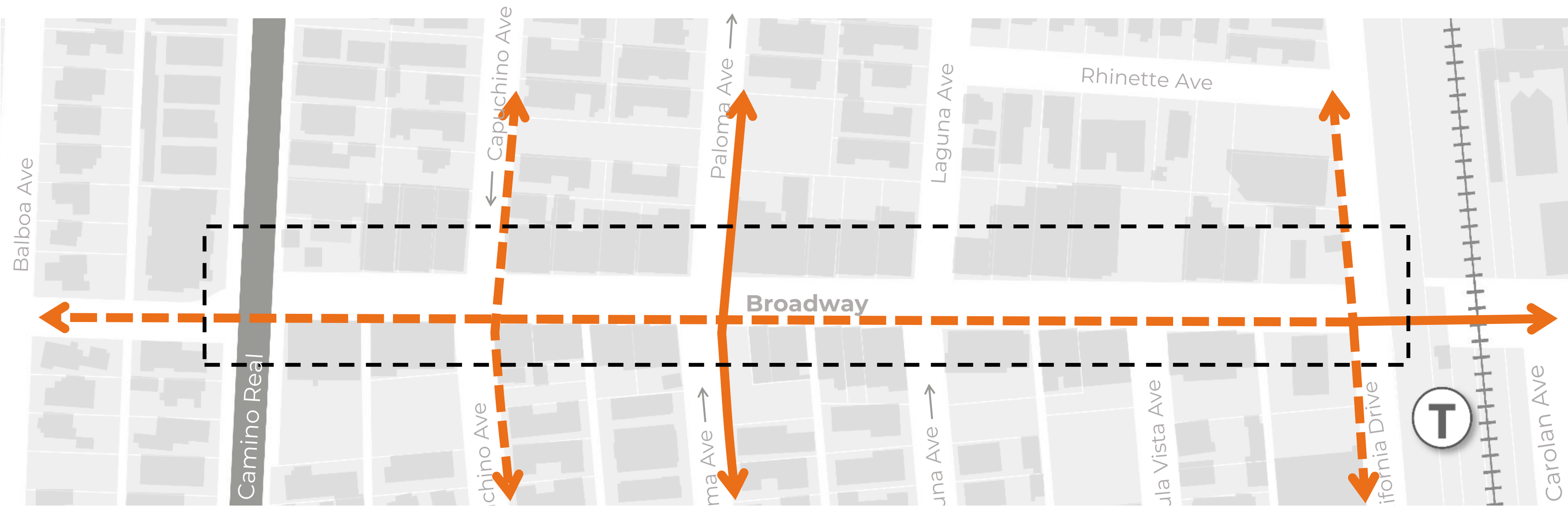






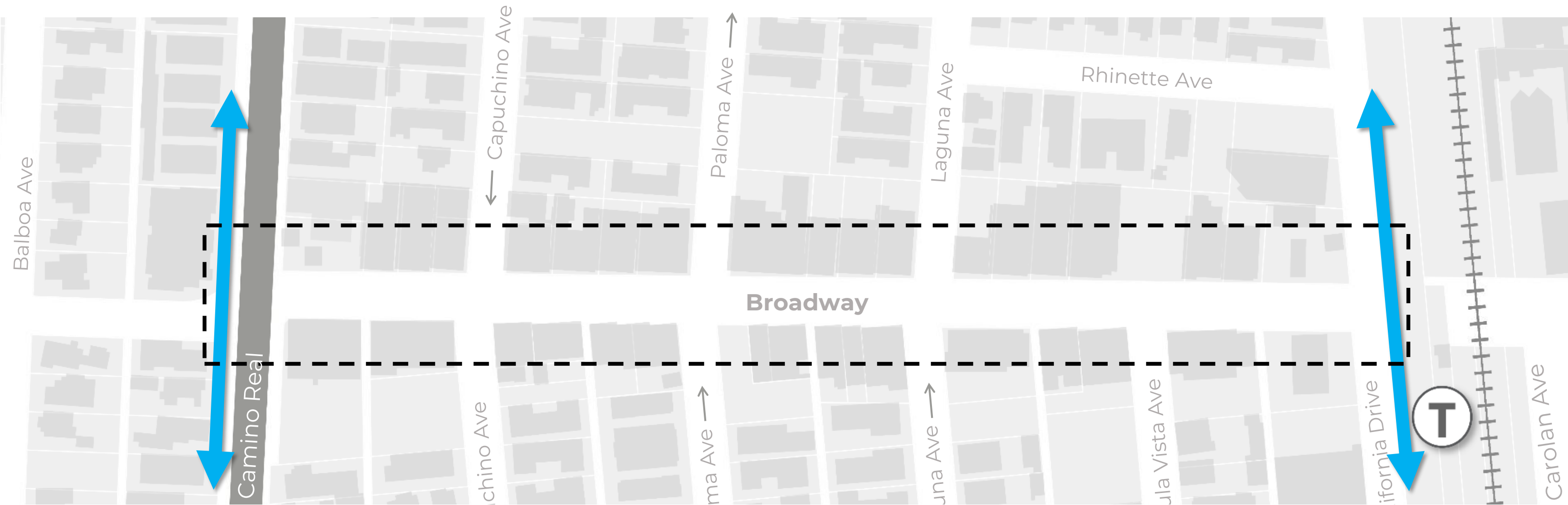
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- Parcels
 - Buildings
 - Major Roads
 - Project Study Area
 - Caltrain
 - Caltrain Stop
 - Wider Sidewalk
 - Narrow Sidewalk



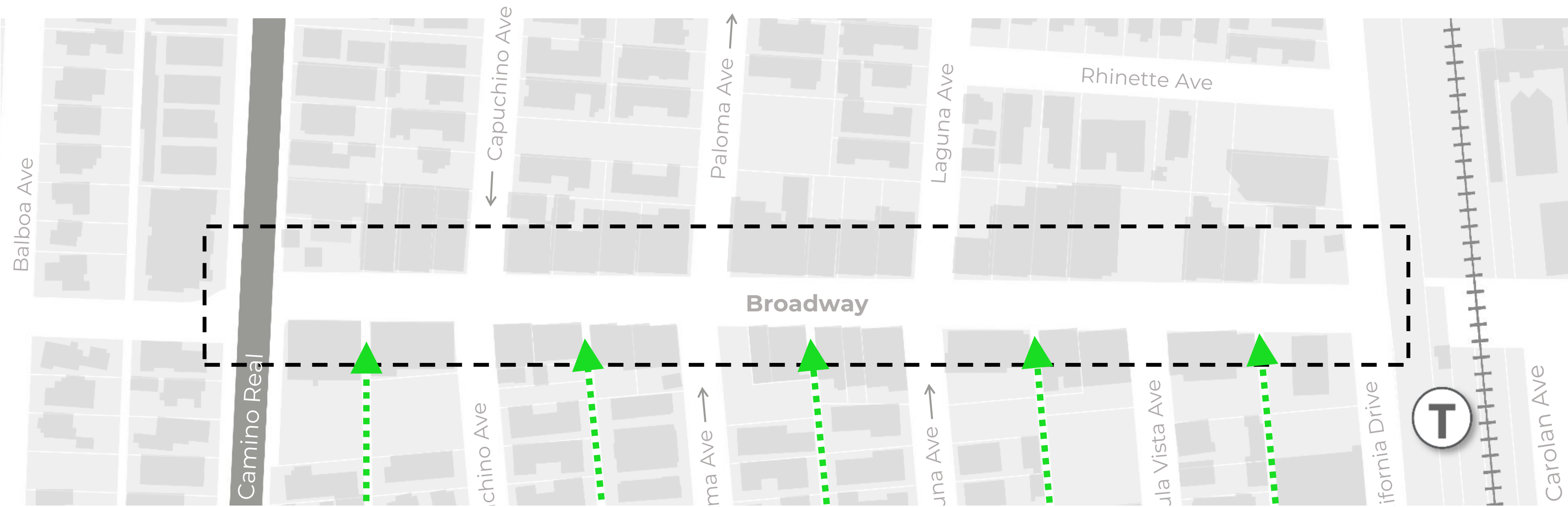
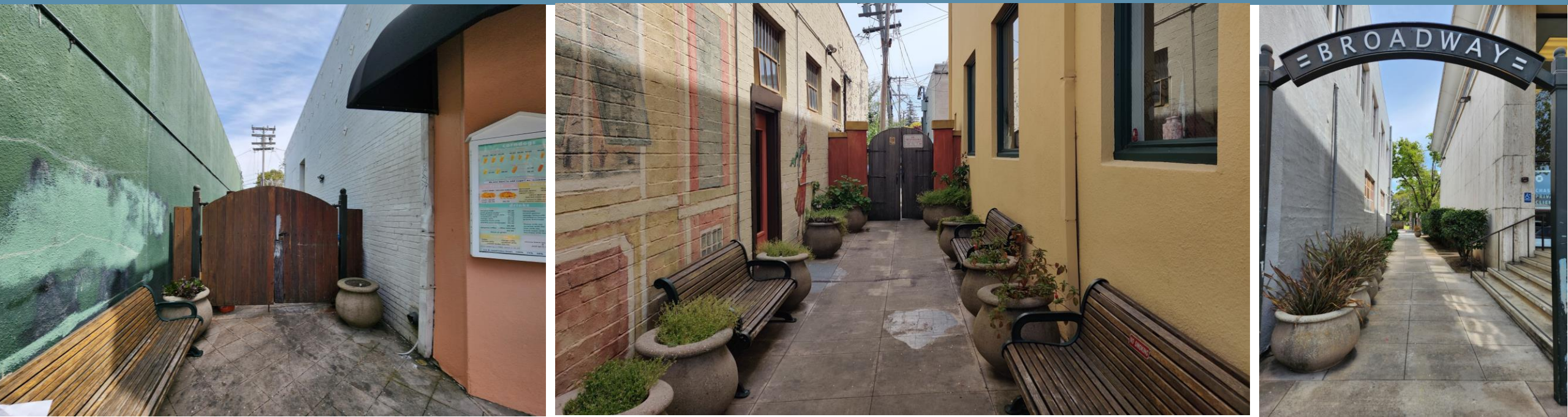


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| Parcels | Major Roads | Caltrain | Planned Bike Facility |
| Buildings | Project Study Area | Caltrain Stop | Existing Bike Facility |





- Legend:
- Parcels
 - Buildings
 - Major Roads
 - Project Study Area
 - Caltrain
 - Caltrain Stop
 - Transit Routes



- Legend:
- Parcels
 - Buildings
 - Major Roads
 - Project Study Area
 - Caltrain
 - Caltrain Stop
 - Activated Mews



Before



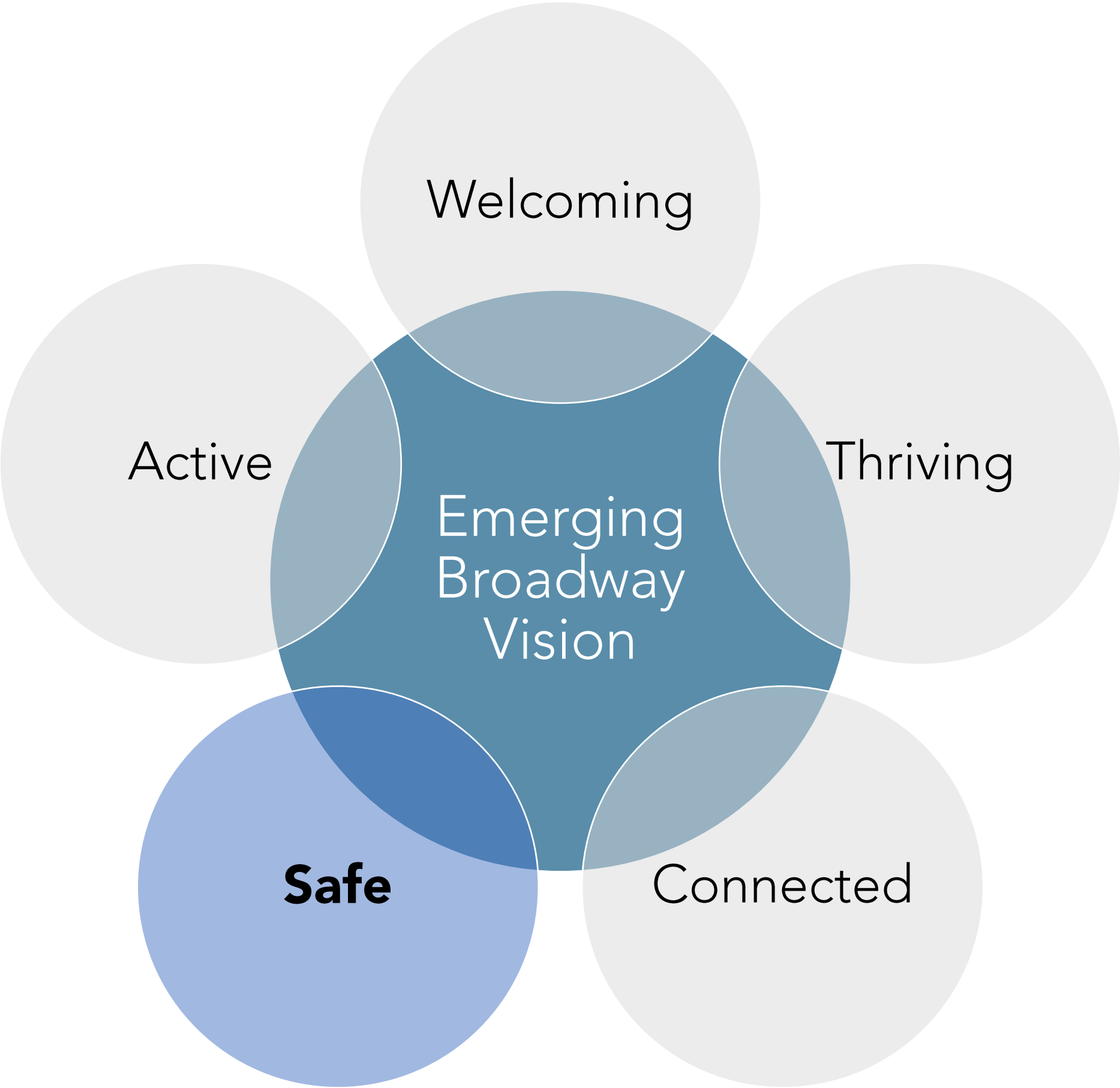
After





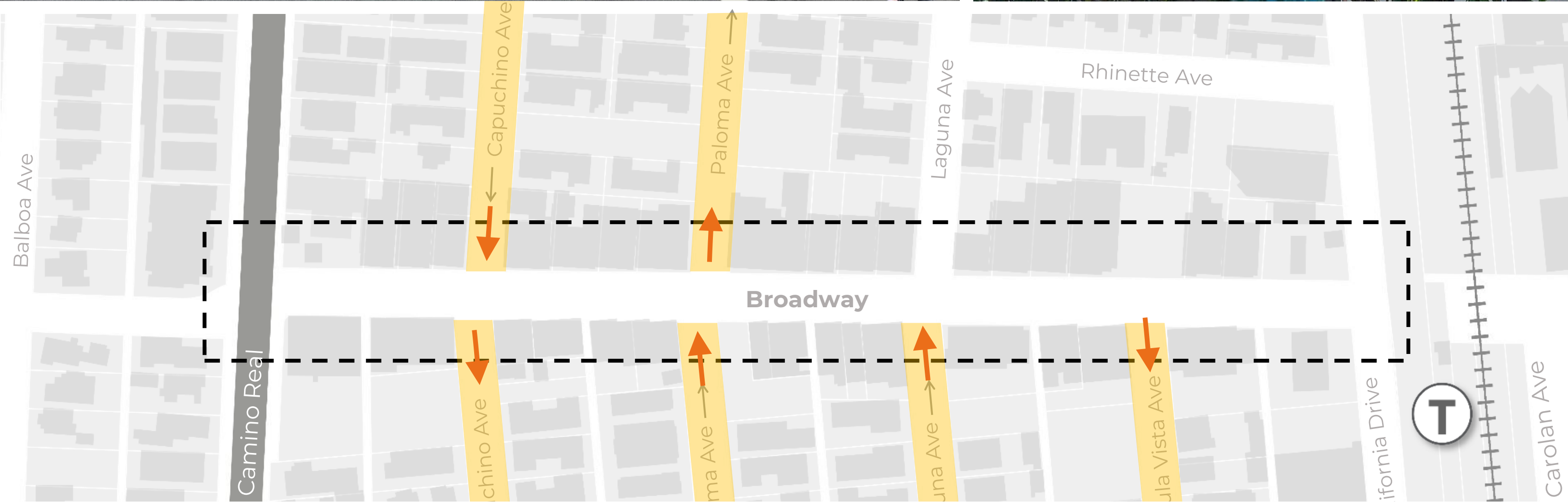












Legend:

Parcels	Major Roads	Caltrain	One-way streets
Buildings	Project Study Area	Caltrain Stop	













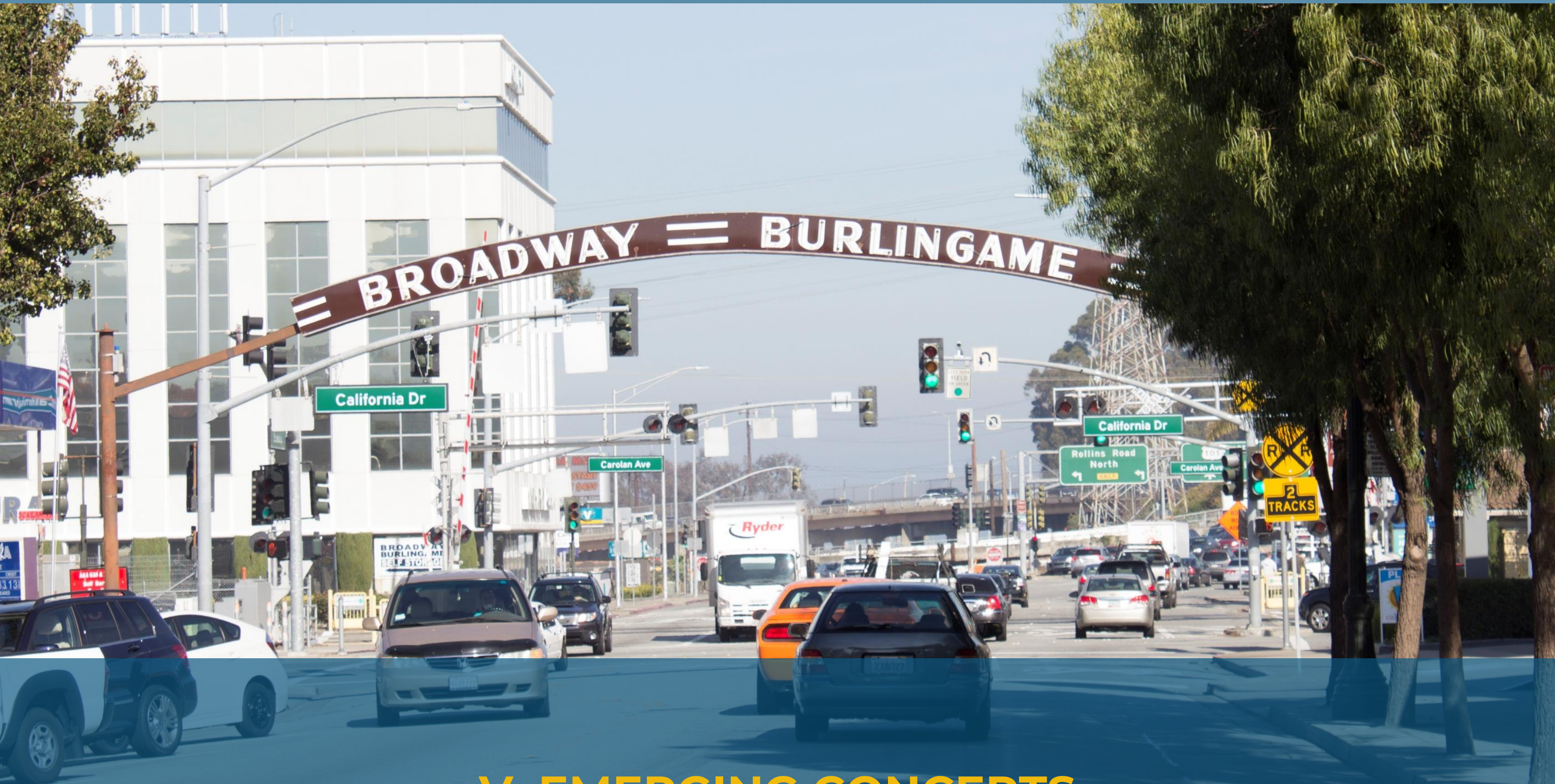




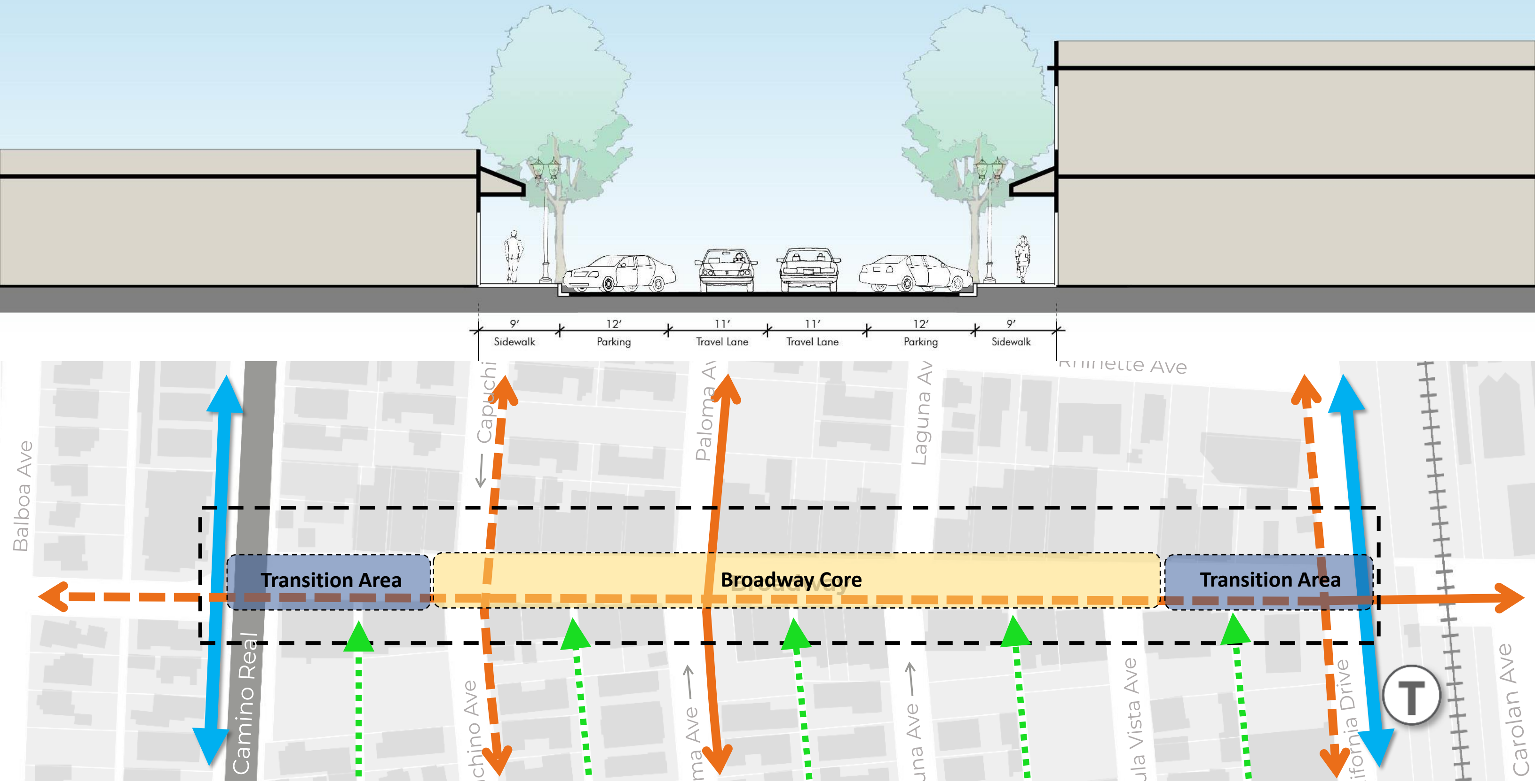








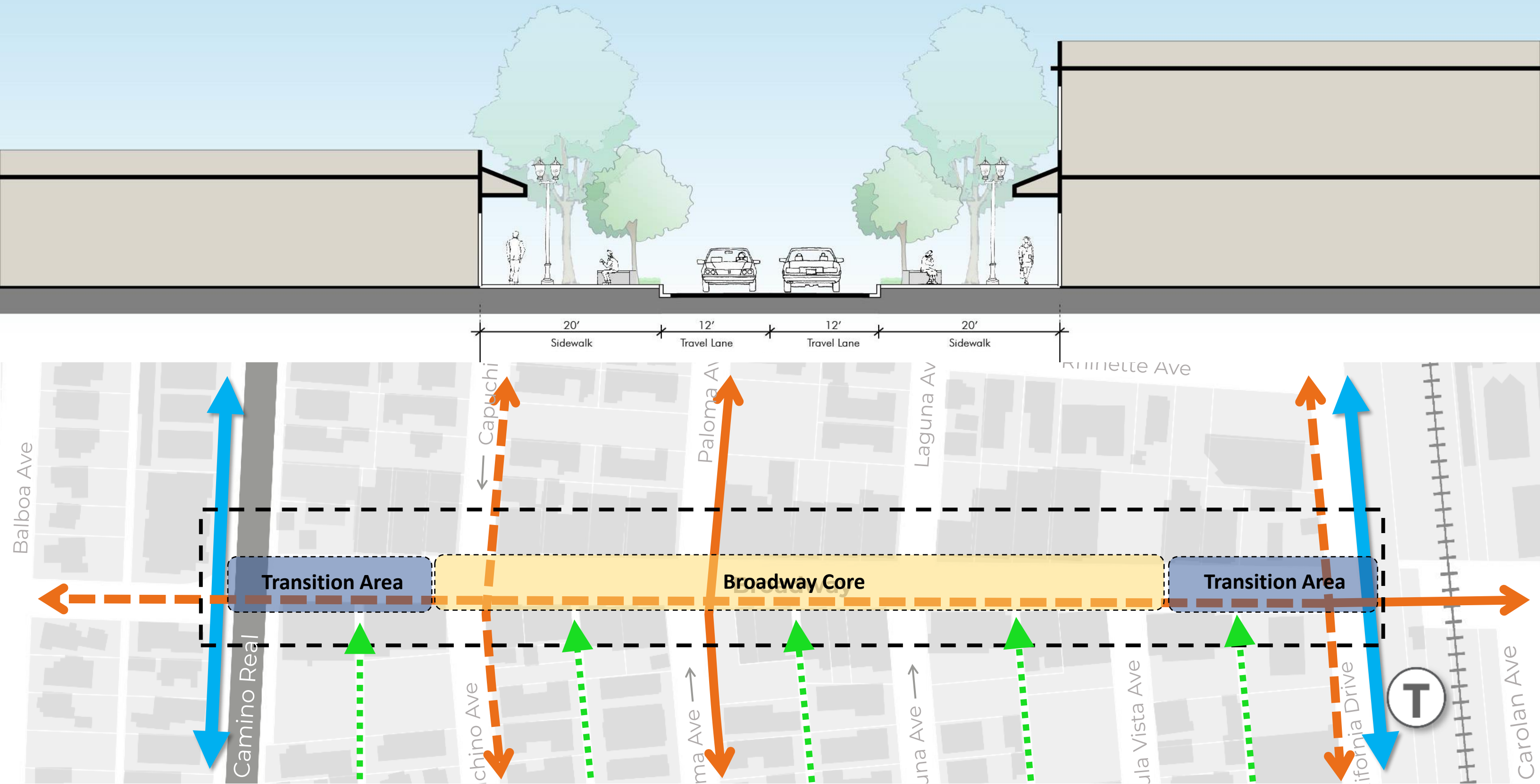
V. EMERGING CONCEPTS



Legend:

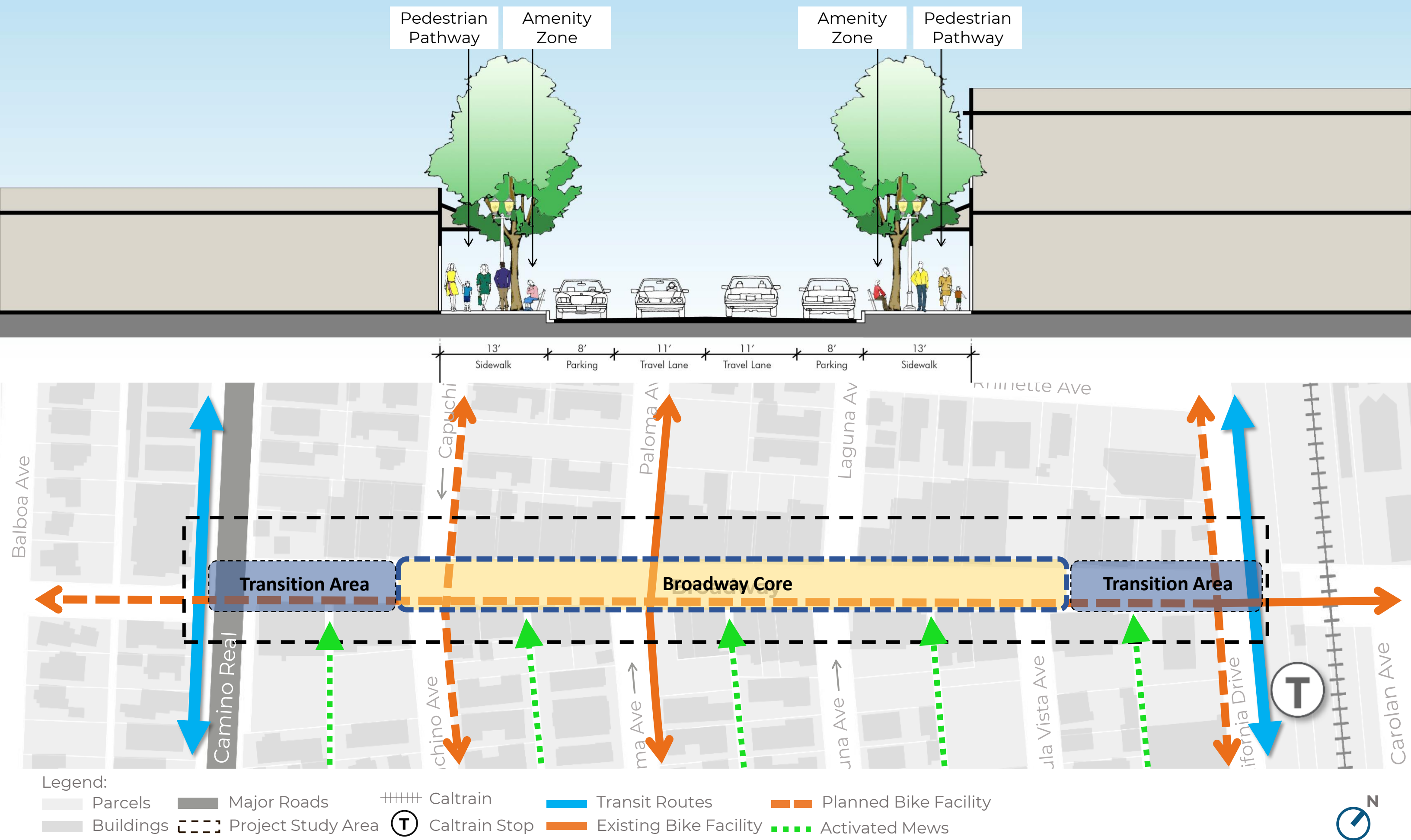
Parcels	Major Roads	Caltrain	Transit Routes	Planned Bike Facility
Buildings	Project Study Area	Caltrain Stop	Existing Bike Facility	Activated Mews



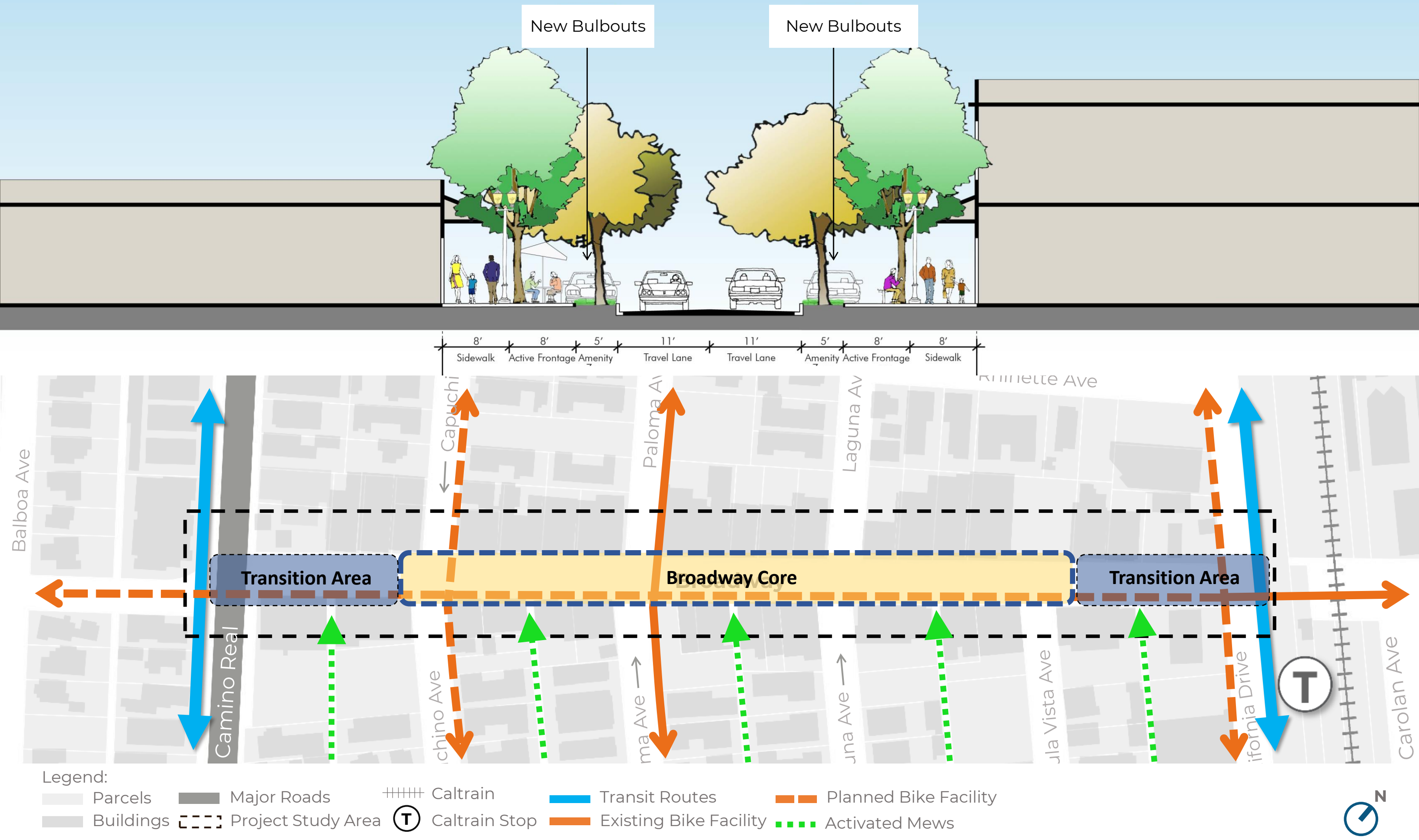


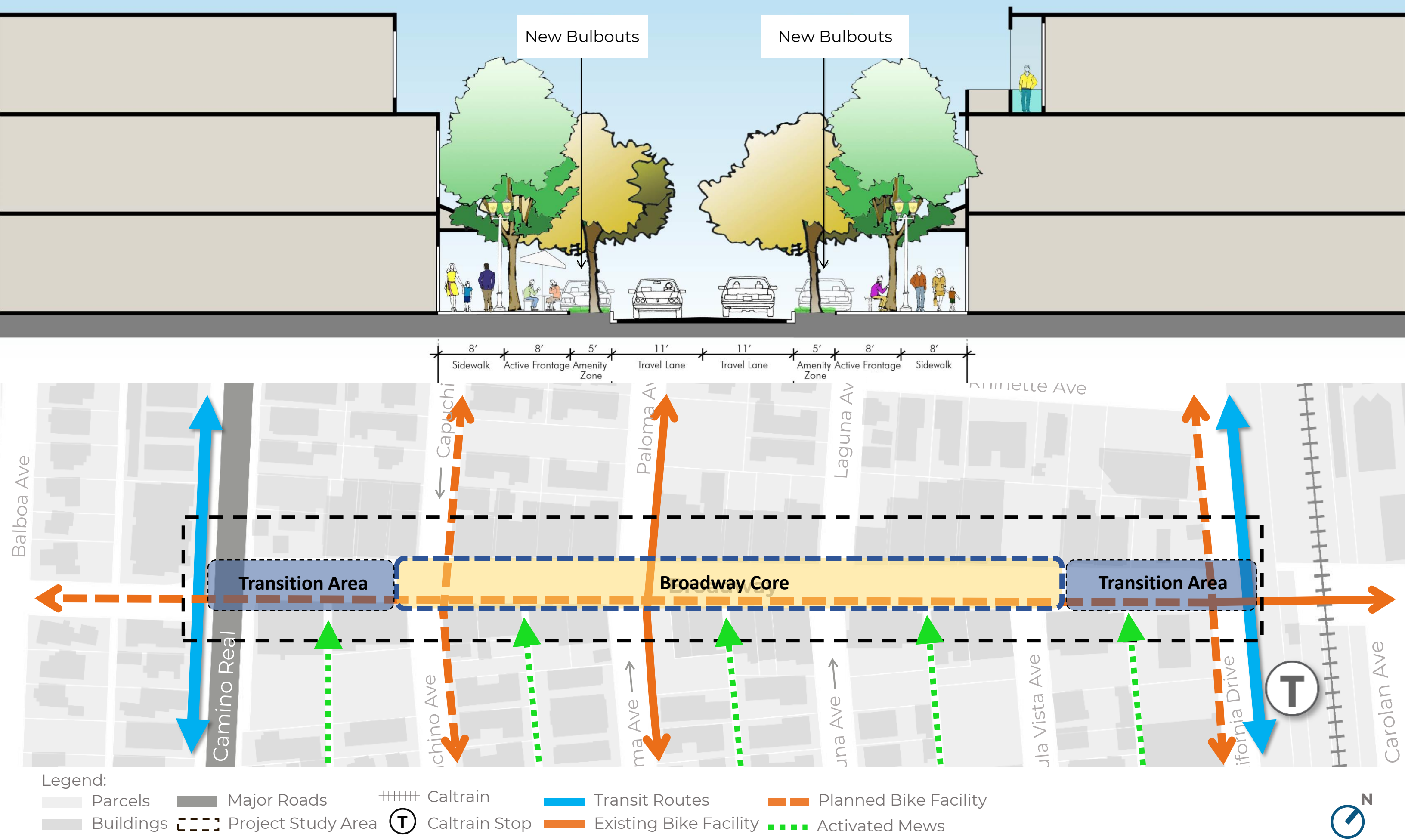
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| Parcels | Major Roads | Caltrain | Transit Routes | Planned Bike Facility |
| Buildings | Project Study Area | Caltrain Stop | Existing Bike Facility | Activated Mews |

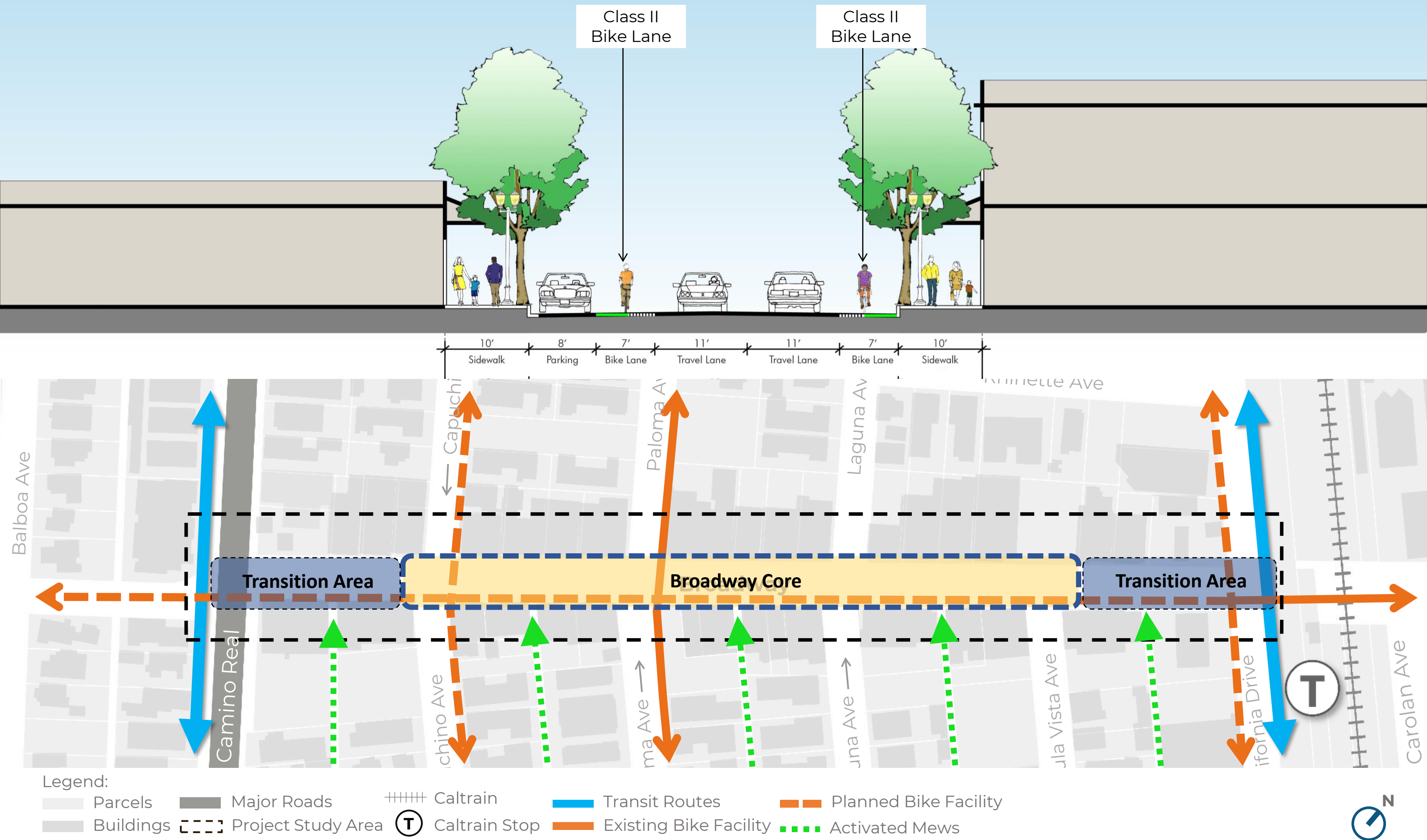


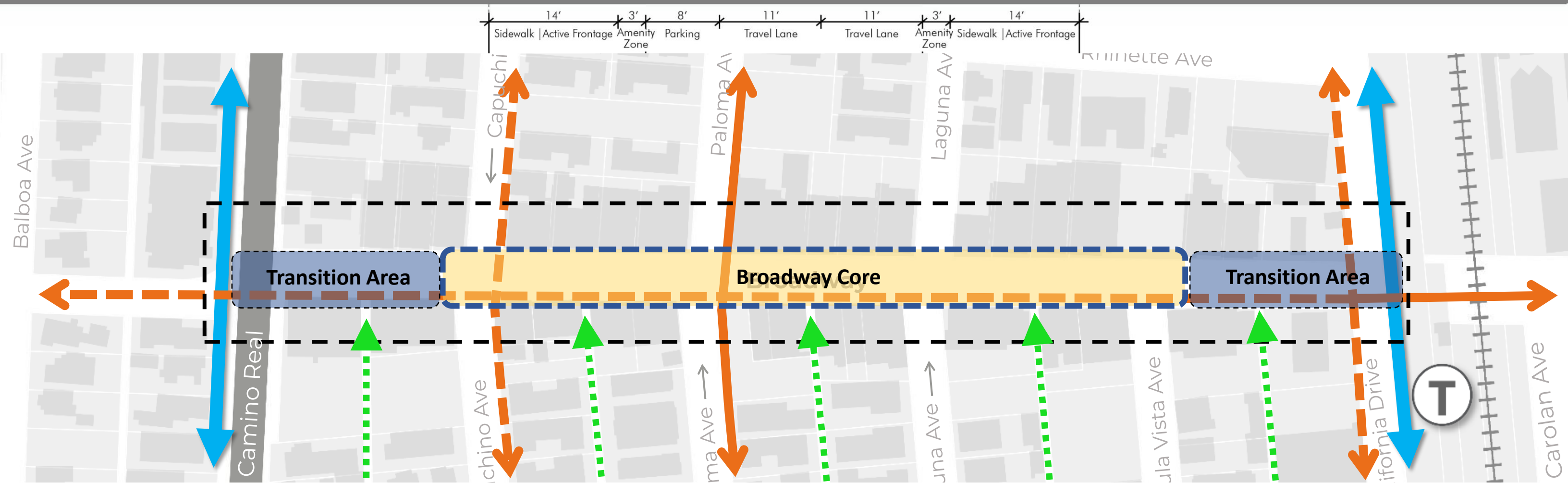
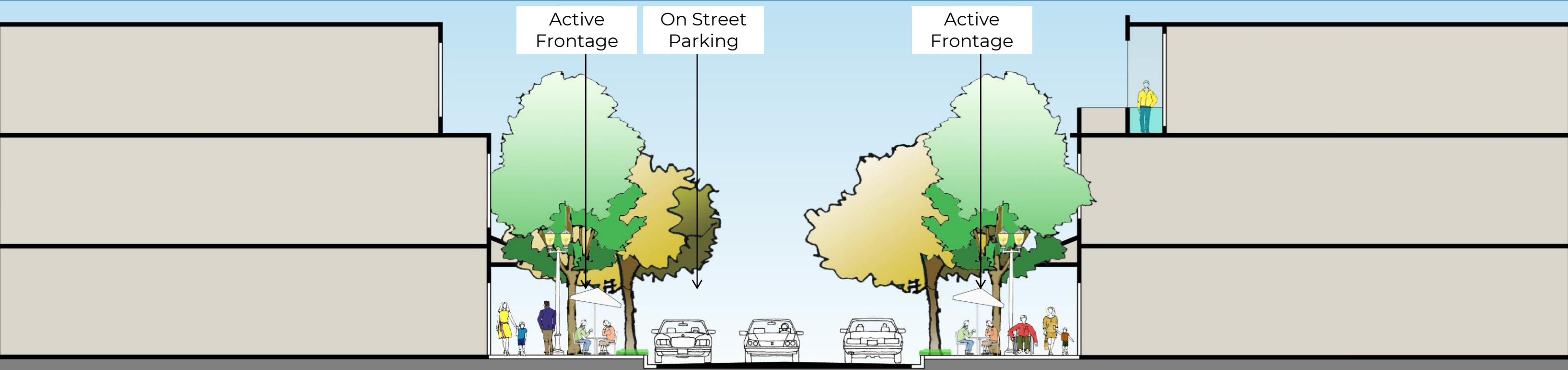


Broadway Core Option A2: Repurpose Parking Spaces for Wider Sidewalks and Active Frontage



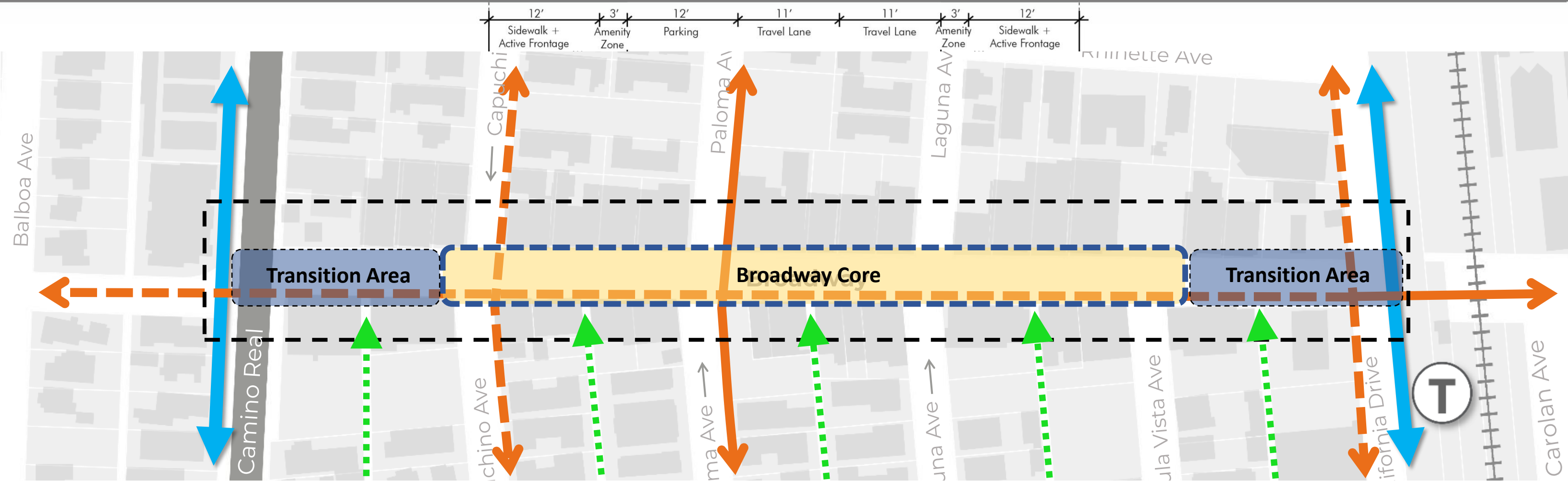
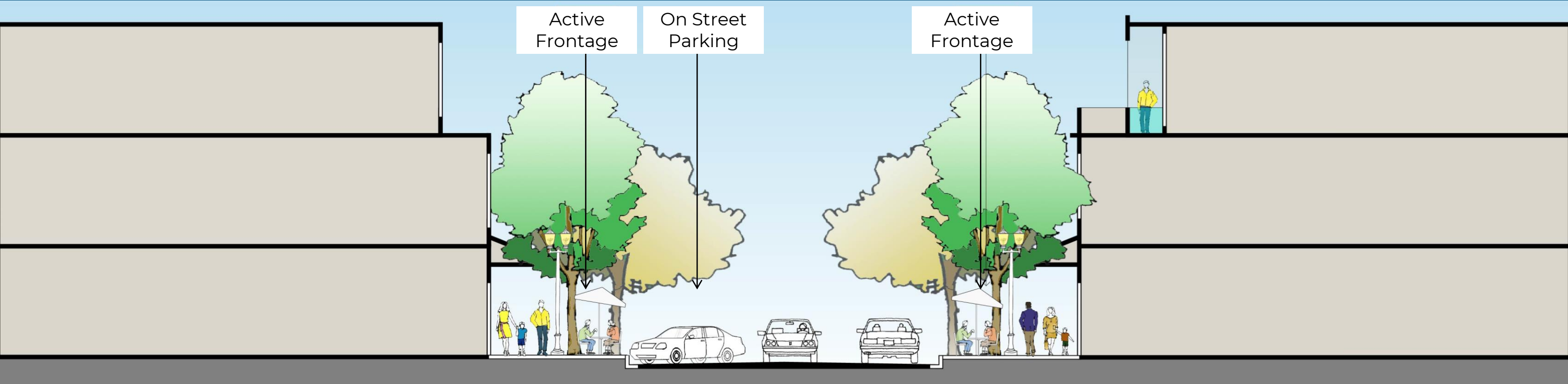






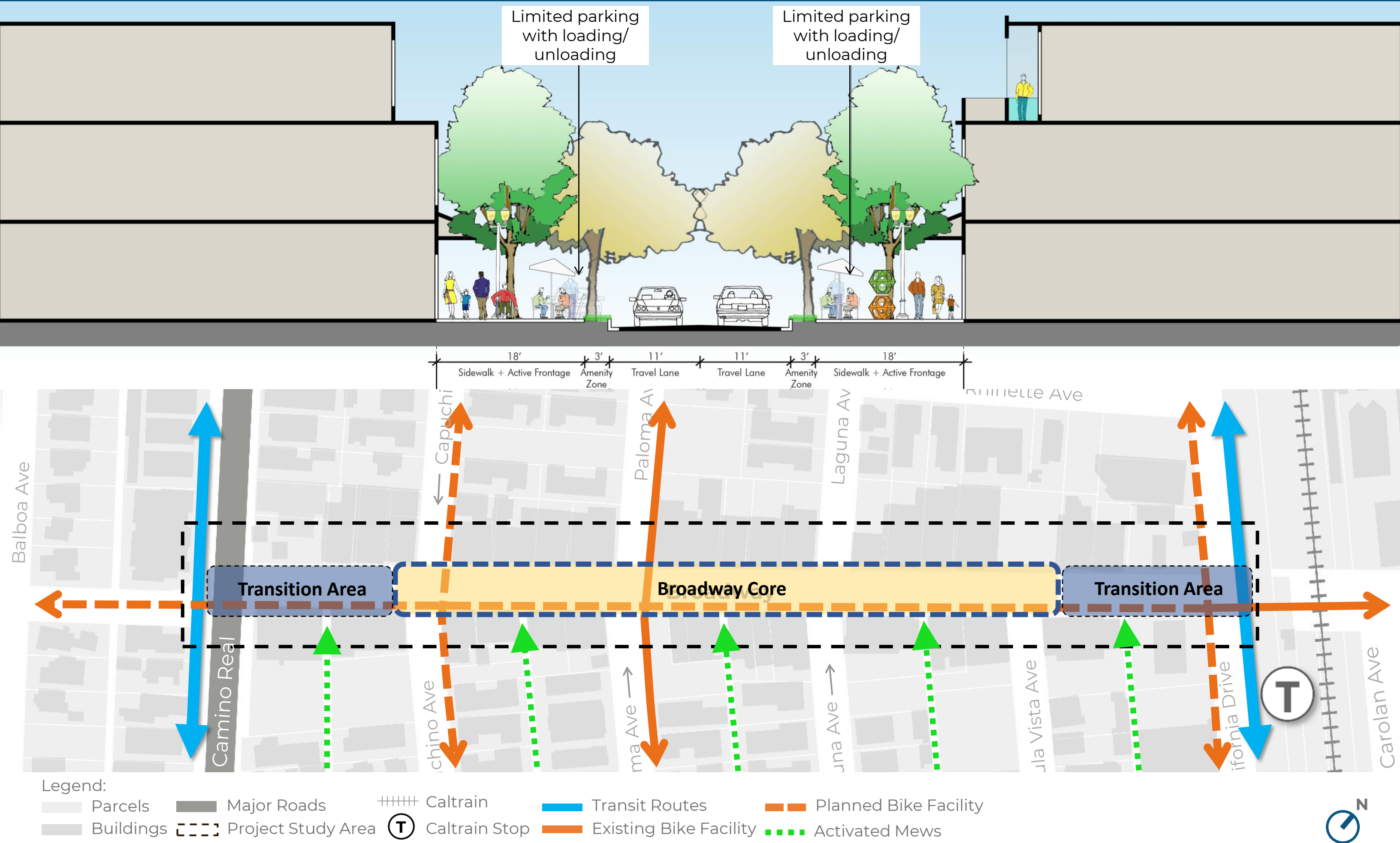
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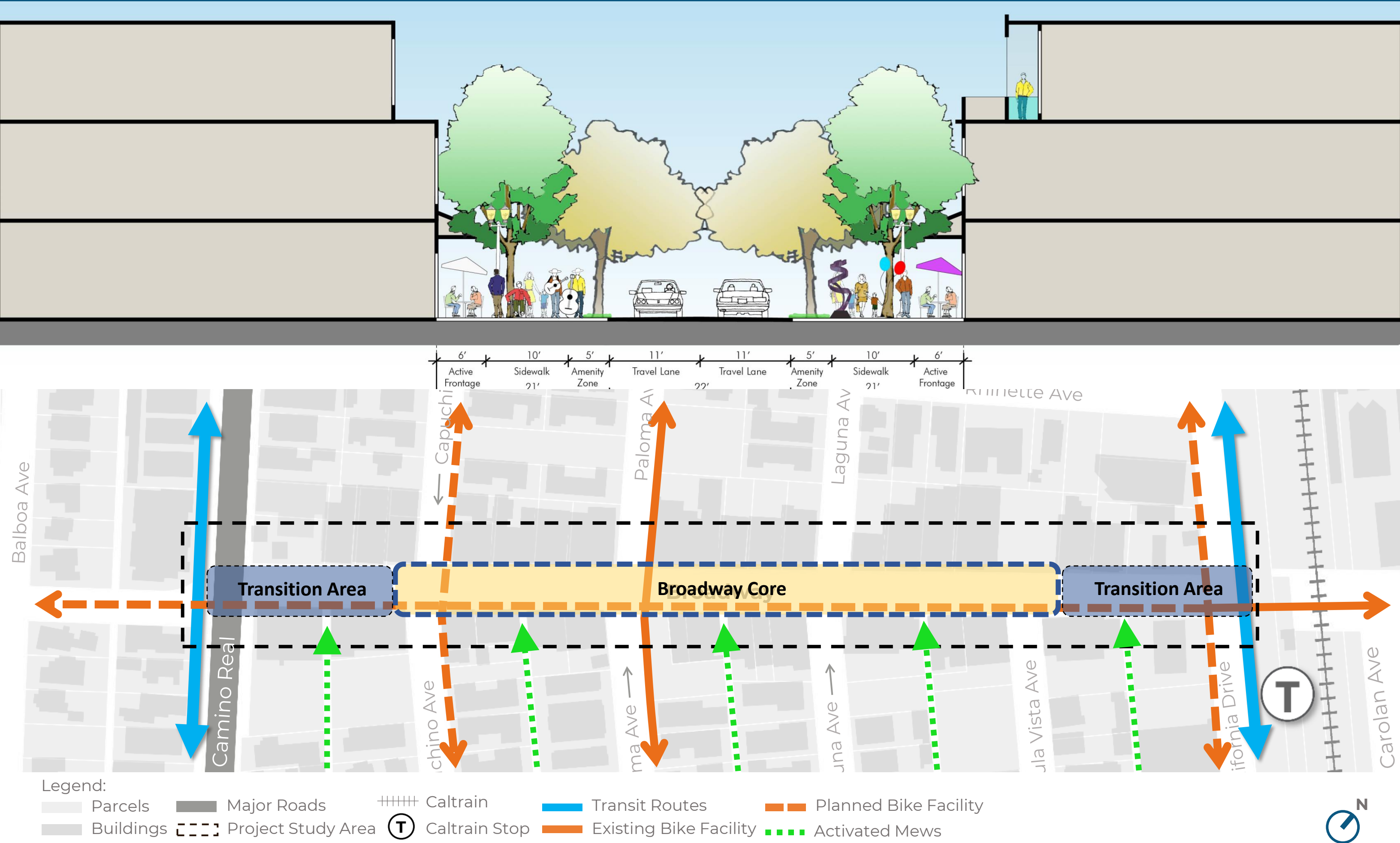


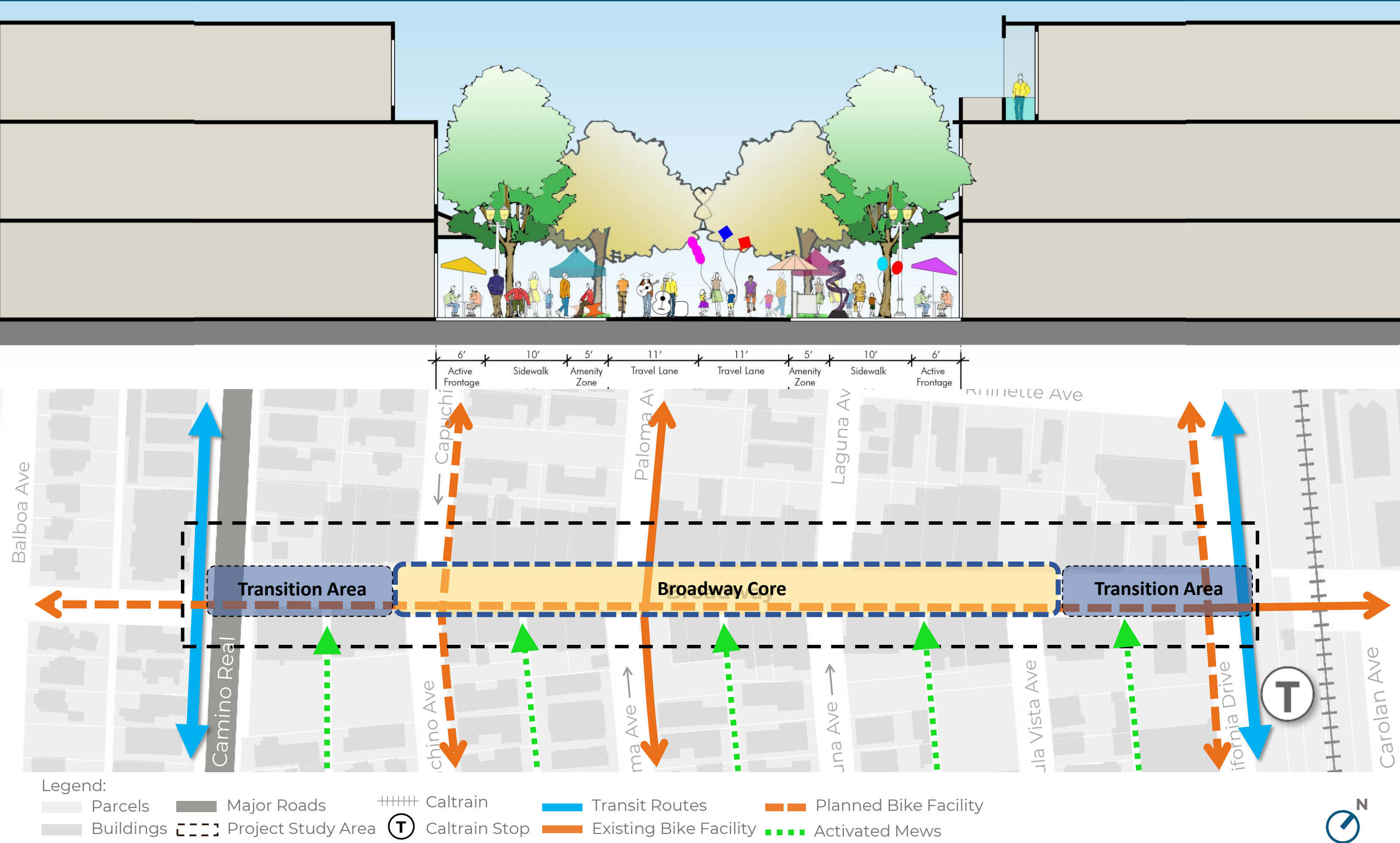


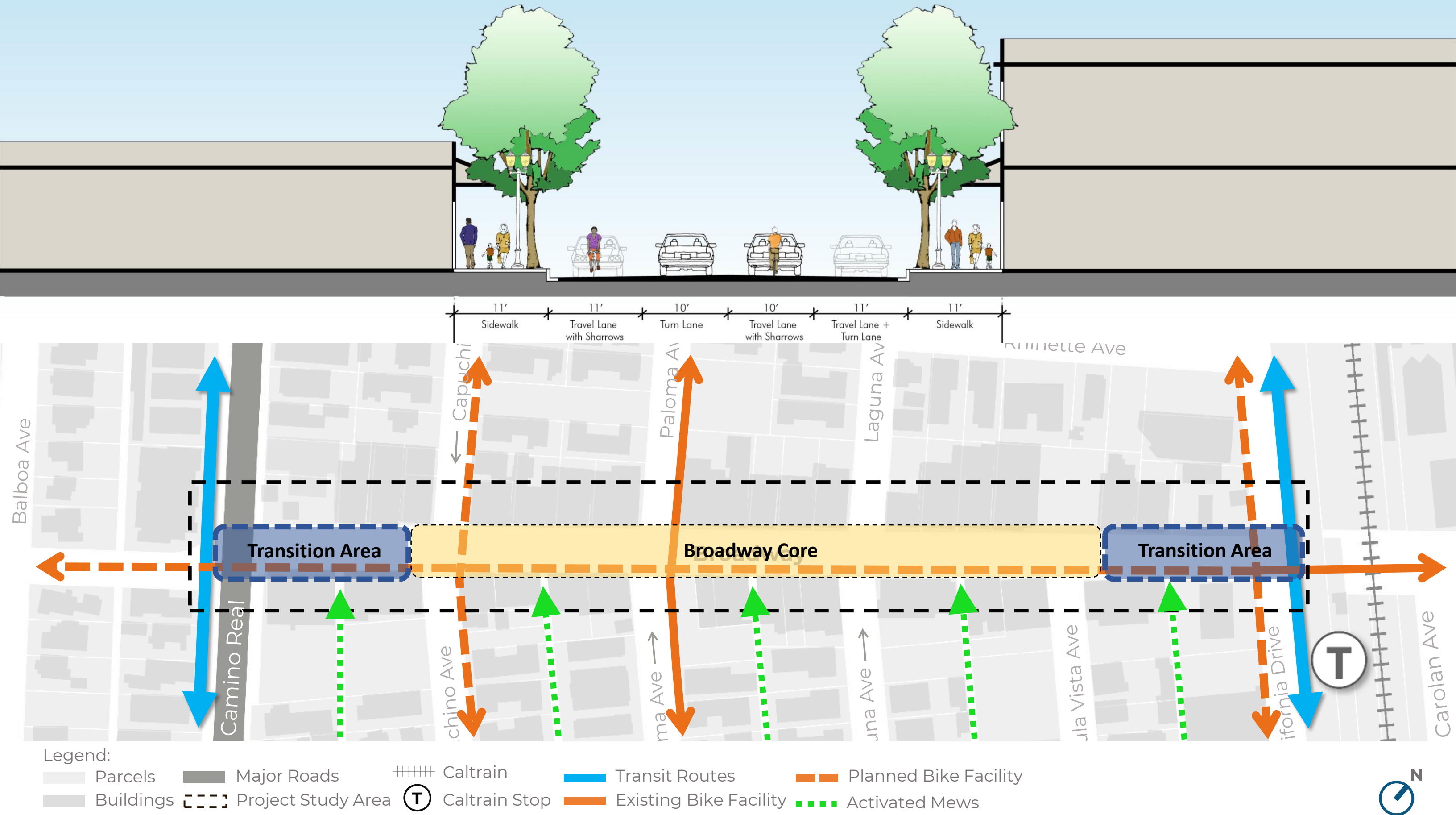
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GROUP DISCUSSION
EMERGING VISION ELEMENTS AND CONCEPTS



VI. NEXT STEPS

I. Admin Draft Existing Conditions Report (Feb 2024)

- Community Assessment
- Equity Assessment
- Housing and Jobs Assessment
- Mobility Assessment
- Infrastructure Assessment
- Resilience and Climate Adaptation Assessment
- Economic Background and Market Assessment
- Historic Resource Assessment

II. Upcoming Meetings (Feb to Apr 2024)

- TAC #3
- CAC #3
- Community Workshop #2
- Pop-up Events

