



AGENDA
CITY OF KENTWOOD PLANNING COMMISSION
TUESDAY, JUNE 28, 2022
KENTWOOD COMMISSION CHAMBERS
4900 BRETON AVENUE
7:00 P.M.

- A. Call to Order
- B. Pledge of Allegiance (Ricardo Rogers)
- C. Roll Call
- D. Approval of the Minutes of June 14, 2022.
- E. Approval of the Agenda for June 28, 2022
- F. Acknowledge visitors and those wishing to speak to non- agenda items.
- G. Old Business

There is no Old Business

- H. Public Hearing

Case#7-22 – GRR Ancillary M/U Requested Master Plan Amendment from Industrial to Commercial - Located at the NW Corner of Patterson Avenue and 36th Street SE

Case#14-22 - Bretonfield Preserve Phase 6 –Final Site Plan Review of a PUD Phase Located at 5735 Breton Ave SE

Case# 15-22 – Bretonfield Preserve Phase 6 – Preliminary Plat Review located at 5735 Breton Ave SE

- I. Work Session

Case#16-22 - Kum & Go Major Change to the PUD located at 1570 52nd Street, 5215 Kalamazoo and 5249 Kalamazoo

Case #17-22 - Kum Special Land Use and Site Plan Review for a gas station located at 1570 52nd Street, 5215 Kalamazoo and 5249 Kalamazoo

- J. New Business

Set public hearing date of , for:

K. Other Business

1. Commissioners' Comments
2. Staff's Comments

L. Adjournment

*Public Hearing Format:

1. Staff Presentation – Introduction of project, Staff Report and Recommendation
Introduction of project representative
2. Project Presentation – By project representative
3. Open Public Hearing (please state name, address and speak at podium. Comments are limited to five minutes per speaker; exceptions may be granted by the chair for representative speakers and applicants.)
4. Close Public Hearing
5. Commission Discussion – Requests for clarification to project representative, public or staff
6. Commission decision – Options
 - a. postpone decision – table to date certain
 - b. reject proposal
 - c. accept proposal
 - d. accept proposal with conditions.

PROPOSED MINUTES OF THE REGULAR MEETING
OF THE KENTWOOD PLANNING COMMISSION
JUNE 14, 2022, 7:00 P.M.
COMMISSION CHAMBERS

- A. Chair Jones called the meeting to order at 7:00 p.m.
- B. The Pledge of Allegiance was led by Commissioner Pemberton.
- C. Roll Call:
Members Present: Bill Benoit, Dan Holtrop, Sandra Jones, Ray Poyner, Mike Pemberton, Darius Quinn, Ricardo Rogers
Members Absent: Ed Kape (with notification)
Others Present: Community Development Director Terry Schweitzer, Senior Planner Joe Pung, Planning Assistant Monique Collier and the applicant.

Motion by Pemberton, supported by Poyner, to excuse Kape from the meeting.

- Motion Carried (7-0) –
- Kape absent -

- D. Approval of the Minutes and Findings of Fact

Motion by Commissioner Benoit, supported by Commissioner Holtrop, to approve the Minutes of May 24, 2022 and the Findings of Fact for: Case#13-22 – Scooter's Coffee – Kentwood – Special Land Use and Site Plan Review for a Drive-Through Establishment Located at 2380 44th Street SE

- Motion Carried (7-0) –
- Kape absent -

- E. Approval of the Agenda

Motion by Commissioner Holtrop, supported by Commissioner Pemberton, to approve the agenda for the June 14, 2022 meeting.

- Motion Carried (7-0) –
- Kape absent -

- F. Acknowledge visitors wishing to speak to non-agenda items.

There was no public comment.

- G. Old Business

There was no Old Business

H. Public Hearing

There were no public hearings.

I. Work Session

Case#7-22 – GRR Ancillary M/U Requested Master Plan Amendment from Industrial to Commercial - Located at the NW Corner of Patterson Avenue and 36th Street SE

Schweitzer stated the request has been previously reviewed by the Planning Commission. He stated we are now at the end of the 43 day period for public comment. He stated there will be a public hearing on June 28th to formally take action on the request. Schweitzer stated we have not received any additional comments since last discussed on April 12.

Benoit questioned if there were any comments from the neighbors. Schweitzer stated just the one comment from the Sinke family. They spoke very highly of their interaction with the developer as they have gone through this process.

Schweitzer stated Elliott Mueller is present and looking at making some changes to the concept plan. Schweitzer reminded the commissioners that they are only looking at the land use.

Poyner noted with the subsequent CPUD zone request City staff anticipated difficulty meeting the 30% open space requirement.

Elliott Muller, with Ben Mueller Realty was present. He stated they have engaged Exxel Engineering since the last meeting and that will help allocate open space, but all of that will be part of what the zoning ordinance requires. He stated if that means smaller building footprints, they will accommodate that. Muller stated they are not going to seek any variances on the greenspace requirement.

Jones questioned when the traffic analysis would be available. Schweitzer stated the traffic analysis is generated by the applicant. He stated traffic was highlighted in the recent consideration of a land use change at 2400 Forest Hill Avenue and it was noted in the land use decision. In a similar manner the action on the land use change will not include a traffic analysis condition. The traffic analysis would be part of the subsequent rezoning and site plan review process.

Case#14-22 - Bretonfield Preserve Phase 6 –Final Site Plan Review of a PUD Phase Located at 5735 Breton Ave SE

Case# 15-22 – Bretonfield Preserve Phase 6 – Preliminary Plat Review located at 5735 Breton Ave SE

Pung stated the requests are for the final PUD site plan of a PUD phase for Bretonfield and also the associated preliminary plat which will be a recommendation to the city commission to take final action. Pung stated this request will be the final phase of the Bretonfield PUD. He stated we are looking at 51 lots on about 23 acres. He stated the final phase will extend Breton Avenue up to the south line of the Paul Henry Thornapple trail. He stated the Engineering Department is working on an RFP for the design of the overpass for Breton going over the trail.

Pung stated as noted in the staff report, there were a few issues. He stated in the current plan they did not indicate any street trees on the east side of Breton Avenue. The trees will be added back in after discussion at the staff work session.

Pung stated there were also issues with regard to the design of the trail connection between Breton Avenue and the Paul Henry Thornapple trail. He stated the trail connection will be shown at a minimum width of 10 feet and there will be a note on the plan that final location and orientation would be determined when they work with City staff to figure out exactly where it is going to intersect with the Paul Henry trail and what the layout would be.

Pung stated there is an open space area between lots 191-192 currently with no access. As designed the only way to access is through one of the 3 adjacent lots and this is supposed to be one of the common open space areas. He stated they will be adding some type of access whether it is an easement or they modify the property line. But there will be access for the homeowners association and others to get there. Pung stated since that is part of the open space the HOA is responsible for maintenance. Pung stated staff wants to make sure there is access if maintenance is required so the HOA has a way to get to it. Pung stated there is currently a mowed path proposed to go through the back of lots 191-192 they will be reorienting that so that it does not encroach on those properties therefore it won't have to be an easement over them.

Pung stated staff did clarify that there are minimum pavilion dimensions and the pavilion will be 20 x 32 feet. Another requirement is that there will be picnic tables, that was also a requirement of the PUD

Pung stated there were issues with lots 208 and 226. The way they have been laid out, there would be a slender bit of open space that comes up to Breton Avenue. He stated which in effect makes them not corner lots even though they will have the look of being a corner lot. Pung stated on a corner lot the minimum building setback would have been 20 feet from the property line. They are going to find a way to have a minimum 20 foot building setback from the Breton Avenue ROW.

Pung stated another concern is having a large blank wall up against Breton Avenue. There are options such as enhanced landscaping or adding windows on to the side elevation so that it is not look like a blank wall on Breton Avenue.

Mike West, with Westview Capital/ Allen Edwin Homes, 795 Clyde Ct SW Byron Center, MI was present. He stated regarding the street trees they will put them back in the plan. He stated there are some site distance lines where county road commissions don't like trees within that area. He stated they will also be deciduous trees. They will add them back in along the east side of Breton.

West stated regarding the open space area behind lots 191 and 192, they didn't intend it to be a functional usable open space rather more of a passive buffer area. He stated that is a natural low area for that area of the overall site. They are going to have rear yard drainage from units 204 – 190. It will be a backyard swale and that will drain to the south in the event of large rainfalls. He stated they don't see a lot of value for it. They understand the need to provide access, however, what they would like to do instead of actually constructing a path to it because it is less than a ½ acre; they will put an easement along the south side of unit 190 so the association can get to it if there is a need for maintenance.

West acknowledged they have referenced the path connection as 6 feet on the plan but between now and the public hearing they will revise that and show a 10 foot path. He stated they are also going to center it within a 20 foot public easement for the City to take over. They will work with staff on the final location when they get closer to construction plans.

West stated regarding the side elevations for the homes on the 4 lots 176, 207, 208 and 226. There was some discussion concerning landscaping or some windows to break it up a bit. They will be adding the landscaping back in so the street trees will go in along the west side of lots 176 and 207. He stated as far as windows they have previously submitted a package of elevations to the City. He stated he drove through the development and there are 12 corner lots and 9 out of the 12 have at least one window, some have 3-5 windows. He stated they can work with staff on trying to insert windows, however, it depends on the model of home being purchased by the homeowner. He stated he doesn't know if he actually wants to make a commitment to a certain number of windows. He stated it wasn't done on the previous 5 phases and this is their last phase. He stated they can do their best to put in windows but he isn't sure if he wants to commit to the amount of windows. However, if it is strongly the will of the commissioners he can discuss it with their building construction people and see about the minimum number that might be acceptable, regardless most of the corner lot homes will have windows.

West stated regarding lots 208 and 226, the two lots on the west side of Breton, they spoke to the Engineer after meeting with staff and they think they can make an adjustment to the cul-de-sac and push it just a little bit west, modify some of the other property lines/lot lines to gain more width. West stated staff explained they want it a minimum of 20 feet off the Breton ROW. He stated they will get it to 20 feet from the Breton ROW on lots 208 and 226. He stated they will show that on the revised site plan.

West stated regarding the proposed path on east side of Breton, they are going to modify that and push it back into the open space.

West stated the dimensions of the pavilion will be 20x32. He will have the dimensions on the revised plan that they submit. He stated as far as the tot lot area that is approximately 54 x 58 feet. He stated he will get that on the revised plan as well.

West stated there was a comment regarding fencing between the tot lot and Breton. He stated they would rather not install any fencing. Most of the people who bring their kids to the tot lot are going to be accessing it from the path along the west side Breton. He stated what was shown on the landscape plan will be a connection from the path along Breton and they have some decorative vinyl fencing and some landscaping and a path that will lead people to the tot lot. He stated they want to keep it an open area an amenity that people are going to go to. He stated he doesn't think a lot of kids are going to be running out to Breton because the majority will be coming from Breton to go to the tot lot. He stated they would like to have that extra open space in case someone wants to throw a Frisbee or football. They would rather not install a fence.

He stated they acknowledge the need for consistency with the PUD agreement.

Rogers noted there was mention they are going to consider pushing the end of the culdesac to the west to address the corner lot building setback issue he questioned whether there would then be any problems with the wetlands. West stated they might push it back 6 or 8 feet and with that with adjustments on the other lot lines they might be able to pick up another 8 or 10 feet for those lots west of Breton. He acknowledged that the wetlands may be a limiting factor.

Holtrop questioned what happens to the land between the corner lots 208 and 226 between their lot line and Breton. West stated that will be common just like the roadway ROW. Holtrop stated it will be maintained under the HOA. West stated that is correct they will mow it up to that point in addition to the greenspace along Breton.

Holtrop stated the tot lot picture has the walkway to and from Breton. Immediately across Breton is the other walkway. Is there any thought to an island for safety crossing. Holtrop stated the walkways are directly across from each other from the east and west. West stated they can work with the City on something, he doesn't know what it will be. Pung stated as part of the road design they are putting in a mid-block crossing. Pung stated there is a mid-block crossing in so there is a paved landing within that median. He stated basically we would be looking at doing the same thing put in a crossing within that median so they can go right across.

Holtrop stated 190, 191 and 192 there is a vacant open space. Holtrop stated it looks like there is a sidewalk from Breton coming up behind 189 and then into the neighborhood. He wanted clarification why they don't want to continue that behind 190, 191 and 192. West stated the purpose for the sidewalk at the end of the cul-de-sac is to get people into

the cul-de-sac out to Breton and then get them to the path. West stated to extend the sidewalk to the east along the south side of lot 190 you are only going to the open space area. He stated they will provide an easement but they don't think it is a lot of value to put a paved path or mowed path there. He stated there is really nothing there, it is not going to be a formal connection to the trail system. Holtrop stated how about another connection to the trail otherwise you are going all the way north and that would provide another access. Holtrop questioned if the sidewalk could go between 190 and 191 and come across the back and then between 190 and 191 for the neighborhood and then a gravel trail across the open space to the Paul Henry. It would still be walkable and bikeable for people. West stated he will discuss with his engineers but at a minimum they will provide the access easement for the association but he doesn't know about another connection to the Paul Henry. Holtrop encouraged West to go look at Baileys Grove and look at the gravel trail south of the gazebo that leads back into the neighborhood. He noted that he hasn't noticed a lot of maintenance, but stated it does get damp at times.

Holtrop questioned the topographical elevations of lots 208, 226, 207, and 176 relative to Breton Avenue. West stated he doesn't know yet. Holtrop stated a number of people have been dismayed by the relative topographical elevations of the homes that are fronting Breton and 60th. He stated they have plain sides. West stated as a former municipal planner he understands that is not the most desirable look along their major thoroughfare unfortunately that pre-dated him. West stated the commissioners are not going to get that same circumstance along this area of Breton Avenue. Holtrop strongly suggested that they get more than two windows, it has to be broken up for those 4 lots somehow. West stated there is a spec home on the corner of Plover and Breton to the south. That is a ranch spec home that has 2 windows on the Breton side. Holtrop strongly urged that he doesn't want the homes to look like Breton and 60th Street. West stated it won't.

Poyner stated even if it is one home on the road we want to make sure that house doesn't look like the homes that are along 60th Street. He stated he as well is going to strongly suggest that they take a hard look at that and try to make sure that we have a product that doesn't look like the homes on 60th Street.

Poyner also questioned why he can't find access to the Paul Henry Trail on the south side. Poyner suggested finding a way to do that. West stated they are now at the final stage and adding another access that hasn't been envisioned in any of the previous phases is just difficult. He stated they have homes and don't want to have another pathway going through the back of those homes when we have 2 other options that are just up the road. Poyner asked him to take a look at it because the trail is used by a lot of residents and having an additional access on the trail he thinks is a real advantage.

Poyner questioned if there is any way to encourage the developer to consider the report from the planning and city commission recommendations, can we review the facades and sideyard setbacks to the applicant. Schweitzer stated there has been discussion with Allen Edwin Home's indicating to them that there may be new requirements in the future. He

stated we haven't actually come up with the draft requirements, it may be several months before we do. Discussion ensued.

Quinn questioned if the greenspace behind lots 189 and 190 would be used as a drain. West stated the small open space area on the southeast side of lot 190 and 191 is going to be receiving rear yard drainage from the lots along the Paul Henry.

Pemberton stated he would echo the other commissioners to encourage adding some beautification to blank walls. He stated 60th Street homes are the primary catalyst behind discussions that both city and planning commission have had. The homes just jump out at you and we wish we could have done something about it then. However, we don't have the rules yet therefore technically we can't hold them to it. We can strongly encourage them to take the initiative to make this look better. Until we make this into an ordinance we can just encourage more. He stated he likes the use of different materials, colors, some trims etc. There are a lot of creative things that are out there that aren't horribly expensive that can make a big difference in the way things look. He stated if he puts a little more pizzazz in the street appeal it won't hurt them. Even though it is not the rule of the land yet, it is going to be eventually and encouraged him to get a feel for what we are talking about and have an idea of what we want and work into the game plan.

Holtrop questioned if it would be possible for the commissioners to see the full range of previously approved elevations for the remaining 51 homes. Pung stated yes he can provide them to the commissioners. West stated there is language in the PUD agreement that speaks to elevations and variety and architectural diversity etc. He stated they would like to continue that with the final phase and believe they will meet the commissioners concerns with those homes following what is in the approved PUD agreement.

Jones questioned if he was aware of the City's concerns of the current Allen Edwin development. West stated he has heard the issues with the 60th Street properties. He stated he doesn't know of any other ones. West stated he has driven through the whole development several times and hasn't been involved with the previous phases, but it is a very popular subdivision. He doesn't know if the people that are buying in there share the same views as the City does. West stated it is one of their bestselling subdivisions, people like living there. They don't mind the smaller lots, they don't mind the smaller sideyard setbacks in return for the amenities that they have with the trail systems and the connectivity to the Paul Henry. The amenities they are building in the final phase with the tot lot and the pavilion is going to be very nice.

Jones stated the concerns are not related to lot size. She stated she thinks the density makes sense. She is not suggesting that people are unhappy with the homes there. She stated what she is saying is there are concerns about what you can see from 60th Street. She stated this development was the impetus for the planning commission and city commission to come together and have some conversations about what we want to see and what we don't want to see. She stated the back side of the homes on 60th Street is nothing that we would have ever expected and we don't want to see more of that. Jones

stated it is an eyesore to see what has been done there. She stated they have looked at other Allen Edwin developments and we know what they can do. She stated the Bretonfield development may be attractive and families are happy moving into it, but, there are families who are just happy to find a home in Kentwood. However, she knows this can't be their best work. She stated she is concerned about the blank walls facing Breton. She stated the commission cannot compel them to do anything beyond what has been defined within the PUD, however, we can implore, and we can ask. She stated please put windows more than one, please continue this development in such a way that it is more reflective of the kind of work Allen Edwin does in other communities. She stated she has seen Allen Edwins work in Caledonia, in Byron Center and it is not what they have in Kentwood. She understand the issues with affordability, price points etc. she just wants Allen Edwin to get a sense of Kentwood's level of frustration with the houses on 60th Street. She would also like to see what he comes back with after he has had some discussion with their Engineer about the potential of doing a gravel trail connection. Discussion ensued. She thinks the tot lot should have some fencing around it. In terms of safety we weren't expecting a fence along all of Breton Avenue.

West stated he has read the agendas and doesn't see a clear consensus on what exactly it is the commission wants. He stated you want to improve elevations and keep the cost of housing down, it is a difficult balance to get. He stated by being on both the planning and Allen Edwin sides he can say that the developer just wants to know what the rules are. He stated he sees the approved PUD language and it is difficult, to try to compare 60th Street to the 4 lots on the corner is not a fair comparison. He stated the 60th Street frontage is elevated and you have the back sides of those homes, if they were flipped and the front facing 60th Street he doesn't think they would have the same concerns. Discussion ensued

Poyner stated we don't know what the outcome is going to be with these 4 homes. That is why the commissioners are discussing what we have been discussing and the things we would like to see and what he can do to make sure that outcome is not like 60th Street homes. Poyner said he says they are going to be different, but, based on what, we don't have an outcome yet. West stated the scenario is different and what they are looking at is not the situation on 60th Street. He suggested that the commissioners drive through the development. The commissioners are comparing side elevations off an internal street to rear elevations on a major street. He stated they have negotiated a PUD agreement that is very specific. West stated he doesn't think they will have a problem with a couple windows and put some more trees in and widen the lots out and get to what the commissioners are getting at. Discussion ensued.

J. New Business

Motion by Holtrop, supported by Benoit,, to set public hearing date of July 12, 2022, for: Case#16-22 - Kum & Go Major Change to the PUD located at 1570 52nd Street, 5215 Kalamazoo and 5250 Newcastle; Case #17-22 - Kum Special Land Use and Site Plan Review for a gas station located at 1570 52nd Street, 5215 Kalamazoo and 5249 Kalamazoo

- Motion Carried (7-0) –
- Kape absent -

K. Other Business

1. Commissioners' Comments

Quinn stated he enjoyed the Food truck event. Quinn stated there is a citizen's group holding a fundraiser for the Park and Rec millage going on now.

Holtrop stated the concert in the series is starting on Thursday and along with the Farmers Market.

Jones let the commissioners know that Brainerd has resigned from the Planning Commission.

Rogers stated he will be leaving the Planning Commission and moving to Houston, TX in about 4 weeks.

Jones stated there is a Parks and Rec info meeting on Monday June 20 at Northeast Park.

2. Staff's Comments

Schweitzer noted that the Grand Valley Metro Council is in the process of studying the roadway network in and around the Gerald R Ford International Airport in an effort to improve accessibility to the airport for both passenger and freight traffic. As the study progresses staff will share the prospective improvements with the Planning and City Commission.

L. Adjournment

Motion by Commissioner Benoit, supported by Commissioner Pemberton, to adjourn the meeting.

- Motion Carried (7-0) –
- Kape absent -

Meeting adjourned at 8:25pm

Respectfully submitted,

Ed Kape, Secretary

PLANNING STAFF RECOMMENDATION

Golder 6/22/22

PROJECT: GRR Ancillary M/U

APPLICATION: 7-22

REQUEST: Change of Master Plan designation from Industrial to Commercial

LOCATION: Northwest corner of Patterson Avenue and 36th Street SE

HEARING DATE: June 28, 2022

RECOMMENDATION: Recommend conditional approval of the request to amend the Master Plan designation for ten acres of land located at the northwest corner of Patterson Avenue and 36th Street from Industrial to Commercial, as described in Case No. 7-22. Approval is conditioned on the following:

CONDITION:

1. Any PUD Plan submitted for the site shall include a traffic analysis that will be reviewed by the City and the Kent County Road Commission.
2. The PUD site plan shall include a driveway connection to the north, in order to connect to the traffic signal at 33rd Street/Patterson Avenue.
3. The Patterson Avenue and 36th Street frontages shall each allow only one curb cut serving the development.

BASIS:

1. The proposed uses are compatible with the surrounding uses.

The city understands the need to preserve industrial land within the city. However, a limited amount of commercial use can provide services for the employees of the industrial sector. The current zoning already allows for certain services such as vehicle fuel stations, day care centers and restaurants.

2. The proposed use is generally consistent with the Guiding Principles identified in the Master Plan for Section 13.

The Master Plan currently recommends Guiding Development that include:

- a. **Development of Section 13 as a PUD.**
- b. **Internal roads and trails within the development to provide connectivity through the site and to adjacent properties.**
- c. **Limited driveways on Patterson Avenue.**
- d. **Design guidelines required for a unified and cohesive development.**
- e. **Restrictions on uses adjacent to residences.**

The proposed development meets most of these principles; or will be able to meet the principles within a future PUD development.

3. The proposed master plan change has received support from area property owners.

The Gerald R. Ford International Airport has indicated its support for the master plan amendment. Adjacent residents have also indicated their support during the public comment period for the Master Plan amendment.

4. The current zoning allows several of the uses that the applicant intends to include in a proposed CPUD development plan.

Vehicle fuel stations, restaurants, day care, and personal service establishments are allowed in the current IPUD zoning district. Other uses that have been proposed, such as a hotel, are subject to CPUD approval and Special Land Use criteria.

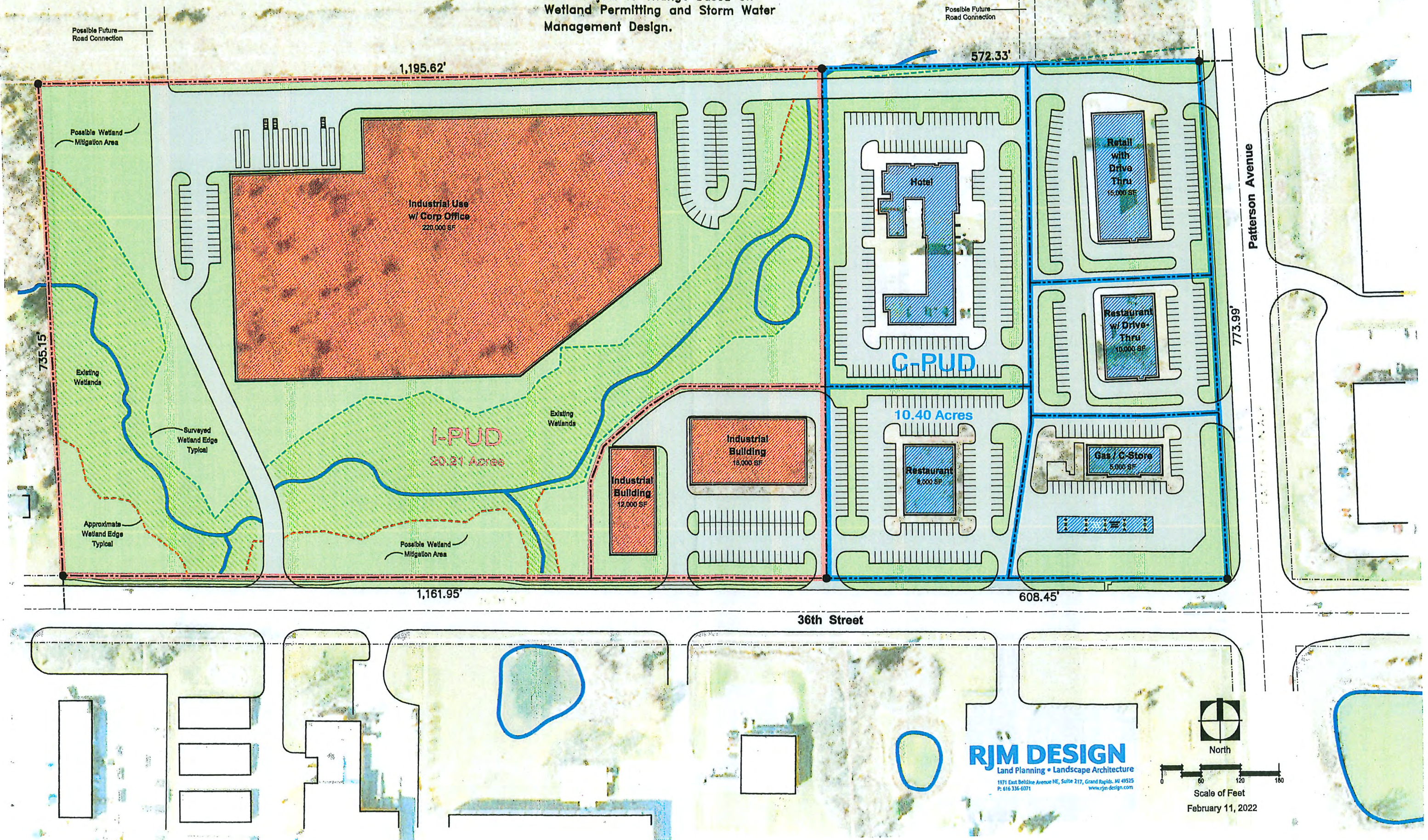
5. Although the Master Plan is recommended to be amended to allow for commercial use, the Plan also clearly requires the rezoning to a Commercial Planned Unit Development. The CPUD zoning, if approved, will allow the city to address access and use issues that are of concern for the development.
6. The driveway connection to 33rd Street will allow the eventual connection to a signalized intersection for the development. The allowance for one driveway each to access Patterson Avenue and 36th

Street is consistent with the qualifying conditions for PUD districts outlined in Chapter 12 of the Zoning Ordinance. Interior driveways providing connection between uses is a standard outlined in Section 12.05 4 of the Zoning Ordinance. The PUD rezoning and site plan review for this site will include an analysis of the types of uses proposed to ensure that they support the area's industrial base. In addition, the traffic analysis and site review will ensure the development will not have a negative impact on adjacent roadways and that the internal circulation between uses is achieved.

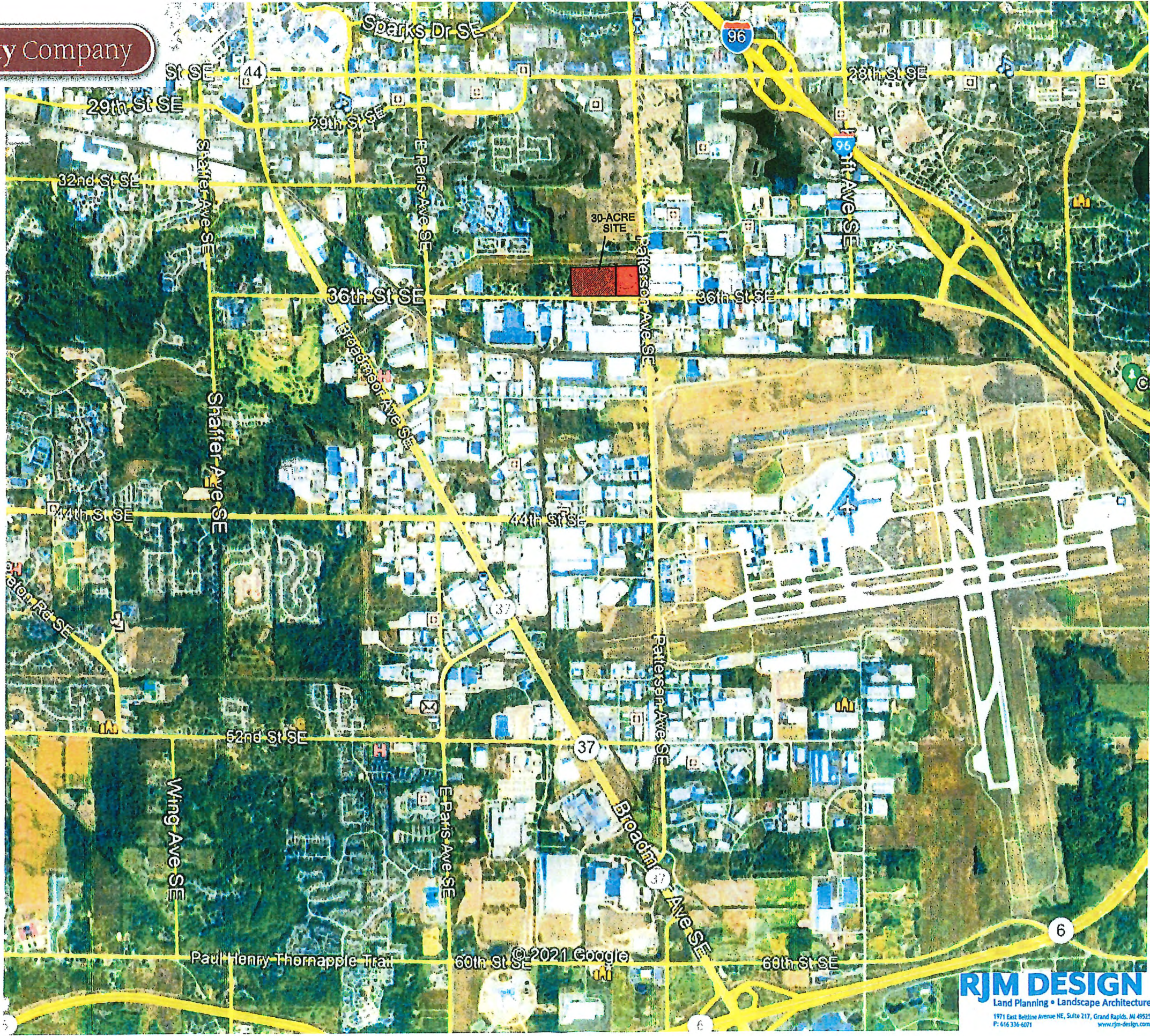
7. Discussion during work session and public hearing.

GRR Ancillary Preliminary Site Plan

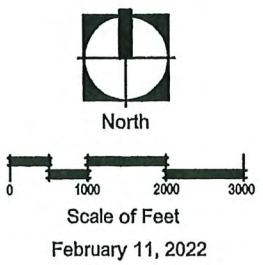
The Site Plan Layout Is Preliminary and subject to change based on Wetland Permitting and Storm Water Management Design.



Vicinity Map



RJM DESIGN
Land Planning • Landscape Architecture
1971 East Beltline Avenue NE, Suite 217, Grand Rapids, MI 49525
P: 616.336.6071
www.rjm-design.com



1979 PUD PLAN

C.C. Approval Aug. 21, 1979

60' VOLTAGE-TRANSMISSION CORRIDOR

0' SETBACK

50' SETBACK

COMMON SHIPPING & RECEIVING COURT

COMMON OFFICE ENTRANCE COURT

RETENTION POND

EXISTING CREEK

100' SETBACK

LANE BUFF

100'

200'

SHIP RECEIVING ENT

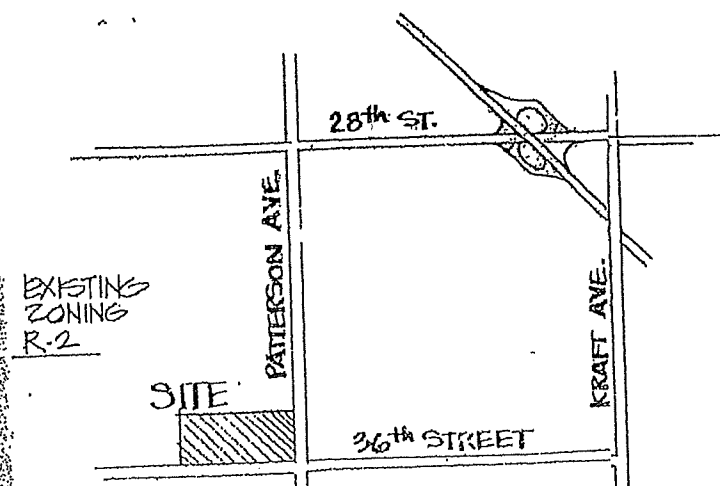
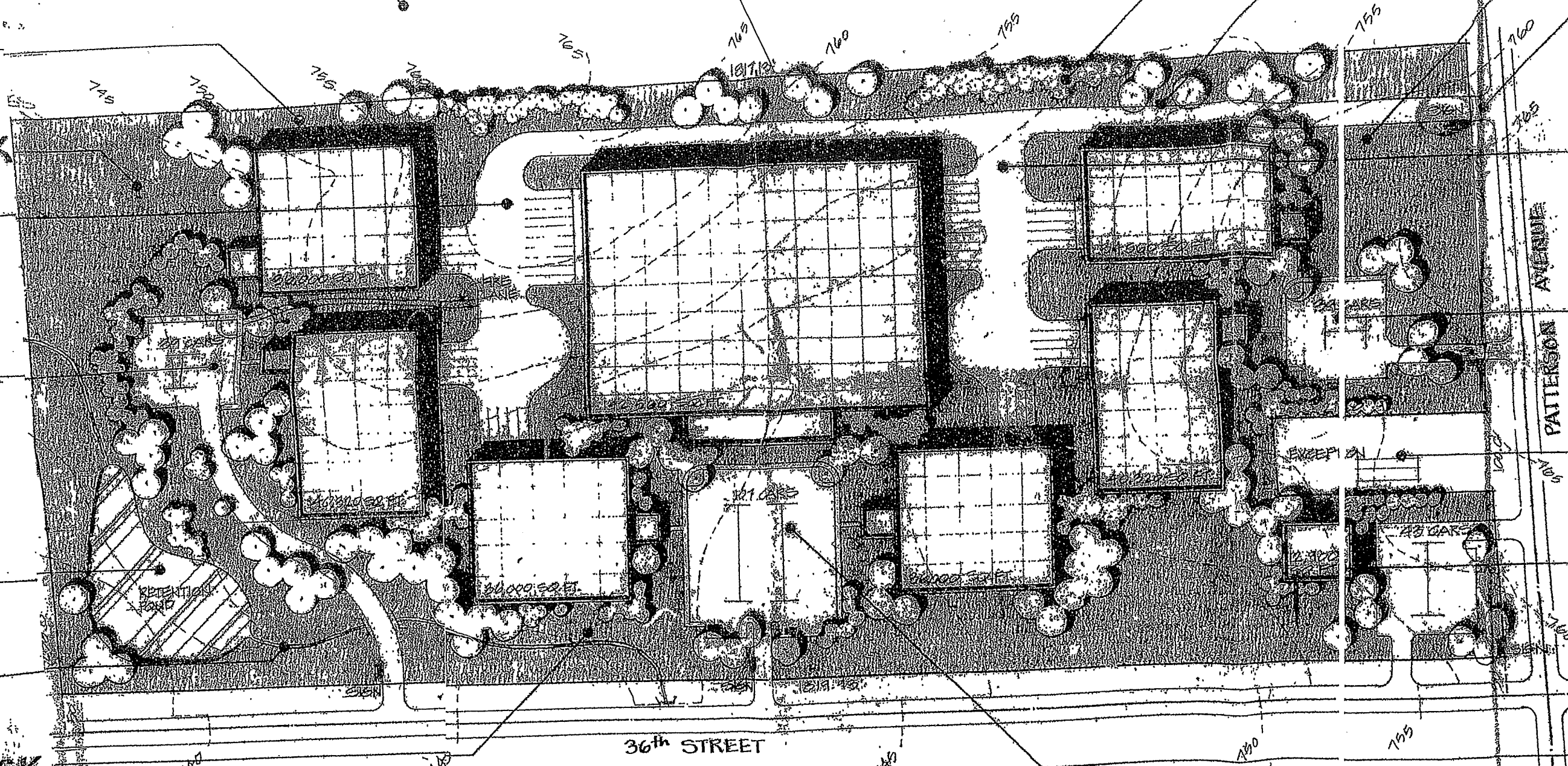
COMMON SHIPPING RECEIVING COURT

COMMON OFFICE ENTRANCE COURT

EXC (POT) OFFICE

25' OFF BUILD

COMMON OFFICE ENTRANCE COURT



PROPERTY SIZE
33.02 ACRES / 1,438,351 SQ. FT.
(INCLUDING TO & 36th ST.)
30.93 ACRES / 1,347,310 SQ. FT. (NET)

SCALE 1" = 100'
NORTH

PARKING	CARS	TRUCKS
WAREHOUSES	185	50
OFFICE	43	-
TOTAL	228	50



PLANNING STAFF RECOMMENDATION

Pung 06/23/22

PROJECT: Bretonfield Preserve Phase 6

APPLICATION: 14-22

LOCATION: 5735 Breton Avenue, SE

HEARING DATE: June 28, 2022

REVIEW TYPE: Final PUD Phase Site Plan Review

RECOMMENDATION: Recommend conditional approval of the final PUD site plan dated June 17, 2022, for Phase 6 of Bretonfield Preserve as described in Case 14-22. Approval is conditioned on the following:

CONDITION: 1. Compliance with the Amended and Restated Planned Unit Development Agreement dated April 12, 2018.

2. City staff review and approval of a final landscape plan.

3. A homeowner's association shall be created that will address maintenance of all common areas, common area amenities and sidewalk & paths adjacent to common areas. The documentation establishing the Homeowners Association shall be reviewed and approved by the City.

4. City staff approval of a final tree preservation plan and relocation plan for this phase of the development. No tree designated on the approved plan to be preserved and protected during construction shall be removed without prior approval by staff. Trees to be preserved must be fenced during construction and any of those trees that die or are removed shall be replaced by the Developer with a tree of at least 4-inch caliper of the same species or other species as approved by Staff. Any significant trees that cannot be preserved shall be replaced.

5. Street trees shall be installed in accordance with the approved landscaping plan, the Planned Unit Development Agreement, and the requirements of the Kentwood Zoning Ordinance. A performance guarantee, in an amount determined by the City, for

street tree planting will be required if installation is deferred until after construction of the homes.

6. As stated by the applicant at the Architecture Review Committee meeting on January 6, 2021, and in the letter dated June 17, 2022, homes on lots 176, 207, 208, and 226 shall be constructed utilizing an upgraded siding package.
7. As stated by the applicant in the letter dated June 17, 2022, the homes on lots 176, 207, 208, and 226 shall have a minimum of two (2) windows on the elevations facing Breton Avenue.
8. As stated in the letter dated June 17, 2022, the Developer and City shall determine the final location of the path connection and public easement from Breton Avenue to the Paul Henry Thornapple Trail.
9. As stated in the letter dated June 17, 2022, the homes on lots 208 and 226 shall have a minimum setback of fifteen (15) feet from the east property lines.
10. *I1835, Integrity 2060*, and *i1560* homes combined shall comprise no more than twenty-five (25) percent of any phase of the development; no *i1835, Integrity 2060*, or *i1560* home shall be located within 2 lots (adjacent to or across the street from) another *I1835, Integrity 2060*, or *i1560* home.
11. Compliance with all applicable City of Kentwood Engineering Department regulations and requirements.
12. Compliance with all applicable City of Kentwood Fire Department regulations and requirements.

BASIS:

1. To ensure appropriate landscaping and installation of required street trees.
2. One of the conditions of approval of the rezoning and preliminary PUD plan was review and approval by Planning Staff of a final tree preservation plan prior to construction of each phase of the development.
3. To assure adherence to all the conditions and clauses agreed upon in the Amended and Restated Planned Unit Development Agreement.
4. To ensure public access from the Breton Avenue to the Paul Henry Thornapple Trail.

5. With minimum building setbacks of fifteen (15) feet from the east property lines on lots 208 and 226, the minimum building setback (as measured at front yard setback line) from the Breton Avenue right-of-way would be nineteen (19) feet for lot 226 and twenty-three (23) feet for lot 208. The minimum setback would be twenty (20) feet for a street side yard of a corner lot.
6. Based on discussions with the City Engineer, it is staff understanding that the elevation difference between Breton Avenue and the lots on the south/west side of Sanderling Court would not be as significant as the grade difference between the lots on the south side of Sage Wing Drive and 60th Street thereby reducing the visual impact of the homes from Breton Avenue. In addition, there is an open space area with a width up to 150 feet between the lots and Breton Avenue which would also help reduce the visual impact.
7. To ensure compliance with Engineering and Fire Department regulations and requirements.
8. The development otherwise meets the requirements of the Kentwood Zoning Ordinance.
9. Applicant's representation at the work session and public hearing.
10. Discussion at the work session and public hearing.

Westview Capital, LLC
795 Clyde Ct
Byron Center, MI 49315

Joe Pung, Senior Planner
City of Kentwood
Department of Community Development
4900 Breton Ave
Kentwood, Michigan 49508

June 17, 2022

Subject: Bretonfield Preserve Phase 6, Revised Tentative Preliminary Plat/Final PUD Site Plan

Dear Joe,

Based on the review comments contained in the June 8th staff reports and the discussion at the June 14th Planning Commission Work Session, attached please find 15 copies (24" by 36") of the revised Tentative Preliminary Plat/Final PUD Site Plan and 15 copies (11" by 17") of the revised Tree Preservation & Landscape Plan for Phase 6 of Bretonfield Preserve site condominium subdivision. A brief summary of the plan changes is provided below:

Street Trees (East side of Breton Avenue)

- Street trees have been added along the East side of Breton Avenue.
- A total of 15 additional trees will also be installed within the open space area located along the East side of Breton Avenue, in addition to the existing trees that will be preserved in this area.

Open Space Area (Behind Units 190, 191 and 192)

- A 10-foot wide easement has been added along the south side of Unit 190 which will provide the homeowners association access to this open space area for maintenance purposes, if needed.
- Extension of a formal path through this open space area with a secondary connection to the Paul Henry Thornapple Trail is not desirable for the following reasons:
 - 1) Open space area is small in size (approximately ½ acre), characterized by shrub growth and is a natural low area in this portion of the site.
 - 2) Open space will serve as a storm water conveyance area that will receive storm water drainage from the rear yards of Units 190 thru 204.
 - 3) Creation of a formal path in this open space area would result in pedestrians walking within the backyards of adjacent residences.
 - 4) The 10-foot wide paved path along the West side of Breton Avenue and associated connection to the Paul Henry Thornapple Trail (behind Units 220 thru 226) was envisioned as the focal point of the subdivision to provide safe and convenient pedestrian access, from 60th Street to the Paul Henry Thornapple Trail, and eventually North to 52nd Street.
 - 5) A secondary path and connection to the Paul Henry Thornapple Trail through this open space area was not envisioned or identified on the approved amended PUD plan.

Path Connection from Breton Avenue to Paul Henry Thornapple Trail (Behind Units 220 thru 226)

- The path connection has been revised to 10-feet wide with a 20-foot wide public easement. It is understood the final location of the path and related details will be discussed between the Developer and the City with submittal of the detailed engineering plans.

Westview Capital, LLC
795 Clyde Ct
Byron Center, MI 49315

Side Home Elevations Facing Breton Avenue (Units 176, 207, 208 and 226)

- Home construction within Phase 6 will continue consistent with the Building Elevation requirements contained in the Amended and Restated PUD Agreement dated 4/12/18 with each home plan being reviewed/approved by the Community Development Department prior to construction.
- With an earlier phase of Bretonfield Preserve, the Developer also voluntarily agreed to install upgraded Bold Siding on home elevations which face Breton Avenue.
- Regarding future home construction on Units 176, 207, 208 and 226 within Phase 6, the Developer will also voluntarily agree to install a minimum of two windows per home on the side elevations of homes that face Breton Avenue.

East Side Building Setbacks for Units 208 and 226 Facing Breton Avenue

- Adjustments have been made to Sanderling Court and units located West of Breton Avenue to provide an additional 8-foot lot width for Units 208 and 226 (previously 52-wide, now 60-feet wide).
- Units 208 and 226 will now provide a minimum 15-foot East side setback (facing Breton Avenue) and a minimum 5-foot West side setback. Lot width adjustments are shown on the revised plan and the enhanced side yard setbacks are referenced in General Note 6.

Proposed Path in Open Space along East Side of Breton Avenue (Units 187, 188 and 189)

- Location of proposed path has been adjusted so as not to encroach upon Units 187, 188 and 189.

Tot Lot/Pavilion Design

- The proposed size of the pavilion (20' by 32') and tot lot (54' by 58') have been added to the plan.
- The pavilion will include picnic tables and the tot lot will include benches.
- The tot lot and pavilion area is situated approximately 75 feet from Breton Avenue with the entry point from the 10' path along the West side of Breton Avenue defined with a concrete sidewalk, decorative split rail fencing and landscaping.
- Given these design characteristics and the desire to maintain an open/natural setting for this community amenity, additional fencing is not considered desirable or necessary.

We look forward to continuing discussions of this final phase of the Bretonfield Preserve subdivision with the Planning Commission at the June 28th meeting.

If you have any questions or require additional information, please contact me at your earliest convenience.

Sincerely,



Michael West, AICP
Land Planning Project Manager
Westview Capital, LLC
795 Clyde Court SW
Byron Center, Michigan 49315
(269) 365-8548



To: Kentwood Planning Department
From: Brad Boomstra, P.E.
City Engineer
Date: June 17, 2022
Re: **Bretonfield Preserve Phase 6**
Review of Site Plan Received 6-17-2022

We have completed our review of the proposed preliminary plat site plan dated 6-17-2022 and received in our office on 6-17-2022 for the above referenced project. While there isn't sufficient information to perform a detailed review, we do offer the following comments to be incorporated as construction plans are developed.

General Comments:

1. The engineer and developer will need to coordinate the design of future Breton Avenue with the crossing of the Paul Henry Trail. The City of Kentwood is issuing an RFP for design services for the Breton Avenue Trail crossing. Coordination with the City of Kentwood's engineering consultant will be necessary.
2. Breton Avenue oversizing agreements, similar to previous phases, will be necessary as well as utility agreements.
3. One storm lateral for every two parcels is required and shall contain a wye to accommodate two (2) adjacent units. These leads can be cored and booted into the storm sewer pipe and do not need to discharge directly into a manhole.
4. Kentwood Engineering is concerned with the locations of paths in several locations. In several areas the paths are shown in low areas and crossing wetland areas. The design shall locate the paths so that they are adequately drained, and water will not cause ponding on the paths.

Utilities (Sanitary & Water):

1. The City of Grand Rapids owns, operates and maintains the sanitary sewer collection system in this portion of Kentwood. Any alterations, extensions or new connections will require approval from Grand Rapids. Construction plans (drafting standards) and specifications for the proposed sanitary sewer must conform to Grand Rapids requirements.
2. A construction agreement with the City of Grand Rapids will be required for the new sanitary sewer.
3. The City of Kentwood owns, operates and maintains the water distribution system in this portion of Kentwood. Construction plans (drafting standards) and specifications for the proposed water main must conform to Kentwood requirements.

Bretonfield Preserve Phase 5

5735 Breton Avenue

Page 2 of 2

4. Additional comments regarding utilities will be made upon receipt of construction plans.

Required Permits, Bonds, Fees [and Escrow]:

1. A Permit for an Earth Change (fee \$400) and a \$5,000 soil erosion control performance bond or an irrevocable letter of credit using the City of Kentwood format will be required through Kentwood Engineering. The Owner/Developer must sign the Permit for an Earth Change. Both the Owner/Developer and the Contractor must be named on the bond. Please contact us if you need a permit and/or a bond template form, or one can be accessed on the City's website at <http://www.ci.kentwood.mi.us> (hover over "CITY SERVICES" and "DEPARTMENTS" then click "ENGINEERING", then click the "SOIL EROSION AND STORM WATER" link near the top of the page). There is a 365-day limit to complete the work under this permit.
2. An NPDES Notice of Coverage will also be needed as the proposed area of disturbance exceeds 5 acres. At the time the Earth Change permit is issued by the Kentwood, your Notice of Coverage will immediately become effective as a "permit-by-rule" as soon as the NOC has been properly filed and the appropriate fee is paid.
3. A \$30,000 Site Grading and Stormwater Management Bond or an irrevocable letter of credit using the City of Kentwood format and an administrative fee of \$600 will be required through Kentwood Engineering. This bond is posted to assure that the plan, once approved, is constructed in the field according to that plan. Both the Owner/Developer and the Contractor must be named on the bond. Let us know if you need our bond template, or it can be accessed on the City's website.
4. For construction of this plat, an irrevocable letter of credit will be required if final plat approval is secured (or before building permits for the homes can be issued) prior to completing all construction. This is to cover all incomplete items, including placement of monumentation, watermain and sanitary sewer installation, detention basin, storm system, street construction, right-of-way stabilization, and all other appurtenances that will become a public entity.
5. Per City Resolution 37-07, an escrow fund in the amount of \$10,000 plus 3% of the estimated construction cost of the proposed public street and storm sewer improvements will be required to pay for the costs incurred by the City relating to construction engineering, testing and inspection services. A construction agreement between the Developer and the City of Kentwood will be required.

Should you have any questions regarding this department's review, please feel free to contact our office.

cc: Kentwood Engineering Permit Staff
Kent County Drain Commission
Jeff Van Laar, P.E. – Exxel Engineering – jvanlaar@exxelengineering.com
Dan Larabel – Allen Edwin – dlarabel@allenedwin.com

BENCHMARK ELEVATION 768.61
TOP OF S.W. FLANGE BOLT ON
HYDRANT (UNDER "E"), HYD. IS
N. OF N.W. HSE. COR. OF HSE.
NO. 2680 60TH STREET

BENCHMARK ELEVATION 756.39
TOP OF S.W. FLANGE BOLT ON
HYDRANT (UNDER "E"), HYDRANT
IS BETWEEN HSE. 2750 AND HSE.
2730 60TH STREET

BENCHMARK ELEVATION 789.64
TOP OF S.W. FLANGE BOLT ON
HYDRANT (UNDER "E"), HYD.
IS N. OF N.W. HSE. COR. OF
HSE. NO. 2586 60TH STREET

PP#41-18-34-100-018
2500 52ND STREET

R1-B

FUTURE DEVELOPMENT SHOWN FOR
REFERENCE, LAYOUT NOT FINAL

BLOOMING BUD LANE

SABLE RIDGE DRIVE

PLOVER DRIVE

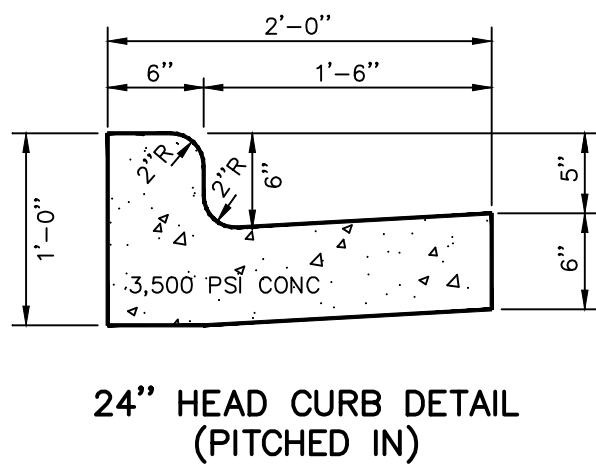
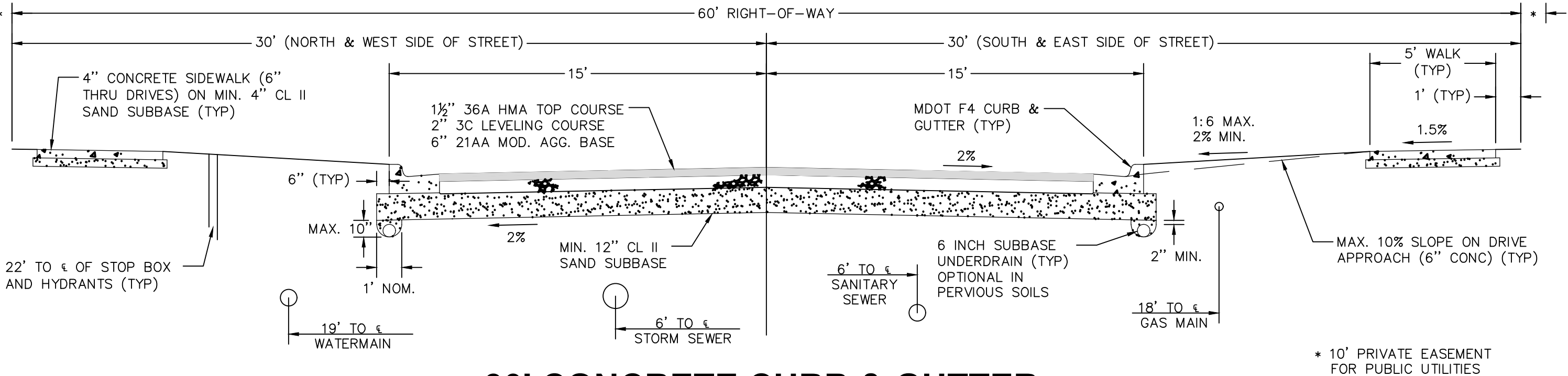
KIVERTON RIDGE DRIVE

HAWK RIDGE COURT

PAUL HENRY THORNAPPLE TRAIL

R1-B

R1-B



30' CONCRETE CURB & GUTTER SECTION

REQUIRED FOR STREETS SERVING MORE THAN 10 LOTS

GENERAL NOTES

1. DESCRIPTION:

Part of the SW 1/4 of Section 34 and part of the SE 1/4 of Section 34, T6N, R11W, City of Kentwood, Kent County, Michigan described as: Commencing at the South 1/4 Corner of said Section 34; thence N00°46'24"W 1325.44 feet along the East line of said SW 1/4 to the Northwest corner of Fields of Breton (as recorded in Instrument Number 201902140010062) and the POINT OF BEGINNING; thence S72°58'10"W 47.49 feet; thence Northwesterly 293.75 feet along a 640.00 foot radius curve to the left, the chord of which bears N30°10'47"W 291.18 feet (Central Angle = 26°17'53"); thence Northwesterly 71.29 feet along a 500.00 foot radius curve to the right, the chord of which bears N39°14'35"W 71.23 feet (Central Angle = 08°10'10"); thence S54°50'27"W 80.00 feet; thence Northwesterly 73.67 feet along a 580.00 foot radius curve to the right, the chord of which bears N31°31'14"W 73.62 feet (Central Angle = 07°16'38"); thence S89°13'36"W 299.71 feet; thence N62°01'00"W 139.52 feet; thence N32°50'07"W 273.23 feet; thence N68°33'07"W 140.25 feet; thence N71°47'33"W 47.55 feet to a Southeasterly corner of Bretonfield Preserve No. 4 (as recorded in Instrument No. 202108030086276); thence N21°08'41"E 217.20 feet along the Easterly line of said Bretonfield Preserve No. 4; thence S68°51'19"E 10.00 feet along said Easterly line; thence N21°08'41"E 94.44 feet along said Easterly line; thence N05°47'52"E 86.60 feet along said Easterly line; thence N27°07'39"W 53.59 feet along said Easterly line; thence N31°00'14"E 436.59 feet along said Easterly line to the Southwesterly line of a former railroad right of way (100 feet wide); thence S43°13'18"E 1676.54 feet along said Southwesterly line; thence Southeasterly 338.46 feet along said Southwesterly line on a 8000.00 foot radius curve to the right, the chord of which bears S42°00'35"E 338.44 feet (Central Angle = 02°25'27") to the Northeast corner of said Fields of Breton and the North line of the SW 1/4 of the SE 1/4 of said Section 34; thence N88°57'17"W 618.18 feet along said North line of the SW 1/4 of the SE 1/4 of Section 34 and the North line of said Fields of Breton to the Point of Beginning.

Contains 51 lots, 4 private parks and 22.93 acres

Part of Parcel Number: 41-18-34-300-031 Address: 5735 Breton Avenue SE

2. Mapping:

a. Boundary is based on ALTA survey by Exxel Engineering dated 3/22/2004.

b. Existing ground contours and topographical features are based on field work by Exxel Engineering dated May 15, 2017 and Feb. 2004.
c. Existing utilities are based on available construction records.

3. Zoning: RPUD-2 Single Family Residential Planned Unit Development

4. Proposed Use: Full service single family residential subdivision homes.

5. Proposed Zoning: No change

6. RPUD-2 District Regulations

	Statutory
a. Lot area	6,500 s.f. min.
b. Lot width	52' (65' for corner lots)
c. Front setback	20' min.
d. Sideyard	5' min. (12' total)
e. Sideyard for Units 208 & 226	15' min. (east), 5' (west)
f. Sideyard (street side of corner lot)	20' min.
g. Rear yard	30' min.

7. Proposed Improvements:

- Municipal water and sanitary sewer to each lot.
- Public streets will conform to the City of Kentwood specifications for subdivision streets - see cross section.
- Drainage design will conform to the City of Kentwood and KCDC specifications for subdivision drainage.
- The front 10' of each lot is reserved for installation of buried power & communication lines to service each lot.
- Construction will conform to all applicable state and local codes.
- Sidewalks will be installed at the completion of the house construction on each lot.
- Breton Ave street trees shall be planted at 50' increments consistent with PUD plan.

8. Statistical Summary: (Overall PUD)

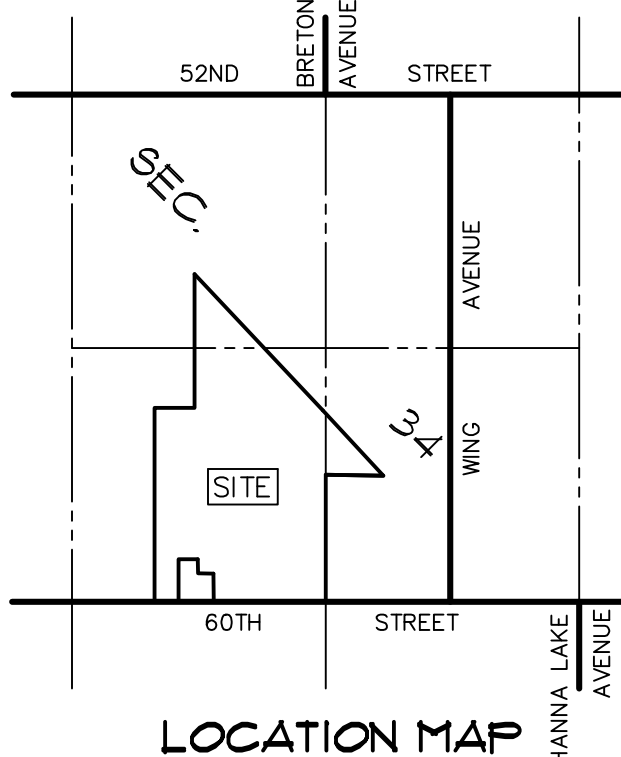
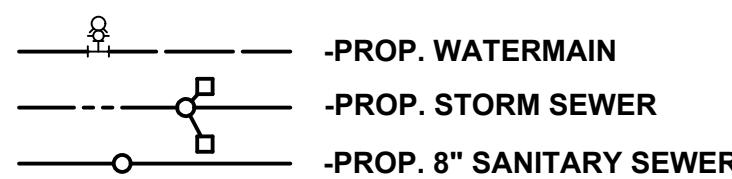
a. Total area	= 102.36 ac.
b. Total site condo units & lots	= 309
c. Total Open Space	= 31.36 Ac. (30.6%)
d. Net Area (excluding Open Space)	= 71.00 Ac.
e. Gross Density = $\frac{309 \text{ lots}}{102.36 \text{ Ac.}}$	= 3.02 lots/ Ac.
e. Net Density = $\frac{309 \text{ lots}}{71.00 \text{ Ac.}}$	= 4.35 lots/ Ac.

Phase 6 Only:

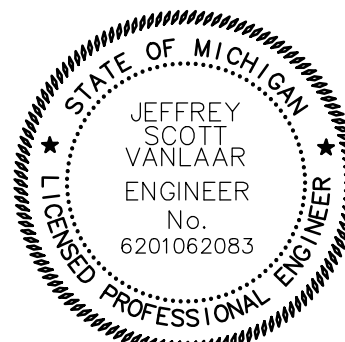
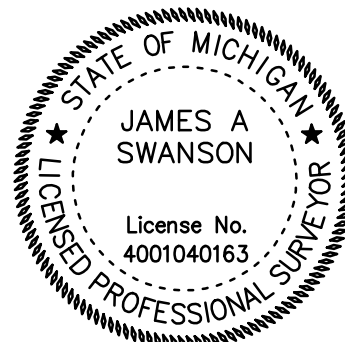
a. Total area	= 22.9 ac.
b. Total lots	= 51
c. Total Open Space	= 10.9 Ac. (47.6%)
d. Net Area (excluding Open Space)	= 12.0 Ac.
e. Gross Density = $\frac{51 \text{ lots}}{22.9 \text{ Ac.}}$	= 2.22 lots/ Ac.
e. Net Density = $\frac{51 \text{ lots}}{12.0 \text{ Ac.}}$	= 4.25 lots/ Ac.



SCALE: 1" = 100'
2' CONTOUR INTERVAL



NOTE: REFER TO AMENDED PUD SITE PLAN FOR
ADDITIONAL DETAIL OF SITE AMENITIES.



PRELIMINARY PLAT
BRETONFIELD PRESERVE PHASE 6
FOR: WESTVIEW CAPITAL, LLC
ATTN: DAN LARABEL
975 CLYDE COURT
BYRON CENTER, MI 49315
IN: PART OF THE S 1/2 OF SECTION 34, T6N, R11W,
CITY OF KENTWOOD, KENT COUNTY, MICHIGAN.

REVISIONS:

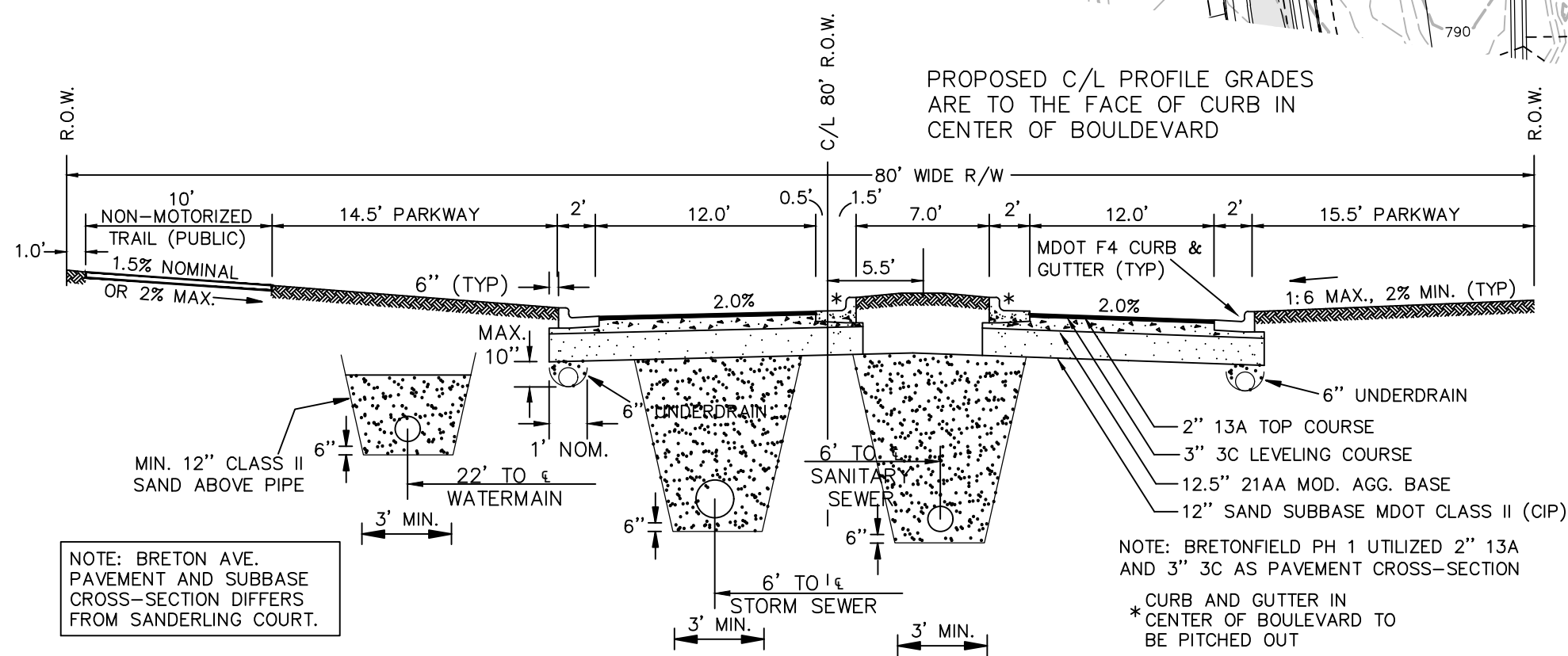
exxel engineering, inc.
planners • engineers • surveyors
5252 Clyde Park, S.W. • Grand Rapids, MI 49509
Phone: (616) 531-3660 www.exxelengineering.com

DRAWN BY: JDR
APPROVED BY: JSV
FILE NO.: 221240E

PROJ. ENG.: JSV
PROJ. SURV.: KJV
DATE: 06/17/2022

SHEET
1 of 1

BRETON AVENUE BOULEVARD CROSS SECTION (WITH ISLAND)



NOTE: BRETON AVE.
PAVEMENT AND SUBBASE
CROSS-SECTION DIFFERS
FROM SANDERLING COURT.

NOTE: BRETONFIELD PH 1 UTILIZED 2" 13A
AND 3" 3C AS PAVEMENT CROSS-SECTION
*CENTER OF BOULEVARD TO
BE PITCHED OUT

P:\Projects\2022\221240\Drawings\221240.dwg, TREES, 6/16/2022 7:16:49 AM, jroot



- EXISTING DECIDUOUS TREE
- EXISTING EVERGREEN TREE

PROPOSED STREET TREES

- RED MAPLE
- PRINCETON ELM
- AUTUMN BLAZE MAPLE
- LINDEN GREENSPIRE

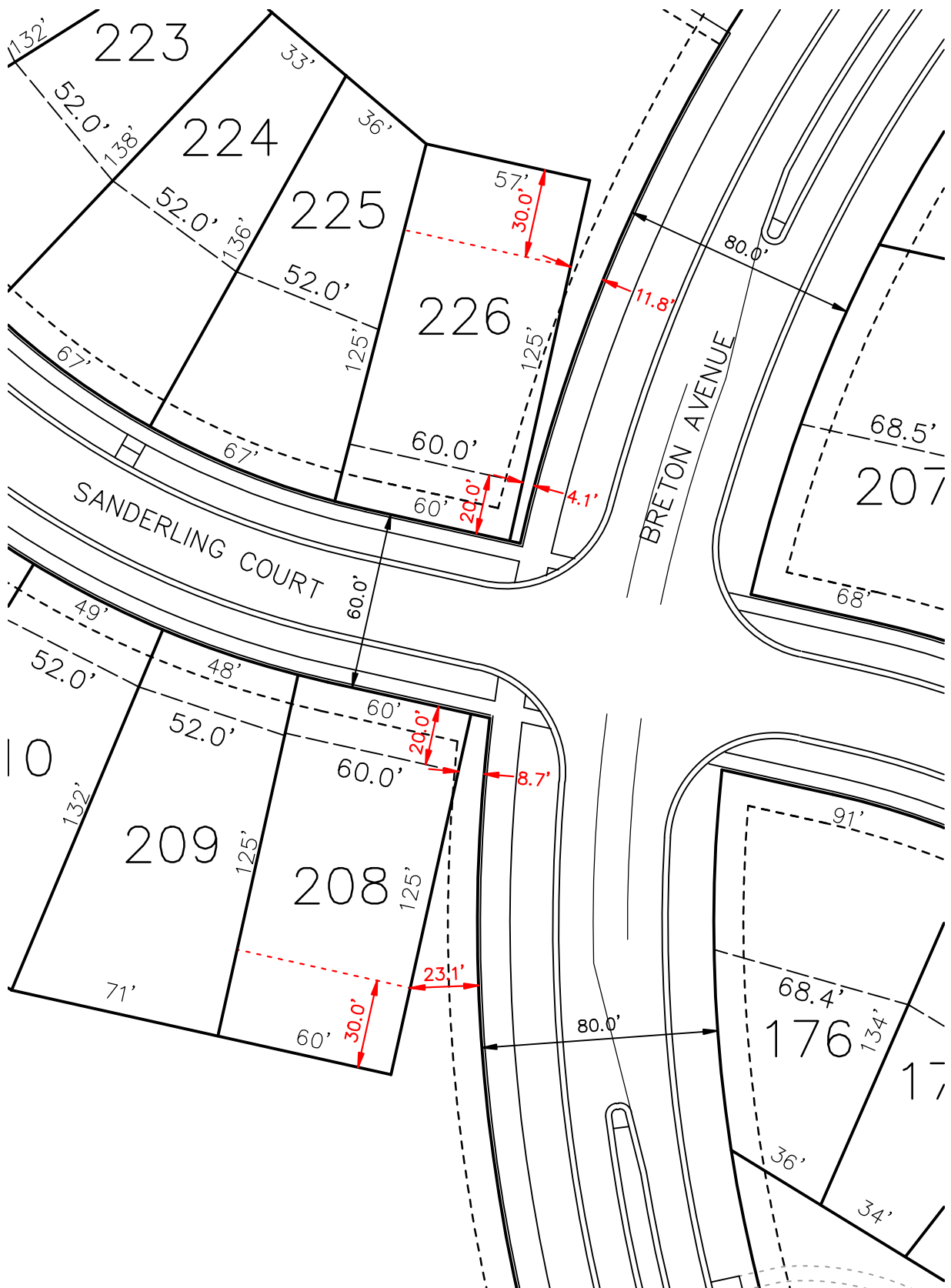


SCALE: 1" = 150'

TREE PRESERVATION AND
LANDSCAPING PLAN

BRETONFIELD PRESERVE-PHASE 6

exxel engineering, inc.
planners • engineers • surveyors
5252 Clyde Park, S.W. • Grand Rapids, MI 49509
Phone: (616) 531-3660 www.exxelengineering.com



Scale 1" = 50'

BRETONFIELD PRESERVE PH. 6



exxel engineering, inc.
planners • engineers • surveyors

5252 Clyde Park, S.W. • Grand Rapids, MI 49509
Phone: (616) 531-3660 www.exxelengineering.com

FILE NO.: 221240E

DATE: 06/22/2022



PLANNING STAFF RECOMMENDATION

Pung 06/23/22

PROJECT: Bretonfield Preserve Phase 6

APPLICATION: 15-22

LOCATION: 5735 Breton Avenue, SE

HEARING DATE: June 28, 2022

REVIEW TYPE: Preliminary Plat for a Residential Subdivision.

RECOMMENDATION: Recommend to the City Commission conditional tentative approval of the Preliminary Plat dated June 17, 2022, for Phase 6 of Bretonfield Preserve as described in Case 15-22. Approval is conditioned on the following:

- CONDITION:
1. Planning Commission final site plan approval for this phase of the planned unit development.
 2. Compliance with the Amended and Restated Planned Unit Development Agreement April 12, 2018.
 3. City staff review and approval of a final landscape plan.
 4. A homeowner's association shall be created that will address maintenance of all common areas, common area amenities and sidewalk & paths adjacent to common areas. The documentation establishing the Homeowners Association shall be reviewed and approved by the City.
 5. City staff approval of a final tree preservation plan and relocation plan for this phase of the development. No tree designated on the approved plan to be preserved and protected during construction shall be removed without prior approval by staff. Trees to be preserved must be fenced during construction and any of those trees that die or are removed shall be replaced by the Developer with a tree of at least 4-inch caliper of the same species or other species as approved by Staff. Any significant trees that cannot be preserved shall be replaced.

6. Street trees shall be installed in accordance with the approved landscaping plan, the Planned Unit Development Agreement, and the requirements of the Kentwood Zoning Ordinance. A performance guarantee, in an amount determined by the City, for street tree planting will be required if installation is deferred until after construction of the homes.
7. As stated by the applicant at the Architecture Review Committee meeting on January 6, 2021, and in the letter dated June 17, 2022, homes on lots 176, 207, 208, and 226 shall be constructed utilizing an upgraded siding package.
8. As stated by the applicant in the letter dated June 17, 2022, the homes on lots 176, 207, 208, and 226 shall have a minimum of two (2) windows on the elevations facing Breton Avenue.
9. As stated in the letter dated June 17, 2022, the Developer and City shall determine the final location of the path connection and public easement from Breton Avenue to the Paul Henry Thornapple Trail.
10. As stated in the letter dated June 17, 2022, the homes constructed on lots 208 and 226 shall have a minimum setback of fifteen (15) feet from the east property lines.
11. *I1835, Integrity 2060*, and *i1560* homes combined shall comprise no more than twenty-five (25) percent of any phase of the development; no *i1835, Integrity 2060*, or *i1560* home shall be located within 2 lots (adjacent to or across the street from) another *I1835, Integrity 2060*, or *i1560* home.
12. Compliance with all applicable City of Kentwood Engineering Department regulations and requirements.
13. Compliance with all applicable City of Kentwood Fire Department regulations and requirements.

BASIS:

1. To ensure appropriate landscaping and installation of required street trees.
2. One of the conditions of approval of the rezoning and preliminary PUD plan was review and approval by Planning Staff of a final tree preservation plan prior to construction of each phase of the development.
3. To assure adherence to all the conditions and clauses agreed upon in the Amended and Restated Planned Unit Development Agreement.

4. To ensure public access from the Breton Avenue to the Paul Henry Thornapple Trail.
5. With minimum building setbacks of fifteen (15) feet from the east property lines on lots 208 and 226, the minimum building setback (as measured at front yard setback line) from the Breton Avenue right-of-way would be nineteen (19) feet for lot 226 and twenty-three (23) feet for lot 208. The minimum setback would be twenty (20) feet for a street side yard of a corner lot.
6. Based on discussions with the City Engineer, it is staff understanding that the elevation difference between Breton Avenue and the lots on the south/west side of Sanderling Court would not be as significant as the grade difference between the lots on the south side of Sage Wing Drive and 60th Street thereby reducing the visual impact of the homes from Breton Avenue. In addition, there is an open space area with a width up to 150 feet between the lots and Breton Avenue which would also help reduce the visual impact.
7. To ensure compliance with Engineering and Fire Department regulations and requirements.
8. The development otherwise meets the requirements of the Kentwood Zoning Ordinance.
9. Applicant's representation at the work session and public hearing.
10. Discussion at the work session and public hearing.

Westview Capital, LLC
795 Clyde Ct
Byron Center, MI 49315

Joe Pung, Senior Planner
City of Kentwood
Department of Community Development
4900 Breton Ave
Kentwood, Michigan 49508

June 17, 2022

Subject: Bretonfield Preserve Phase 6, Revised Tentative Preliminary Plat/Final PUD Site Plan

Dear Joe,

Based on the review comments contained in the June 8th staff reports and the discussion at the June 14th Planning Commission Work Session, attached please find 15 copies (24" by 36") of the revised Tentative Preliminary Plat/Final PUD Site Plan and 15 copies (11" by 17") of the revised Tree Preservation & Landscape Plan for Phase 6 of Bretonfield Preserve site condominium subdivision. A brief summary of the plan changes is provided below:

Street Trees (East side of Breton Avenue)

- Street trees have been added along the East side of Breton Avenue.
- A total of 15 additional trees will also be installed within the open space area located along the East side of Breton Avenue, in addition to the existing trees that will be preserved in this area.

Open Space Area (Behind Units 190, 191 and 192)

- A 10-foot wide easement has been added along the south side of Unit 190 which will provide the homeowners association access to this open space area for maintenance purposes, if needed.
- Extension of a formal path through this open space area with a secondary connection to the Paul Henry Thornapple Trail is not desirable for the following reasons:
 - 1) Open space area is small in size (approximately ½ acre), characterized by shrub growth and is a natural low area in this portion of the site.
 - 2) Open space will serve as a storm water conveyance area that will receive storm water drainage from the rear yards of Units 190 thru 204.
 - 3) Creation of a formal path in this open space area would result in pedestrians walking within the backyards of adjacent residences.
 - 4) The 10-foot wide paved path along the West side of Breton Avenue and associated connection to the Paul Henry Thornapple Trail (behind Units 220 thru 226) was envisioned as the focal point of the subdivision to provide safe and convenient pedestrian access, from 60th Street to the Paul Henry Thornapple Trail, and eventually North to 52nd Street.
 - 5) A secondary path and connection to the Paul Henry Thornapple Trail through this open space area was not envisioned or identified on the approved amended PUD plan.

Path Connection from Breton Avenue to Paul Henry Thornapple Trail (Behind Units 220 thru 226)

- The path connection has been revised to 10-feet wide with a 20-foot wide public easement. It is understood the final location of the path and related details will be discussed between the Developer and the City with submittal of the detailed engineering plans.

Westview Capital, LLC
795 Clyde Ct
Byron Center, MI 49315

Side Home Elevations Facing Breton Avenue (Units 176, 207, 208 and 226)

- Home construction within Phase 6 will continue consistent with the Building Elevation requirements contained in the Amended and Restated PUD Agreement dated 4/12/18 with each home plan being reviewed/approved by the Community Development Department prior to construction.
- With an earlier phase of Bretonfield Preserve, the Developer also voluntarily agreed to install upgraded Bold Siding on home elevations which face Breton Avenue.
- Regarding future home construction on Units 176, 207, 208 and 226 within Phase 6, the Developer will also voluntarily agree to install a minimum of two windows per home on the side elevations of homes that face Breton Avenue.

East Side Building Setbacks for Units 208 and 226 Facing Breton Avenue

- Adjustments have been made to Sanderling Court and units located West of Breton Avenue to provide an additional 8-foot lot width for Units 208 and 226 (previously 52-wide, now 60-feet wide).
- Units 208 and 226 will now provide a minimum 15-foot East side setback (facing Breton Avenue) and a minimum 5-foot West side setback. Lot width adjustments are shown on the revised plan and the enhanced side yard setbacks are referenced in General Note 6.

Proposed Path in Open Space along East Side of Breton Avenue (Units 187, 188 and 189)

- Location of proposed path has been adjusted so as not to encroach upon Units 187, 188 and 189.

Tot Lot/Pavilion Design

- The proposed size of the pavilion (20' by 32') and tot lot (54' by 58') have been added to the plan.
- The pavilion will include picnic tables and the tot lot will include benches.
- The tot lot and pavilion area is situated approximately 75 feet from Breton Avenue with the entry point from the 10' path along the West side of Breton Avenue defined with a concrete sidewalk, decorative split rail fencing and landscaping.
- Given these design characteristics and the desire to maintain an open/natural setting for this community amenity, additional fencing is not considered desirable or necessary.

We look forward to continuing discussions of this final phase of the Bretonfield Preserve subdivision with the Planning Commission at the June 28th meeting.

If you have any questions or require additional information, please contact me at your earliest convenience.

Sincerely,



Michael West, AICP
Land Planning Project Manager
Westview Capital, LLC
795 Clyde Court SW
Byron Center, Michigan 49315
(269) 365-8548

STAFF REPORT: June 6, 2022
PREPARED FOR: Kentwood Planning Commission
PREPARED BY: Lisa Golder
CASE NO.: 16-22 Kum and Go Major Change to a Commercial PUD

GENERAL INFORMATION

APPLICANT: Kum & Go L.C. rep by: Michael McPherson, Atwell LLC
Ryan Halder 12745 23 Mile Road Suite 200
1459 Grand Ave Shelby Twp MI 48315
Des Moines IA 50309

STATUS OF APPLICANT: Owner and owner's representative

REQUESTED ACTION: Applicant is requesting a Major Change to a Commercial Planned Unit Development located at 5215 Kalamazoo Avenue, 1570 52nd Street and 1549 Kalamazoo Avenue (part)

EXISTING ZONING OF SUBJECT PARCEL: CPUD Commercial Planned Unit Development

GENERAL LOCATION: 5215 Kalamazoo Avenue, 1570 52nd Street and 1549 Kalamazoo Avenue (part)

PARCEL SIZE: 2.28 Acres

EXISTING LAND USE ON THE PARCEL: Existing automotive repair, two single family residential homes

ADJACENT AREA LAND USES: N: 52nd Street and gas station
S: Duplex
E: Kalamazoo Avenue, vacant land
W: Single family homes

ZONING ON ADJOINING PARCELS: N: C-2 Commercial
S: R-2 Two Family Residential
E: C-4 Office
W: R1-C Single Family Residential

Compatibility With Master Plan

The Master Plan recommends commercial use for this location. The 2010 Master Plan incorporated a charrette process to obtain neighborhood input into the development of the major intersections along the Kalamazoo corridor. Neighborhood commercial uses or office uses were recommended. The 2020 Master Plan recommends small-scale development to serve the immediate residents as well as convenience for commuters. The 2020 Master Plan also establishes some development concepts for the intersection of 52nd Street and Kalamazoo Avenue, as follows:

- Establish opens space (green or hardscape plaza) at the intersection corners
- Provide parking behind buildings, if possible
- No expansion of commercial or office
- Landscaped buffers/screens for residential uses adjacent to commercial or office uses
- Area must be walkable
- Shared access must be provided between buildings
- Ensure that access to public streets works with boulevard design of Kalamazoo Avenue.
- A traffic analysis will be required with the development.
- Commercial uses to serve the immediate area or commuters
- Plan is long term and marketable.

Relevant Zoning Ordinance Sections

Section 12.05 addresses the purpose and development requirements for the CPUD district.

Zoning History

The site was initially developed as a gas station, and then approved as an automotive repair establishment called Don and Sons in 1995. At one time the site was also considered for a pharmacy. In 2003 the Planning and City Commissioners approved a city-sponsored rezoning of the site to Commercial Planned Unit Development (CPUD). The CPUD approval was based on a PUD Statement dated February 6, 2003 and February 24, 2003 (See attached PUD Statement and City Commission approval). The rezoning was for property of approximately 3 acres in size; this is larger than the acreage proposed for the Kum and Go development because the rezoning included the 52nd Street and Kalamazoo Avenue rights of way (extended to the centerline). Excluding the rights of way, the proposed site is 2.28 acres in area.

The 2003 PUD Statement specified a preference for Permitted or Special Land Uses that would not generate significant traffic in the peak hour of operation for the intersection. The CPUD also specifies that the hours of operation for any use be compatible with residential uses, from approximately 7:00AM-10:00PM.

The 2003 PUD statement was intended to be utilized in the review of future proposed plans for the corner. A plan for Thompsons Auto Repair was approved by the city in 2004. The use was seen as an interim measure prior to a development project that would incorporate all of the land zoned Commercial Planned Unit Development.

The proposed Kum and Go plan was reviewed by the Land Use and Zoning Subcommittee in January of 2022; the Committee was generally in favor of the concept.

TECHNICAL INFORMATION

Street and Traffic

The site is located at the southwest corner of 52nd Street and Kalamazoo Avenue. Kalamazoo Avenue at the 52nd Street intersection is a 4-lane road that includes two northbound and two southbound lanes and a right turn lane at 52nd Street. Lanes for indirect left turns are located north and south of the intersection; only the north indirect left crossover is suitable for truck traffic. A grass median prohibits left turns into and out of the proposed Kum and Go development from Kalamazoo Avenue. 52nd Street is a two-lane road with a center turn lane. At the intersection with Kalamazoo Avenue, 52nd Street allows for direct left hand turns for both northbound and southbound traffic. There are dual left turns onto Kalamazoo from west-bound 52nd Street.

The Kum and Go development proposes two driveways. The Kalamazoo driveway allows right-in and right-out movements only. The width of the driveway is 50 feet. The applicant shall provide turning templates to confirm the need for this width. Pavement markings may also be employed to ensure that vehicles entering and exiting the site remain in their designated lanes; a deceleration lane may be necessary to allow traffic to get out of the through lanes and make a turn into the site at a safe speed. Slower speeds are also safer for pedestrians walking through the area.

The 52nd Street driveway allows full movement of traffic in and out of the site. The applicant must address any issues this may create with left turns into Newcastle Drive. The proposed 52nd Street driveway is also 55 feet in width. Again, the applicants shall provide information to justify the need for this width.

Trip Generation and Traffic Analysis

Applicant has submitted a Traffic Impact Study as required by the Zoning Ordinance and by the conditions of the CPUD approval for this site. The traffic analysis was based on 2018 traffic volumes of 20,065 vehicles per day on Kalamazoo Avenue and 2016 traffic volume of 16,469 vehicle trips per day on 52nd Street. The analysis reviewed current conditions, and then estimated future conditions, given the proposed gas station use. The study concluded that all intersection approaches continue to operate at an acceptable level of service (LoS D or better). The study also identified between 16% and 35% of the traffic volume as “pass-by” trips, or trips by vehicles that were already on the roadway and therefore not created by the new development. In addition, the study concluded that there were adequate gaps in through traffic to allow turns left into and out of the 52nd Street driveway without excessive queueing. A summary of the traffic analysis is attached. The city is working with a traffic consultant to review the conclusions of the traffic study and to

determine whether the Commission should require the developer to make additional improvement or changes to the proposed plan.

Staff Review

1. The Kum and Go development is proposed on a 2.28 acre property at the southwest corner of 52nd Street and Kalamazoo Avenue. The property owned by the applicant also includes an attached residentially-zoned property with frontage on Newcastle Drive (not part of the 2.28 acres). The parcel is intended to be split from the CPUD parcel and developed as single family residential.
2. The Kum and Go store is a gas station with 6 pumps (12 pumping stations). The proposed floor plan includes food preparation and sales, a seating area, and retail area for convenience items. The applicant has indicated their desire sell packaged beer and wine at the site. The applicant shall provide a floor plan for review.

Major Change to an approved PUD plan

3. In 2003 the Planning and City Commission approved a city-sponsored rezoning of the site to Commercial Planned Unit Development (CPUD). The CPUD approval was based on a PUD Statement dated February 6, 2003 and February 24, 2003 (See attached PUD Statement and City Commission approval). The rezoning was for property of approximately 3 acres in size; this is larger than the acreage proposed for the Kum and Go development because the rezoning included the 52nd Street and Kalamazoo Avenue rights of way (extended to the centerline). Excluding the rights of way, the proposed site is 2.28 acres in area.
4. The 2003 PUD Statement specified a preference for Permitted or Special Land Uses that would not generate significant traffic in the peak hour of operation for the intersection. The CPUD also specifies that the hours of operation for any use be compatible with residential uses, from approximately 7:00AM-10:00PM. The applicant is proposing a 24 hour operation.
5. The 2003 PUD statement was intended to be utilized in the review of future proposed plans for the overall site. A plan for Thompsons Auto Repair was approved by the city in 2004. The use was seen as an interim measure prior to a development project that would incorporate all of the land zoned Commercial Planned Unit Development.
6. The 2003 PUD Statement states that the building roof elevation should be low in profile and residential in style with a peaked roof or another roof style to be approved by the Planning and City Commissions. The building was not to overpower the residential uses to the west. The proposed Kum and Go building is between 18-21 feet in height; it should not overpower the residential uses to the west. The building itself is 87 feet from the common property line with the single family residential homes on Newcastle Drive. The roof style is not a peaked roof; this roof type would likely result in a taller roofline.

The gas station includes a canopy over the pump islands. The canopy is between 17'-21' in height.

7. The 2003 PUD Statement indicates that servicing the building would occur between the hours of 9:00AM-3:00PM. The applicant is proposing servicing between 8:00AM and 8:00PM. The 2002 PUD Statement also indicates that service traffic would be kept as far away from residential uses as possible. The proposed gas station extends the driveway around the west side of the building. It may be possible to relocate the building further to the west in order to remove the service driveway from the west side of the site. This would also increase the width of the average greenbelt along the public streets to the ordinance-required 30' average width.

Site Plan Review

8. Access to the site for fuel trucks will be from Kalamazoo Avenue only. Since 52nd Street is not a truck route, commercial trucks or fuel trucks delivering to the site would have to travel south to the site from 44th Street, or north to the site from the M-6. A crossover exists north of the Kalamazoo/52nd Street intersection to allow northbound vehicles make an indirect left to access the Kalamazoo driveway. The crossover south of the proposed gas station is about 2000 feet to the south.
9. The applicant shall provide information on truck circulation through the site. Trucks must both enter and exit from the Kalamazoo Avenue driveway.
10. The lighting plan does not meet city standards; the lighting under the canopy exceeds 70 footcandles in some places; a maximum of 22 footcandles is permitted.
11. The landscape plan appears to meet city requirements. It is unclear where gravel is being used in the planting beds.
12. The applicant has indicated the desire for exterior sales of ice, propane, firewood, packaged water and other similar materials. Outside storage of merchandise is not permitted. Other fueling stations have allowed the sale of some of these items (ice, propane, etc.) on the sidewalk adjacent to the building. The location of these sales, if any, should be noted on the site plan.
13. The proposed convenience store building will be 5,620 square feet in area. The use will require 24 parking spaces, provided as per the proposed plan.
14. The applicant shall indicate how the proposed building meets the architectural guidelines of Section 12.05 7 of the Zoning Ordinance.
15. The site has been a gas station and an auto repair establishment for many years; the status of the any known remediation efforts should be provided.

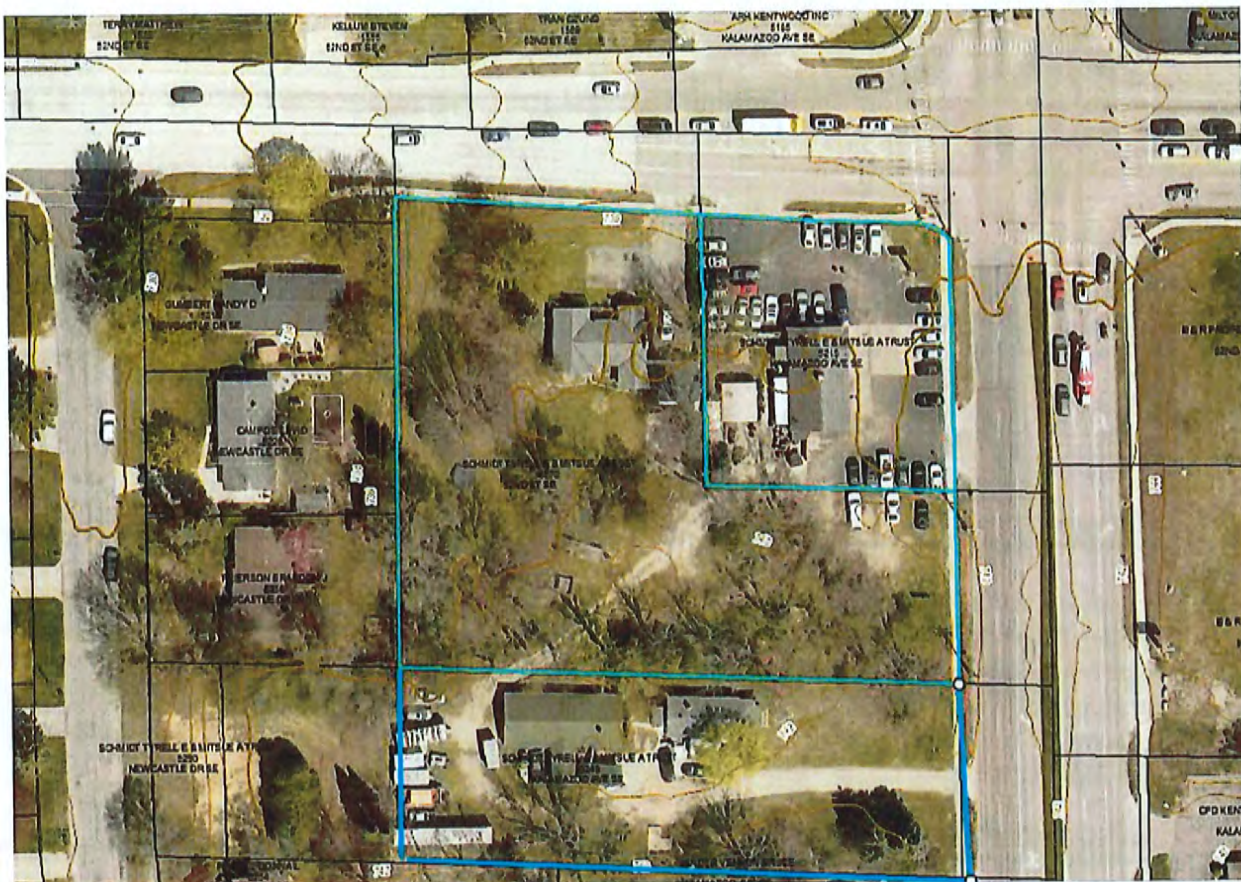
- Eliminates interim use that has been challenging to keep in line with property maintenance requirements
- Low profile building

Issues:

- Lighting standard not met
- Merchandise being sold outside building
- Some inconsistencies with CPUD Plan from 2003—brick façade, hours of operation
- Driveways too wide
- Traffic study to be reviewed by consultant



Proposed Kum and Go location



Existing Conditions



CONSULTING. ENGINEERING. CONSTRUCTION.

May 18, 2022

City of Kentwood
4900 Breton Ave SE
Kentwood, MI 49508

Subject: Site Plan Review
Applicant: Kum & Go, LLC.

INTRODUCTION

Kum & Go, L.C. (The developer) has an option to purchase a +/-2.52-acre property at the southwest corner of 52nd Street SE and Kalamazoo Avenue. The developer proposes to construct a convenience store with vehicle fueling to include alcohol sales and 24-hour operations. The parcels are within a Commercial Planned Unit Development (CPUD) that is further described in a PUD Statement memo dated February 6, 2003. The following is a written description to be included as part of the supporting application materials for the development including commentary on standards outlined in the PUD memo.

NATURE OF PROPOSED USE

The proposed development will include the sale of typical package convenience store items, preparation and sale of food, and alcohol sales. Exterior sales may include ice, propane, firewood, packaged water, road salt, and other similar items typical of exterior sales at a convenience store with vehicle fueling. The vehicle fueling is proposed with six fuel dispensers that can accommodate up to twelve vehicles. The development is proposed to have 24 hours of operations. Up to five employees may occupy the facility. Vehicular traffic will consist of typical passenger traffic with corresponding peaks during morning, lunch, and afternoon commutes.

SITE AND BUILDING LAYOUT

The proposed development sites the convenience store building on the western-central portion of the property. Six fuel dispensers under a canopy will be centrally located to the east of the building. Parking is located to the north and east of the building. Driveway entrances are proposed along 52nd Street SE and Kalamazoo Avenue.

The proposed floor plan includes food preparation and sales, a seating area, and retail area for typical convenience store items in the front of the building. Storage areas are located in the rear of the building.

Landscaped areas are proposed along each of the street frontages and along property boundaries abutting residential zoning. The dumpster area will be screened in accordance with applicable ordinances. Six foot vertical screening wall or fence is proposed along the property boundaries adjacent to residential zoning.

Adjacent property uses include a gasoline dispensing station to the north and residential use to the northwest, beyond 52nd Street SE, vacant land, and a medical office to the east, beyond Kalamazoo Avenue, and residential use to the south and west.

SITE PLANS, BUILDING ELEVATIONS AND FLOOR PLAN

Please refer to the enclosed site plan set. This set contains all the information required per the City site plan review information requests within the zoning ordinance (Section 14.04).

CPUD STANDARDS

The following provides a summary and responses to the items outlined in the February 6, 2003 PUD Statement for the Southwest Corner of 52nd Street and Kalamazoo Avenue from Lisa Golder, Economic Development Planner to the Kentwood Planning Commission.

Standard #1. Compliance with the Master Plan.

Master Plan recommends commercial and office use for the site. The proposed CPUD zoning permits uses in both the C-4 Office Zones as well as the C-2 Community Commercial zone. The Master Plan also recommends neighborhood commercial uses that are walkable for the surrounding residences.

The zoning ordinance for Commercial PUD further states that any "permitted use" within C-2 and C-4 districts and customary accessory uses as permitted in the C-2 or C-4 districts are permitted uses within a CPUD. Further, uses permitted by Special Land Use in the C-2 and C-4 districts may be permitted when all applicable standards as cited in Chapter 15 are met. The proposed convenience store is a permitted use under C-2 and vehicle fuel stations are permitted under C-2 with a Special Land Use determination. The site plan layout is designed for pedestrian access from each of the street frontages.

Standard #2. Use Restrictions.

It is the intent of the City of Kentwood to exercise flexibility in the review and analysis of any proposed use of the property. The uses proposed for the CPUD should be permitted or Special Land Uses that are mid to low traffic generators that do not create high traffic volumes during peak hours for the road system (7:00-8:30 AM and 4:00-6:00 PM). The City will require a traffic analysis to determine the impact of the use on the surrounding road system, to evaluate traffic flow, and to determine the changes to the intersection that may be required to accommodate the use.

The proposed use is anticipated to support, but not significantly increase, existing traffic patterns. As noted in the Traffic Impact Study, a majority of the traffic that will visit the property will be generated by "pass-by" trips that do not add new traffic to the area. A review of proposed site conditions and existing road and intersection conditions indicated that no modifications are required to maintain the existing acceptable operation.

Standard #3. Site Requirements.

All three parcels must be combined into one parcel under a single ownership. The westerly portion of the southernmost parcel shall be split and combined with the small residential lot on Newcastle to create a usable residential lot on that street.

The parcels will be split and combined as required and depicted on the site plans. The lot split/combination is to be conducted concurrently with the site plan submittal.

Standard #4. Driveways.

All existing driveway on Kalamazoo and 52nd Street will be removed. One driveway will be permitted on Kalamazoo and one driveway will be permitted on 52nd Street. The driveway on 52nd Street shall be located on the westernmost side of the development and is anticipated to allow both left and right turns in and out of the driveway. The driveway on Kalamazoo shall either align with the driveway to the east, or shall be located further south than the driveway to the east. A traffic impact analysis will be required in accordance with Section 13.02 of the Kentwood Zoning Ordinance to evaluate traffic flow and final driveway location/alignment.

The proposed layout follows the requirements to remove all existing driveways and place driveways as described at the westernmost area along 52nd Street and the southeastern portion along Kalamazoo. As described in the Traffic Impact Study, a review of proposed site conditions including driveway locations, and existing road and intersection conditions, indicated that no modifications are required to maintain the existing acceptable operation.

Standard #5. Architectural Features.

Building roof elevation shall be in a low profile residential style with peaked roof or another roof style approved by the Planning and City Commissions. The buildings shall not overpower the residential uses to the south and west. The building shall be primarily of brick on all sides, with architectural features that create interest and break up large expanses of wall area.

The proposed building height is consistent with a single-story commercial building with a top of parapet height of 18 feet with architectural features at the entrance extending to 21' and a feature wall up to a height of 22'-8". The proposed features and windows break up large expanses of wall area. Refer to the included color elevation in the site plan for additional information

Standard #6. Site Design.

The building(s) shall be designed to avoid the appearance of overcrowding on the site and the adjoining homes. At least 25% green space shall be provided on the site, but less green space will be considered if other site amenities area provided. The main building shall be limited to approximately 11,000 square feet in area, with an additional 6,000 square foot office building permitted along the south side of the site. The Police and Fire Departments must review and approve the building location for the safety and security of the building occupants and area residents.

The building must be residential in scale and design. Parking shall be held to a minimum for the planned and potential uses; deferred parking shall be encouraged as long as it can be shown that the deferred parking can be provided at a later date on site needed without impacting the total required green space. Servicing/loading within the buildings shall occur between the hours of 9:00 AM to 3:00 PM. Circulation around the site shall ensure separation of pedestrian and service vehicles, and shall provide easy access to sidewalks on Kalamazoo Avenue and 52nd Street. The site will allow for bicycle access and parking. Service traffic shall be kept as far away from residential uses as possible.

The proposed development requested an extension of the servicing/loading hours to 8:00 AM to 8:00 PM based on the additional landscape buffering and screening to be installed.

Sidewalks shall be constructed on the subject property outside the public right-of-way and public easements will be provided to allow public access to the sidewalks.

The proposed 5,620 square foot building is centrally located with landscaping around the perimeter. At least 25% of green space is provided with the proposed layout. The proposed building is

consistent in size and scale for a single-story commercial building and is further depicted in the included building elevations. Sidewalks provided access from 52nd Street SE and Kalamazoo to the proposed building with designated crosswalks when crossing internal driving lanes. A bicycle rack is provided near the proposed building.

Standard #7: Hours of Operations.

The hours of operation for the businesses within the PUD shall be limited time that are compatible with residential uses, from approximately 7:00 AM to 10:00 PM.

The proposed development requests approval 24 hours of operation. The proposed layout includes landscape and wall or fence screening along the property boundaries that about residential use. Proposed lighting will include shielding and placement to control light intrusion to adjacent residential properties.

Standard #8: Lighting Standards.

The site lighting shall be in compliance with Chapter 20 of the Kentwood Zoning Ordinance. After the businesses are closed daily, the lighting for the site shall be reduced to serve for security purposes only.

The proposed site design shall comply with the requirements of the Kentwood Zoning Ordinance. Should 24 hour operation be approved, lighting design will include consideration for control of light intrusion to adjacent properties.

Standard #9: Landscaping/Parking

Landscaping shall be designed to provide a pleasing, permanent look for this corner. Parking shall be limited in the front setback area along the 52nd and Kalamazoo rights of way. Parking shall be screened by landscaping and berming. The landscaping shall be low enough to provide visibility from the street for both customers and employees. Landscaping shall meet or exceed Zoning Ordinance standards along the western and southern property lines. Landscaping shall be maintained at all times, including replacement of dead and damaged plants and regular to maintain proper plant heights.

The proposed landscaping adheres to the requirements of this section and comply with buffer requirements for commercial properties. Refer to the included landscape plan and details for additional information.

Standard #10: Adjacent uses.

The site shall be designed with the clear intention that commercial uses will not be extended further south and west of the parcel.

The proposed layout includes perimeter landscaping and screening along with curb surround the developed area in a manner with a clear intention the site will not be extended further than proposed.

MEMO

VIA EMAIL mmcpherson@atwell-group.com

To: Michael McPherson, PE, CPESC
Atwell, LLC

From: Jacob Swanson, PE
Fleis & VandenBrink

Date: May 18, 2022

Re: **Proposed Convenience Store with Fuel**
Kentwood, Michigan
Traffic Impact Study

1 INTRODUCTION

This memorandum presents the results of the Traffic Impact Study (TIS) for a commercial development in the City of Kentwood, Michigan. The project site is located in the southwest quadrant of the Kalamazoo Avenue & 52nd Street intersection, as shown in the attached **Figure 1**. The proposed development plan includes the construction of convenience store with fueling stations. Site access is proposed via two (2) driveways; one (1) on Kalamazoo Avenue and one (1) on 52nd Street. Both of the study roadways are under the jurisdiction of the City of Kentwood; additional roadway information is summarized in **Table 1**.

Table 1: Roadway Information

Roadway Segment	Kalamazoo Avenue	52 nd Street
Number of Lanes	4 (Median Divided)	4 (2 each direction E. of Kalamazoo Ave.) 3 (1 each direction & TWLTL W. of Kalamazoo Ave.)
Functional Classification	Other Principal Arterial	Minor Arterial
Speed Limit	40 mph	40 mph (East of Kalamazoo Ave.) 35 mph (West of Kalamazoo Ave.)
Traffic Volumes	20,065 vpd (2018)	16,469 vpd (2016)

The scope of the study was developed based on Fleis & VandenBrink's (F&V) understanding of the development program, accepted traffic engineering practice, and methodologies published by the Institute of Transportation Engineers (ITE). The study analyses were completed using Synchro and SimTraffic (Version 11) traffic analysis software.

2 DATA COLLECTION

F&V subconsultant Gewalt Hamilton Associates, Inc. (GHA) performed 24-hour Turning Movement Counts (TMCs) at the signalized study intersection of Kalamazoo Avenue & 52nd Street on Thursday, April 15th, 2021. Additional TMC data was collected during the AM (7:00 AM–9:00 AM) and PM (3:00 PM–6:00 PM) peak periods at the signalized study intersections of SB Kalamazoo Avenue & NB-to-SB Crossover and NB Kalamazoo Avenue & SB-to-NB Crossover on Thursday, April 14th, 2022. The AM and PM peak hours were identified to generally occur between 7:15 AM to 8:15 AM and 3:15 PM to 4:15 PM for a typical weekday.

2960 Lucerne Dr. SE
Grand Rapids, MI 49546
P: 616.977.1000
F: 616.977.1001
www.fveng.com

Due to the impact of COVID-19, traffic volumes data may not necessarily be representative of typical pre-COVID traffic volumes. Therefore, historic traffic data was obtained from the Michigan Department of Transportation (MDOT) Transportation Data Management System (TDMS), and a conservative 0.5% annual growth rate was applied to the traffic volumes to calculate the *expected* 2022 traffic volumes. The expected traffic volumes were then compared to the collected 2021 and 2022 traffic volumes to calculate COVID adjustment factors. The results of the comparison indicated that the 2022 traffic volumes were higher than the *expected* 2022 traffic volumes. Therefore, COVID adjustment factors were only calculated for the 2021 traffic volumes; the applied COVID adjustment factors are summarized in **Table 2**.

Table 2: COVID Adjustment Factors

AM Peak Hour	PM Peak Hour
+23%	+7%

The COVID adjustment factors were then applied to the 2021 peak hour collected data to reflect the "existing – adjusted" traffic volumes, as shown on the attached **Figure 3**. F&V also collected an inventory of existing lane use and traffic controls at the study intersections, as shown in the attached **Figure 2**. Additionally, F&V obtained the current traffic signal timing information from the City of Grand Rapids who maintains the signals for the City of Kentwood.

3 EXISTING CONDITIONS

Existing peak hour vehicle delays and Levels of Service (LOS) were calculated at the study intersections using Synchro (Version 11) traffic analysis software. This analysis was based on the existing lane use and traffic control shown on the attached **Figure 2**, the "existing – adjusted" peak hour traffic volumes shown on the attached **Figure 3**, and the methodologies presented in the *Highway Capacity Manual 6th, Edition* (HCM).

Typically, LOS D is considered acceptable, with LOS A representing minimal delay, and LOS F indicating failing conditions. Additionally, SimTraffic network simulations were reviewed to evaluate network operations and vehicle queues. The results for the existing conditions analysis are attached and summarized in **Table 3**.

Table 3: Existing Intersection Operations

Intersection	Control	Approach	Existing Conditions			
			AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS
10	Signalized	WBL	0.8	A	0.9	A
		NB	Free			
		SB	2.2	A	3.2	A
		Overall	2.1	A	2.9	A
20	Signalized	EBL	54.0	D	52.8	D
		EBTR	39.3	D	42.1	D
		WBL	51.8	D	50.7	D
		WBTR	36.2	D	45.1	D
		NBT	20.2	C	23.6	C
		NBR	20.4	C	21.8	C
		SBT	18.9	B	23.3	C
		SBR	17.5	B	20.3	C
		Overall	28.6	C	33.2	C
30	Signalized	WBL	0.7	A	0.8	A
		NB	2.6	A	3.5	A
		SB	Free			
		Overall	2.5	A	2.8	A

The results of the existing conditions analysis indicates that all study intersection approaches and movements are currently operating acceptably at LOS D or better during both peak periods. Additionally, review of SimTraffic network simulations indicates acceptable operations, with the majority of vehicles at the signalized study intersections observed to be serviced within each cycle length.

4 SITE TRIP GENERATION

The number of peak hour (AM and PM) and daily vehicle trips that would be generated by the proposed development was forecast based on data published by ITE in the *Trip Generation Manual, 11th Edition*. The proposed development includes a gas station with 12 vehicle fueling positions (VFP) and an approximately 5,620 GSF (4,496 GFA) convenience store. The trip generation used in the analysis is summarized in **Table 4**.

Table 4: Site Trip Generation Summary

Land Use	ITE Code	Amount	Units	Average Daily Traffic (vpd)	AM Peak Hour (vph)			PM Peak Hour (vph)		
					In	Out	Total	In	Out	Total
Gas Station with Convenience Market	945	12	VFP	3,086	162	162	324	137	136	273
Pass-By	76% AM, 75% PM			2,330	123	123	246	102	102	204
New Trips				756	39	39	78	35	34	69

As is typical of commercial developments, a portion of the trips generated are from vehicles on the adjacent roadway and will pass the site on the way from an origin to an ultimate destination. Therefore, not all traffic at the site driveways are necessarily new traffic added to the street system. This percentage of the trips generated by the development are considered "pass-by" trips and do not add new traffic to the adjacent street system. These trips are therefore reduced from the total external trips generated by a study site. The percentage of pass-by trips used in this analysis was determined based on the rates published by ITE in the *Trip Generation Manual, 11th Edition*.

5 SITE TRIP DISTRIBUTION

The vehicular trips that would be generated by the proposed development were assigned to the study roads based on the proposed site access plan and driveway configurations, the existing peak hour traffic patterns in the adjacent roadway network, and the methodologies published by ITE. The ITE trip distribution methodology assumes that new trips will enter the network and access the development, then leave the development and return to their direction of origin, whereas pass-by trips will enter and exit the development in their original direction of travel. The site trip distributions used in the analysis is summarized in **Table 5**.

Table 5: Site Trip Distribution

New Trips					Pass-By Trips	
AM	PM	To/From	Via	Direction	AM	PM
22%	29%	North	Kalamazoo Avenue	Northbound	36%	30%
35%	31%	South	Kalamazoo Avenue	Southbound	28%	34%
22%	19%	East	52 nd Street	Eastbound	20%	16%
21%	21%	West	52 nd Street	Westbound	16%	20%
100%	100%	Total			100%	100%

The vehicular traffic volumes shown in **Table 4** were distributed to the roadway network according to the distribution shown in **Table 5**. The proposed development is anticipated to be open and operation within the next 12 months; therefore, the existing conditions traffic volumes were assumed to be equal to the background traffic volumes without the proposed development. Therefore, the site-generated trips shown on the attached **Figure 4** were added to the "existing – adjusted" traffic volumes shown on the attached **Figure 3**, in order to calculate the future peak hour traffic volumes, with the addition of the proposed development. Future traffic volumes are shown on the attached **Figure 5**.

6 FUTURE CONDITIONS

The future peak hour vehicle delays and LOS *with the proposed development* were calculated at the study intersections using Synchro (Version 11) traffic analysis software. This analysis was based on the proposed lane use and traffic control shown on the attached **Figure 2**, proposed site access plan, the future traffic volumes shown on the attached **Figure 5**, and the methodologies presented in the Highway Capacity Manual, 6th Edition (HCM6). The results of the future conditions analysis are attached and summarized in **Table 6**.

Table 6: Future Intersection Operations

Intersection		Control	Approach	Existing Conditions				Future Conditions				Difference			
				AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
				Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
10	SB Kalamazoo Avenue & NB-to-SB X/O	Signalized	WBL	0.8	A	0.9	A	0.9	A	0.9	A	0.1	-	0.0	-
			NB	Free				Free				N/A			
			SB	2.2	A	3.2	A	2.7	A	3.2	A	0.5	-	0.0	-
			Overall	2.1	A	2.9	A	2.4	A	2.8	A	0.3	-	-0.1	-
20	SB Kalamazoo Avenue & 52nd Street	Signalized	EBL	54.0	D	52.8	D	53.7	D	53.1	D	-0.3	-	0.3	-
			EBTR	39.3	D	42.1	D	39.1	D	42.1	D	-0.2	-	0.0	-
			WBL	51.8	D	50.7	D	51.7	D	50.9	D	-0.1	-	0.2	-
			WBTR	36.2	D	45.1	D	36.3	D	45.6	D	0.1	-	0.5	-
			NBT	20.2	C	23.6	C	21.2	C	24.4	C	1.0	-	0.8	-
			NBR	20.4	C	21.8	C	20.8	C	22.1	C	0.4	-	0.3	-
			SBT	18.9	B	23.3	C	19.8	B	24.2	C	0.9	-	0.9	-
			SBR	17.5	B	20.3	C	17.7	B	20.5	C	0.2	-	0.2	-
			Overall	28.6	C	33.2	C	28.8	C	33.5	C	0.2	-	0.3	-
30	NB Kalamazoo Avenue & SB-to-NB X/O	Signalized	WBL	0.7	A	0.8	A	0.7	A	0.9	A	0.0	-	0.1	-
			NB	2.6	A	3.5	A	3.2	A	3.5	A	0.6	-	0.0	-
			SB	Free				Free				N/A			
			Overall	2.5	A	2.8	A	2.9	A	2.7	A	0.4	-	-0.1	-
40	52nd Street & N. Site Drive	Stop (Minor)	EB	N/A				Free				N/A			
			WBL					12.3	B	12.0	B				
			NB					17.5	C	20.3	C				
50	SB Kalamazoo Avenue & E. Site Drive	Stop (Minor)	EBR	N/A				14.5	B	25.1	D	N/A			
			SB					Free							

* Decreases in delay are the result of HCM methodology

The results of the future analysis indicate that all study intersection approaches and movements will continue to operate acceptably, at LOS D or better during both peak periods, in a manner similar to existing conditions. Additionally, all approaches and movements at the proposed site driveways on Kalamazoo Avenue and 52nd Street are expected to operate acceptably, at LOS D or better during both the AM and PM peak hours. Review of SimTraffic microsimulations also indicates acceptable operations, similar to the existing conditions analysis. The majority of vehicle queues at the signalized study intersections observed to be serviced within each cycle length; additionally, vehicles at the stop-controlled site driveways were observed to find adequate gaps within the through traffic, without experiencing significant delays or excessive vehicle queueing.

7 CONCLUSIONS

The conclusions of this TIS are as follows:

1. **Existing Conditions:** All study intersection approaches and movements currently operate acceptably, at a LOS D or better, during both peak periods. Review of SimTraffic network simulations also indicates acceptable operations, with the majority of vehicle queues at the signalized study intersections observed to be serviced within each cycle length.
2. **Future Conditions:** With the addition of the site-generated traffic, all the adjacent study intersection approaches and movements will continue to operate acceptably, at LOS D or better during both peak periods, in a manner similar to existing conditions. The proposed site driveways are also expected to operate at LOS D or better during both peak periods.
 - Review of SimTraffic microsimulations also indicates acceptable operations. The majority of vehicle queues at the signalized study intersections observed to be serviced within each cycle length; additionally, vehicles at the stop-controlled site driveways were observed to find adequate gaps within the through traffic, without experiencing significant delays or excessive vehicle queueing.

Any questions related to this memorandum, study, analysis, and results should be addressed to Fleis & VandenBrink.



I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Michigan.

A handwritten signature in black ink that reads "Jacob Swanson".

Digitally signed by
Jacob Swanson
Date: 2022.05.18
17:59:37 -04'00'

Attached: Figures 1-5
 Proposed Site Plan
 Traffic Volume Data
 Synchro / SimTraffic Results

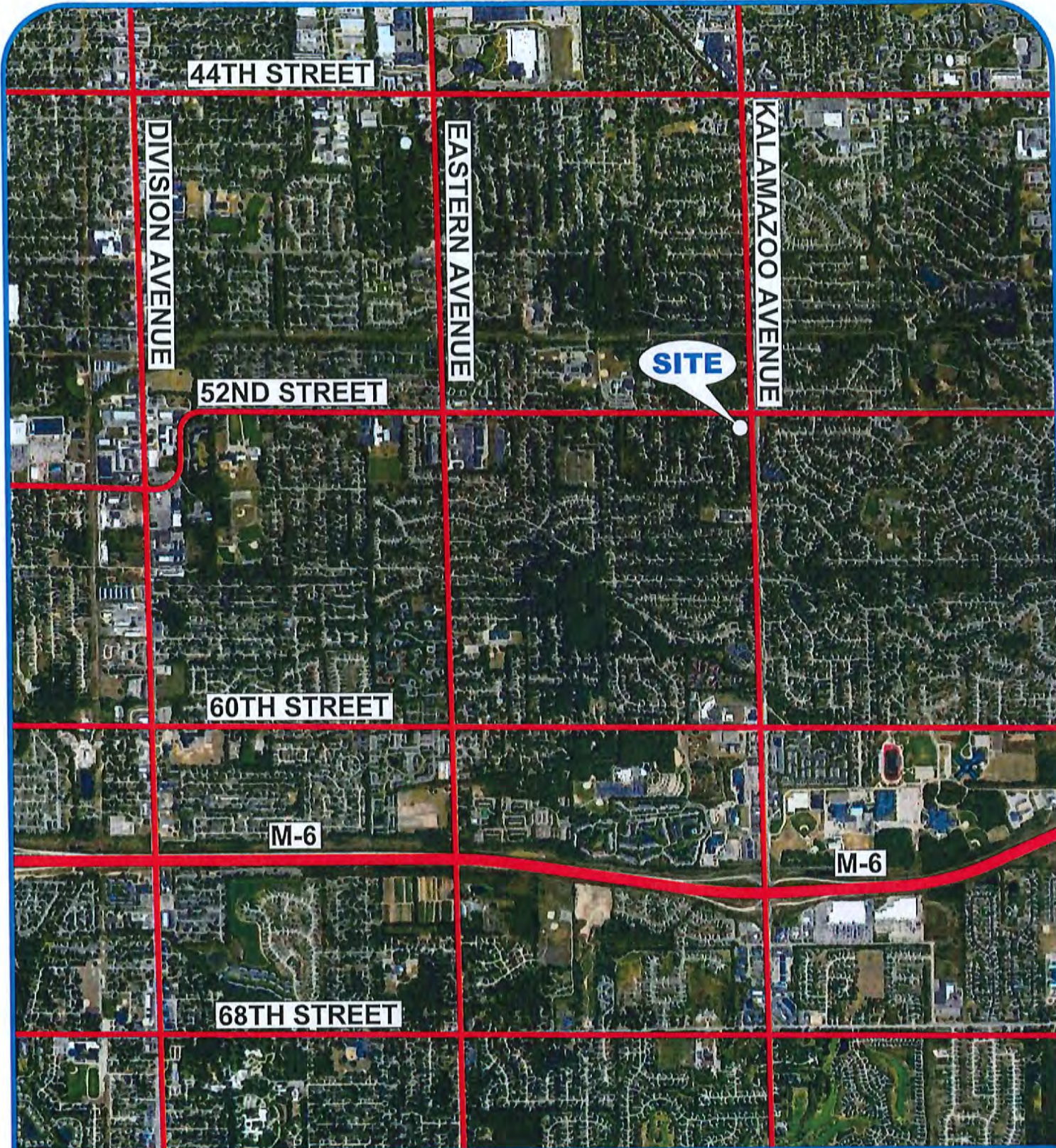


FIGURE 1

SITE LOCATION MAP

KG-2614 TIS - KENTWOOD, MI

LEGEND



SITE LOCATION



NORTH
SCALE: NOT TO SCALE

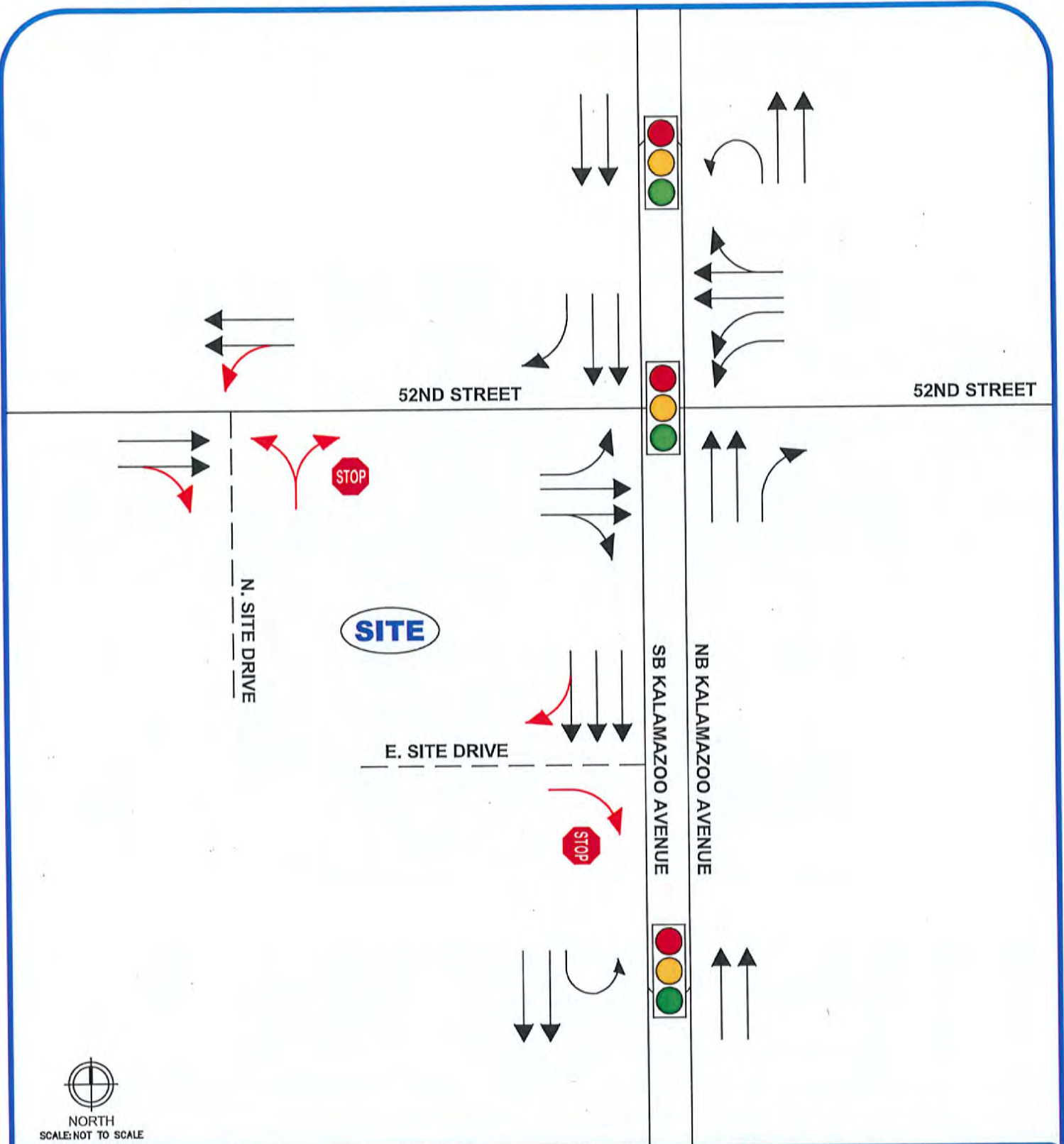
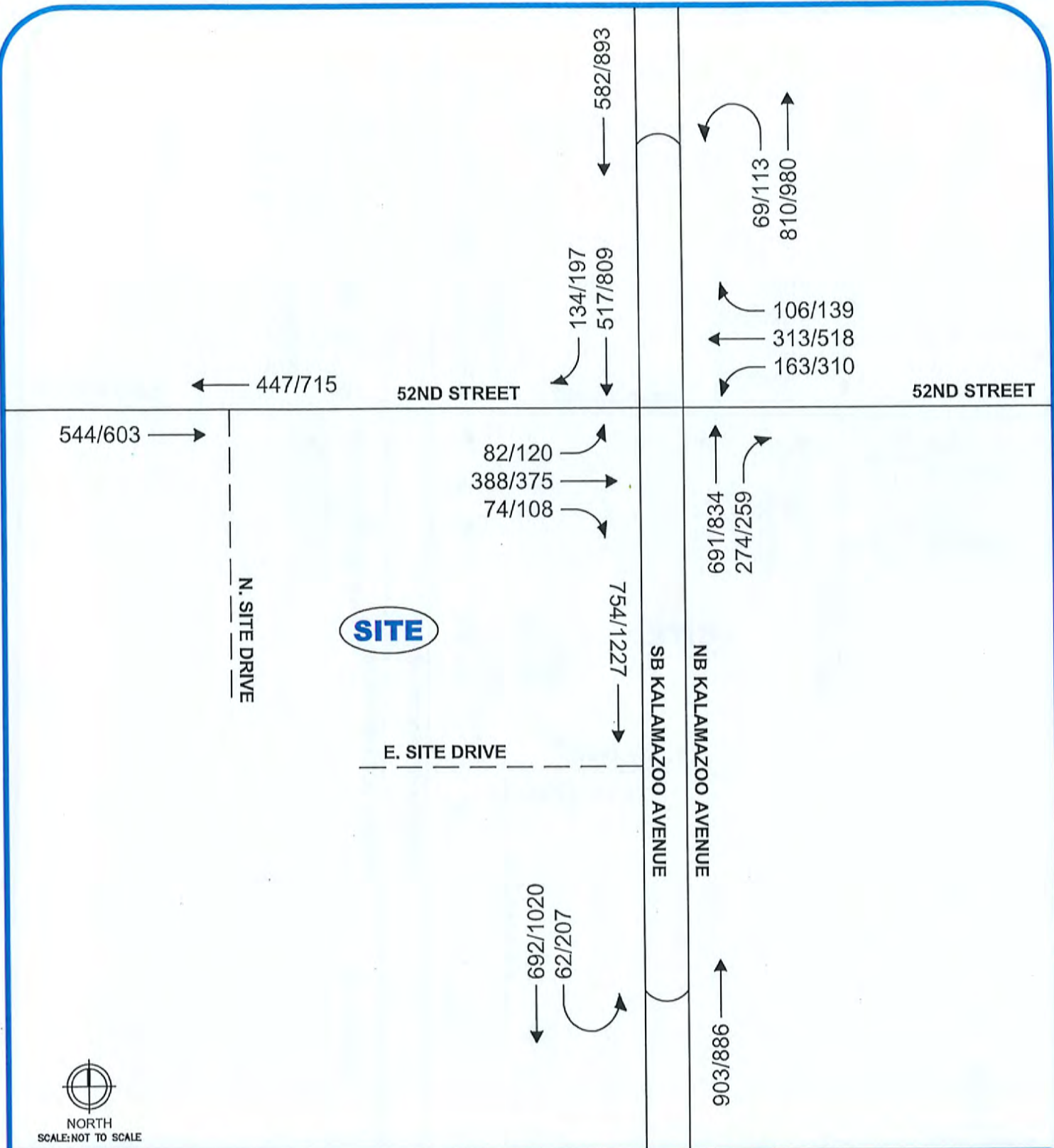


FIGURE 2
LANE USE AND TRAFFIC CONTROL

KG-2614 TIS - KENTWOOD, MI

LEGEND

	ROADS		PROPOSED ROADS
	LANE USE		PROPOSED LANE USE
	SIGNALIZED INTERSECTION		
	UNSIGNALIZED INTERSECTION		
	ROUNDBOUT INTERSECTION		






FIGURE 3

EXISTING TRAFFIC VOLUMES

KG-2614 TIS - KENTWOOD, MI

LEGEND

— ROADS

- - - PROPOSED ROADS

↑ ↓ TRAFFIC VOLUMES (AM/PM)

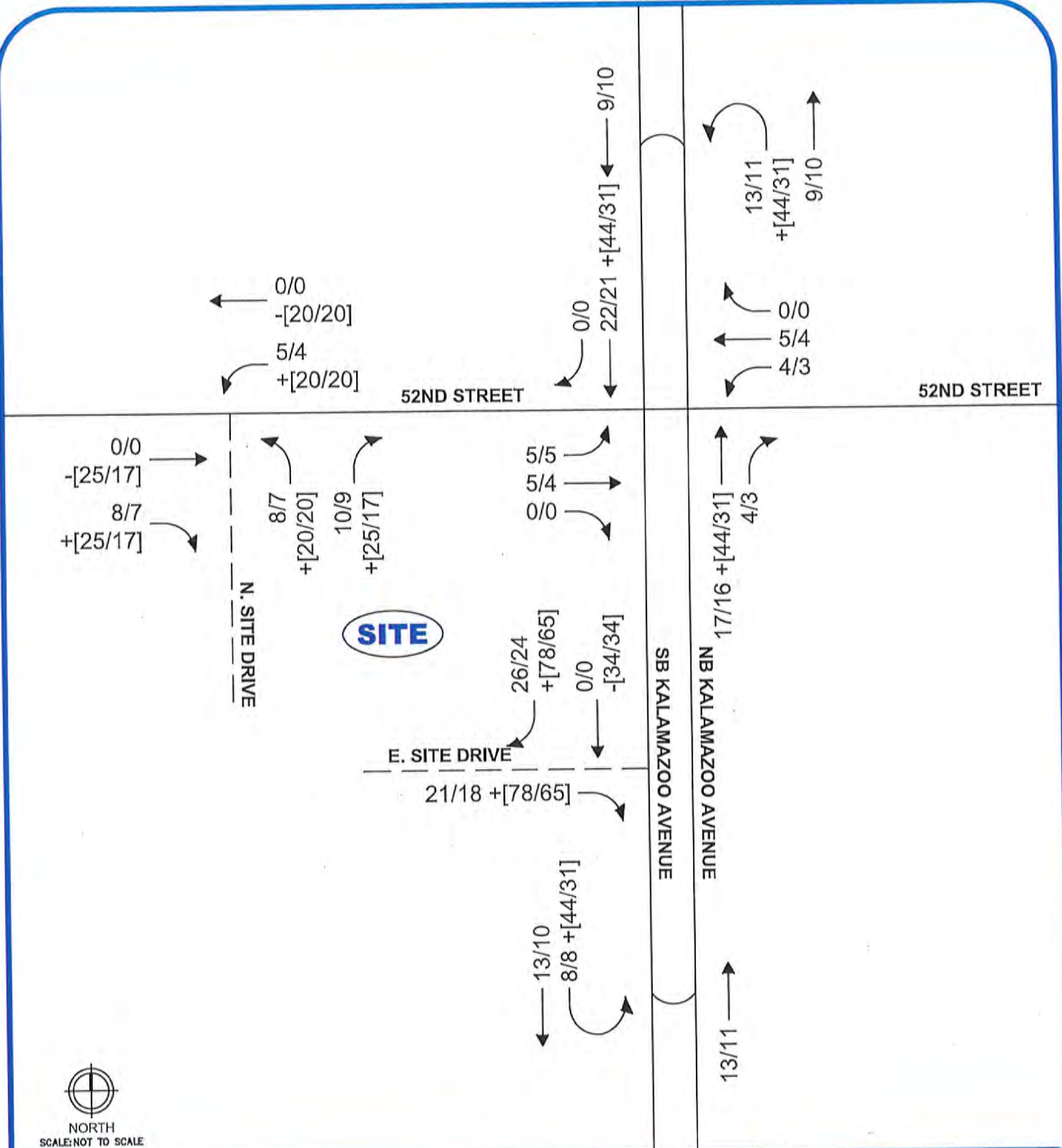


FIGURE 4
SITE-GENERATED
TRAFFIC VOLUMES

KG-2614 TIS - KENTWOOD, MI



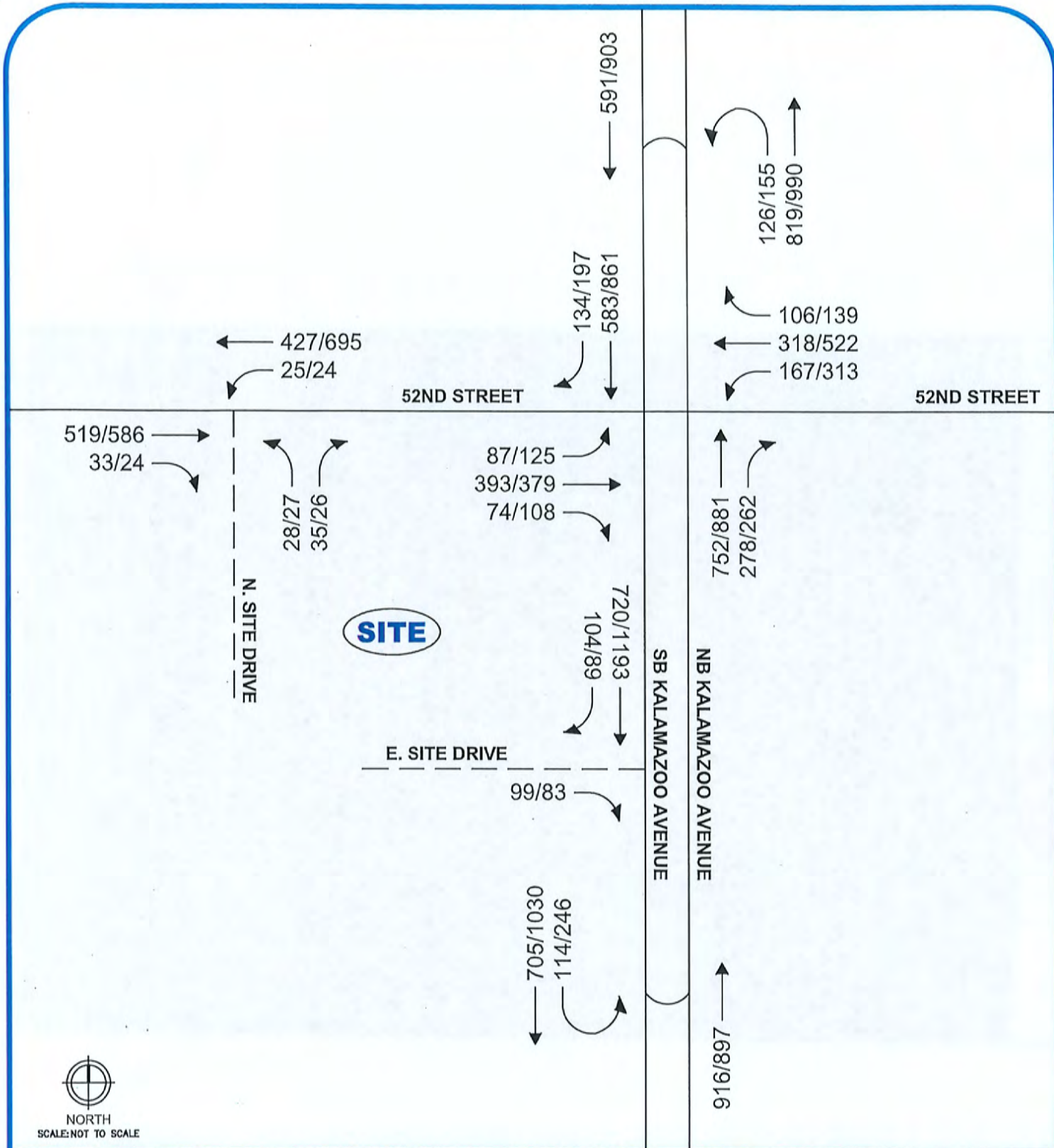


FIGURE 5
FUTURE TRAFFIC
VOLUMES

KG-2614 TIS - KENTWOOD, MI

LEGEND

- ROADS
- PROPOSED ROADS
- TRAFFIC VOLUMES (AM/PM)



52nd & Kalamazoo - TMC

Thu Apr 15, 2021

Full Length (12 AM-12 AM (+1))

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 826999, Location: 42.869189, -85.625322



Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Kalamazoo Northbound					Kalamazoo Southbound					52nd Eastbound					52nd Westbound					Int
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
2021-04-15 12:00AM	0	13	4	0	17	0	10	3	0	13	5	6	2	0	13	0	18	3	0	21	64
12:15AM	0	23	5	0	28	0	8	8	0	16	1	6	4	0	11	2	12	2	0	16	71
12:30AM	0	12	4	0	16	0	8	0	0	8	1	6	1	0	8	2	7	0	0	9	41
12:45AM	0	6	3	0	9	0	11	2	0	13	1	2	0	0	3	0	8	1	0	9	34
Hourly Total	0	54	16	0	70	0	37	13	0	50	8	20	7	0	35	4	45	6	0	55	210
1:00AM	0	9	3	0	12	0	12	0	0	12	1	2	2	0	5	1	7	2	0	10	39
1:15AM	0	4	1	0	5	0	6	0	0	6	1	6	0	0	7	3	4	1	0	8	26
1:30AM	0	4	2	0	6	0	12	3	0	15	1	5	1	0	7	1	5	1	0	7	35
1:45AM	0	6	1	0	7	0	5	3	0	8	3	3	1	0	7	0	2	1	0	3	25
Hourly Total	0	23	7	0	30	0	35	6	0	41	6	16	4	0	26	5	18	5	0	28	125
2:00AM	0	7	0	0	7	0	4	2	0	6	1	0	1	0	2	3	5	0	0	8	23
2:15AM	0	6	3	0	9	0	6	3	0	9	0	5	0	0	5	0	8	2	0	10	33
2:30AM	0	1	1	0	2	0	2	1	0	3	2	9	1	0	12	2	5	1	0	8	25
2:45AM	0	6	2	0	8	0	1	1	0	2	2	6	0	0	8	1	8	2	0	11	29
Hourly Total	0	20	6	0	26	0	13	7	0	20	5	20	2	0	27	6	26	5	0	37	110
3:00AM	0	2	1	0	3	0	6	0	0	6	1	4	1	0	6	2	9	3	0	14	29
3:15AM	0	0	1	0	1	0	4	4	0	8	0	9	0	0	9	3	3	4	0	10	28
3:30AM	0	6	2	0	8	0	2	2	0	4	0	15	0	0	15	5	8	1	0	14	41
3:45AM	0	8	0	0	8	1	2	1	0	4	2	9	1	0	12	1	5	0	0	6	30
Hourly Total	0	16	4	0	20	1	14	7	0	22	3	37	2	0	42	11	25	8	0	44	128
4:00AM	0	3	1	0	4	0	1	4	0	5	0	17	1	0	18	0	10	1	0	11	38
4:15AM	0	3	4	0	7	0	13	2	0	15	1	20	2	0	23	1	11	0	0	12	57
4:30AM	0	7	8	0	15	0	17	4	0	21	0	39	2	0	41	2	8	1	0	11	88
4:45AM	0	17	12	0	29	0	16	4	0	20	0	25	3	0	28	9	8	3	0	20	97
Hourly Total	0	30	25	0	55	0	47	14	0	61	1	101	8	0	110	12	37	5	0	54	280
5:00AM	0	13	5	0	18	1	13	1	0	15	0	31	4	0	35	6	10	3	0	19	87
5:15AM	0	21	13	0	34	0	26	5	0	31	2	34	4	0	40	9	8	3	0	20	125
5:30AM	0	26	22	0	48	0	42	4	0	46	6	79	1	0	86	7	21	8	0	36	216
5:45AM	0	32	28	0	60	1	43	14	0	58	3	61	6	0	70	6	11	4	0	21	209
Hourly Total	0	92	68	0	160	2	124	24	0	150	11	205	15	0	231	28	50	18	0	96	637
6:00AM	0	31	18	0	49	0	44	9	0	53	11	42	7	0	60	12	32	8	0	52	214
6:15AM	0	55	28	0	83	0	52	9	0	61	9	66	6	0	81	15	32	9	0	56	281
6:30AM	0	65	53	0	118	0	59	17	0	76	13	78	3	0	94	17	33	12	0	62	350
6:45AM	0	68	32	0	100	0	88	9	0	97	11	57	8	0	76	12	38	10	0	60	333
Hourly Total	0	219	131	0	350	0	243	44	0	287	44	243	24	0	311	56	135	39	0	230	1178
7:00AM	0	73	31	0	104	0	91	20	0	111	9	49	12	0	70	27	70	5	0	102	387
7:15AM	0	121	50	0	171	0	112	26	0	138	18	72	16	0	106	32	67	28	0	127	542
7:30AM	0	158	61	0	219	2	124	30	0	156	15	78	13	0	106	31	60	20	0	111	592
7:45AM	0	155	65	0	220	0	89	29	0	118	20	112	17	0	149	37	60	26	0	123	610
Hourly Total	0	507	207	0	714	2	416	105	0	523	62	311	58	0	431	127	257	79	0	463	2131
8:00AM	0	129	47	0	176	0	88	23	0	111	14	54	14	0	82	33	68	12	0	113	482
8:15AM	0	106	49	0	155	0	78	32	0	110	12	67	21	0	100	46	36	31	0	113	478
8:30AM	0	115	43	0	158	0	93	30	0	123	19	67	8	0	94	27	34	14	0	75	450
8:45AM	0	90	48	0	138	0	107	24	0	131	27	63	24	0	114	35	69	20	0	124	507
Hourly Total	0	440	187	0	627	0	366	109	0	475	72	251	67	0	390	141	207	77	0	425	1917
9:00AM	0	159	31	0	190	0	97	37	0	134	18	44	23	0	85	42	42	26	0	110	519
9:15AM	0	95	25	0	120	0	84	23	0	107	20	43	17	0	80	28	43	10	0	81	388
9:30AM	0	96	22	0	118	0	69	20	0	89	17	43	17	0	77	29	51	18	0	98	382
9:45AM	0	110	23	0	133	0	60	25	0	85	20	59	14	0	93	22	40	9	0	71	382
Hourly Total	0	460	101	0	561	0	310	105	0	415	75	189	71	0	335	121	176	63	0	360	1671
10:00AM	0	92	27	0	119	0	94	18	0	112	25	40	9	0	74	17	32	9	0	58	363
10:15AM	0	106	33	0	139	0	81	23	0	104	22	39	16	0	77	12	47	9	0	68	388
10:30AM	0	92	37	0	129	0	85	31	0	116	17	42	13	0	72	19	46	4	0	69	386
10:45AM	0	93	25	0	118	0	109	22	0	131	31	55	16	0	102	24	43	8	0	75	426

Leg Direction	Kalamazoo Northbound					Kalamazoo Southbound					52nd Eastbound					52nd Westbound					
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
Hourly Total	0	383	122	0	505	0	369	94	0	463	95	176	54	0	325	72	168	30	0	270	1563
11:00AM	0	89	21	0	110	0	101	33	0	134	24	32	9	0	65	30	40	11	0	81	390
11:15AM	0	115	25	0	140	0	82	25	0	107	16	51	10	0	77	40	41	9	0	90	414
11:30AM	0	120	35	0	155	0	91	36	0	127	18	57	9	0	84	29	58	11	0	98	464
11:45AM	0	103	30	0	133	0	102	33	0	135	20	56	11	0	87	44	45	18	0	107	462
Hourly Total	0	427	111	0	538	0	376	127	0	503	78	196	39	0	313	143	184	49	0	376	1730
12:00PM	0	122	32	0	154	0	100	49	0	149	28	61	23	0	112	40	58	11	0	109	524
12:15PM	0	129	33	0	162	0	113	33	0	146	33	49	27	0	109	41	71	13	0	125	542
12:30PM	0	96	46	0	142	0	115	27	0	142	23	69	23	0	115	39	57	17	0	113	512
12:45PM	0	120	44	0	164	0	115	31	0	146	17	61	20	0	98	49	64	12	0	125	533
Hourly Total	0	467	155	0	622	0	443	140	0	583	101	240	93	0	434	169	250	53	0	472	2111
1:00PM	0	104	46	0	150	0	115	29	0	144	34	67	19	0	120	47	59	22	0	128	542
1:15PM	0	117	39	0	156	0	108	30	0	138	24	70	14	0	108	38	51	15	0	104	506
1:30PM	0	120	53	0	173	0	136	28	0	164	23	76	22	0	121	40	69	22	0	131	589
1:45PM	0	161	53	0	214	0	125	39	0	164	22	80	23	0	125	41	77	14	0	132	635
Hourly Total	0	502	191	0	693	0	484	126	0	610	103	293	78	0	474	166	256	73	0	495	2272
2:00PM	0	130	58	0	188	0	121	27	0	148	30	72	18	0	120	47	114	30	0	191	647
2:15PM	0	160	45	0	205	0	129	41	0	170	33	84	21	0	138	42	98	20	0	160	673
2:30PM	0	146	73	0	219	0	180	43	0	223	28	97	16	0	141	50	99	29	0	178	761
2:45PM	0	138	60	0	198	0	183	42	0	225	25	85	28	0	138	50	98	23	0	171	732
Hourly Total	0	574	236	0	810	0	613	153	0	766	116	338	83	0	537	189	409	102	0	700	2813
3:00PM	0	151	63	0	214	0	180	44	0	224	25	94	23	0	142	89	132	39	0	260	840
3:15PM	0	164	50	0	214	0	166	32	0	198	31	109	25	0	165	62	140	46	0	248	825
3:30PM	0	176	43	0	219	0	184	45	0	229	21	72	23	0	116	71	112	29	0	212	776
3:45PM	0	157	46	0	203	0	177	52	0	229	36	77	30	0	143	69	102	16	0	187	762
Hourly Total	0	648	202	0	850	0	707	173	0	880	113	352	101	0	566	291	486	130	0	907	3203
4:00PM	0	160	43	0	203	0	175	34	0	209	42	64	28	0	134	60	91	33	0	184	730
4:15PM	0	150	54	0	204	0	168	41	0	209	36	82	25	0	143	72	90	19	0	181	737
4:30PM	0	156	55	0	211	0	154	42	0	196	23	77	32	0	132	63	124	35	0	222	761
4:45PM	0	187	62	0	249	0	145	51	0	196	31	77	20	0	128	70	96	22	0	188	761
Hourly Total	0	653	214	0	867	0	642	168	0	810	132	300	105	0	537	265	401	109	0	775	2989
5:00PM	0	165	55	0	220	0	171	54	0	225	34	70	26	0	130	82	116	33	0	231	806
5:15PM	0	182	63	0	245	0	188	44	0	232	22	86	21	0	129	70	109	29	0	208	814
5:30PM	0	149	64	0	213	0	142	46	0	188	34	63	32	0	129	65	88	28	0	181	711
5:45PM	0	159	58	0	217	0	152	54	0	206	29	71	23	0	123	43	93	17	0	153	699
Hourly Total	0	655	240	0	895	0	653	198	0	851	119	290	102	0	511	260	406	107	0	773	3030
6:00PM	0	132	62	0	194	0	136	25	0	161	26	79	31	0	136	37	60	19	0	116	607
6:15PM	0	150	53	0	203	0	129	46	0	175	24	47	18	0	89	44	76	17	0	137	604
6:30PM	0	136	38	0	174	0	113	33	0	146	36	43	19	0	98	37	69	16	0	122	540
6:45PM	0	140	46	0	186	0	103	50	0	153	27	46	20	0	93	37	56	12	0	105	537
Hourly Total	0	558	199	0	757	0	481	154	0	635	113	215	88	0	416	155	261	64	0	480	2288
7:00PM	0	104	35	0	139	0	97	29	1	127	26	42	22	0	90	28	55	13	0	96	452
7:15PM	0	98	33	0	131	0	96	36	0	132	27	47	17	0	91	26	40	11	0	77	431
7:30PM	0	91	53	0	144	0	103	23	0	126	22	35	12	0	69	34	42	11	0	87	426
7:45PM	0	85	29	0	114	0	94	31	0	125	13	38	14	0	65	31	53	6	0	90	394
Hourly Total	0	378	150	0	528	0	390	119	1	510	88	162	65	0	315	119	190	41	0	350	1703
8:00PM	0	89	26	0	115	0	75	20	0	95	28	47	19	0	94	27	31	7	0	65	369
8:15PM	0	79	35	0	114	0	89	24	0	113	19	29	18	0	66	26	34	7	0	67	360
8:30PM	0	82	33	0	115	0	80	19	0	99	23	42	18	0	83	50	37	9	0	96	393
8:45PM	0	57	21	0	78	0	64	24	0	88	11	30	8	0	49	19	22	9	0	50	265
Hourly Total	0	307	115	0	422	0	308	87	0	395	81	148	63	0	292	122	124	32	0	278	1387
9:00PM	0	59	23	0	82	0	54	11	0	65	16	31	3	0	50	18	18	10	0	46	243
9:15PM	0	58	18	0	76	0	75	11	0	86	9	26	12	0	47	13	22	6	0	41	250
9:30PM	0	41	19	0	60	0	38	15	0	53	8	37	5	0	50	12	25	4	0	41	204
9:45PM	0	37	17	0	54	0	49	14	0	63	8	18	4	0	30	6	35	5	0	46	193
Hourly Total	0	195	77	0	272	0	216	51	0	267	41	112	24	0	177	49	100	25	0	174	890
10:00PM	0	37	12	0	49	0	40	9	0	49	10	29	3	0	42	13	51	18	0	82	222
10:15PM	0	39	15	0	54	0	39	13	0	52	11	28	4	0	43	5	29	6	0	40	189
10:30PM	0	33	22	0	55	0	39	12	0	51	5	33	4	0	42	10	14	9	0	33	181
10:45PM	0	28	12	0	40	0	31	8	0	39	5	24	3	0	32	12	17	8	0	37	148

Leg Direction	Kalamazoo Northbound					Kalamazoo Southbound					52nd Eastbound					52nd Westbound					
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
Hourly Total	0	137	61	0	198	0	149	42	0	191	31	114	14	0	159	40	111	41	0	192	740
11:00PM	0	25	9	0	34	0	34	8	0	42	13	13	5	0	31	9	38	19	0	66	173
11:15PM	0	26	8	0	34	0	32	9	0	41	7	18	3	0	28	14	38	9	0	61	164
11:30PM	0	22	8	0	30	0	25	10	0	35	2	14	6	0	22	12	17	5	0	34	121
11:45PM	0	18	5	0	23	0	14	5	0	19	3	12	3	0	18	3	8	5	0	16	76
Hourly Total	0	91	30	0	121	0	105	32	0	137	25	57	17	0	99	38	101	38	0	177	534
Total	0	7836	2855	0	10691	5	7541	2098	1	9645	1523	4386	1184	0	7093	2589	4423	1199	0	8211	35640
% Approach	0%	73.3%	26.7%	0%	-	0.1%	78.2%	21.8%	0%	-	21.5%	61.8%	16.7%	0%	-	31.5%	53.9%	14.6%	0%	-	-
% Total	0%	22.0%	8.0%	0%	30.0%	0%	21.2%	5.9%	0%	27.1%	4.3%	12.3%	3.3%	0%	19.9%	7.3%	12.4%	3.4%	0%	23.0%	-
Lights	0	7663	2824	0	10487	5	7380	2057	1	9443	1504	4327	1121	0	6952	2551	4363	1169	0	8083	34965
% Lights	0%	97.8%	98.9%	0%	98.1%	100%	97.9%	98.0%	100%	97.9%	98.8%	98.7%	94.7%	0%	98.0%	98.5%	98.6%	97.5%	0%	98.4%	98.1%
Articulated Trucks	0	15	0	0	15	0	26	0	0	26	3	6	3	0	12	2	4	3	0	9	62
% Articulated Trucks	0%	0.2%	0%	0%	0.1%	0%	0.3%	0%	0%	0.3%	0.2%	0.1%	0.3%	0%	0.2%	0.1%	0.1%	0.3%	0%	0.1%	0.2%
Buses and Single-Unit Trucks	0	158	31	0	189	0	135	41	0	176	16	53	60	0	129	36	56	27	0	119	613
% Buses and Single-Unit Trucks	0%	2.0%	1.1%	0%	1.8%	0%	1.8%	2.0%	0%	1.8%	1.1%	1.2%	5.1%	0%	1.8%	1.4%	1.3%	2.3%	0%	1.4%	1.7%

*L: Left, R: Right, T: Thru, U: U-Turn



CONSULTING. ENGINEERING. CONSTRUCTION.

May 18, 2022

City of Kentwood
4900 Breton Ave SE
Kentwood, MI 49508

Subject: Site Plan Review
Applicant: Kum & Go, LLC.

INTRODUCTION

Kum & Go, L.C. (The developer) has an option to purchase a +/-2.52-acre property at the southwest corner of 52nd Street SE and Kalamazoo Avenue. The developer proposes to construct a convenience store with vehicle fueling to include alcohol sales and 24-hour operations. The parcels are within a Commercial Planned Unit Development (CPUD) that is further described in a PUD Statement memo dated February 6, 2003. The following is a written description to be included as part of the supporting application materials for the development including commentary on standards outlined in the PUD memo.

NATURE OF PROPOSED USE

The proposed development will include the sale of typical package convenience store items, preparation and sale of food, and alcohol sales. Exterior sales may include ice, propane, firewood, packaged water, road salt, and other similar items typical of exterior sales at a convenience store with vehicle fueling. The vehicle fueling is proposed with six fuel dispensers that can accommodate up to twelve vehicles. The development is proposed to have 24 hours of operations. Up to five employees may occupy the facility. Vehicular traffic will consist of typical passenger traffic with corresponding peaks during morning, lunch, and afternoon commutes.

SITE AND BUILDING LAYOUT

The proposed development sites the convenience store building on the western-central portion of the property. Six fuel dispensers under a canopy will be centrally located to the east of the building. Parking is located to the north and east of the building. Driveway entrances are proposed along 52nd Street SE and Kalamazoo Avenue.

The proposed floor plan includes food preparation and sales, a seating area, and retail area for typical convenience store items in the front of the building. Storage areas are located in the rear of the building.

Landscaped areas are proposed along each of the street frontages and along property boundaries abutting residential zoning. The dumpster area will be screened in accordance with applicable ordinances. Six foot vertical screening wall or fence is proposed along the property boundaries adjacent to residential zoning.

Adjacent property uses include a gasoline dispensing station to the north and residential use to the northwest, beyond 52nd Street SE, vacant land, and a medical office to the east, beyond Kalamazoo Avenue, and residential use to the south and west.

SITE PLANS, BUILDING ELEVATIONS AND FLOOR PLAN

Please refer to the enclosed site plan set. This set contains all the information required per the City site plan review information requests within the zoning ordinance (Section 14.04).

CPUD STANDARDS

The following provides a summary and responses to the items outlined in the February 6, 2003 PUD Statement for the Southwest Corner of 52nd Street and Kalamazoo Avenue from Lisa Golder, Economic Development Planner to the Kentwood Planning Commission.

Standard #1. Compliance with the Master Plan.

Master Plan recommends commercial and office use for the site. The proposed CPUD zoning permits uses in both the C-4 Office Zones as well as the C-2 Community Commercial zone. The Master Plan also recommends neighborhood commercial uses that are walkable for the surrounding residences.

The zoning ordinance for Commercial PUD further states that any “permitted use” within C-2 and C-4 districts and customary accessory uses as permitted in the C-2 or C-4 districts are permitted uses within a CPUD. Further, uses permitted by Special Land Use in the C-2 and C-4 districts may be permitted when all applicable standards as cited in Chapter 15 are met. The proposed convenience store is a permitted use under C-2 and vehicle fuel stations are permitted under C-2 with a Special Land Use determination. The site plan layout is designed for pedestrian access from each of the street frontages.

Standard #2. Use Restrictions.

It is the intent of the City of Kentwood to exercise flexibility in the review and analysis of any proposed use of the property. The uses proposed for the CPUD should be permitted or Special Land Uses that are mid to low traffic generators that do not create high traffic volumes during peak hours for the road system (7:00-8:30 AM and 4:00-6:00 PM). The City will require a traffic analysis to determine the impact of the use on the surrounding road system, to evaluate traffic flow, and to determine the changes to the intersection that may be required to accommodate the use.

The proposed use is anticipated to support, but not significantly increase, existing traffic patterns. As noted in the Traffic Impact Study, a majority of the traffic that will visit the property will be generated by “pass-by” trips that do not add new traffic to the area. A review of proposed site conditions and existing road and intersection conditions indicated that no modifications are required to maintain the existing acceptable operation.

Standard #3. Site Requirements.

All three parcels must be combined into one parcel under a single ownership. The westerly portion of the southernmost parcel shall be split and combined with the small residential lot on Newcastle to create a usable residential lot on that street.

The parcels will be split and combined as required and depicted on the site plans. The lot split/combination is to be conducted concurrently with the site plan submittal.

Standard #4. Driveways.

All existing driveway on Kalamazoo and 52nd Street will be removed. One driveway will be permitted on Kalamazoo and one driveway will be permitted on 52nd Street. The driveway on 52nd Street shall be located on the westernmost side of the development and is anticipated to allow both left and right turns in and out of the driveway. The driveway on Kalamazoo shall either align with the driveway to the east, or shall be located further south than the driveway to the east. A traffic impact analysis will be required in accordance with Section 13.02 of the Kentwood Zoning Ordinance to evaluate traffic flow and final driveway location/alignment.

The proposed layout follows the requirements to remove all existing driveways and place driveways as described at the westernmost area along 52nd Street and the southeastern portion along Kalamazoo. As described in the Traffic Impact Study, a review of proposed site conditions including driveway locations, and existing road and intersection conditions, indicated that no modifications are required to maintain the existing acceptable operation.

Standard #5. Architectural Features.

Building roof elevation shall be in a low profile residential style with peaked roof or another roof style approved by the Planning and City Commissions. The buildings shall not overpower the residential uses to the south and west. The building shall be primarily of brick on all sides, with architectural features that create interest and break up large expanses of wall area.

The proposed building height is consistent with a single-story commercial building with a top of parapet height of 18 feet with architectural features at the entrance extending to 21' and a feature wall up to a height of 22'-8". The proposed features and windows break up large expanses of wall area. Refer to the included color elevation in the site plan for additional information

Standard #6. Site Design.

The building(s) shall be designed to avoid the appearance of overcrowding on the site and the adjoining homes. At least 25% green space shall be provided on the site, but less green space will be considered if other site amenities area provided. The main building shall be limited to approximately 11,000 square feet in area, with an additional 6,000 square foot office building permitted along the south side of the site. The Police and Fire Departments must review and approve the building location for the safety and security of the building occupants and area residents.

The building must be residential in scale and design. Parking shall be held to a minimum for the planned and potential uses; deferred parking shall be encouraged as long as it can be shown that the deferred parking can be provided at a later date on site needed without impacting the total required green space. Servicing/loading within the buildings shall occur between the hours of 9:00 AM to 3:00 PM. Circulation around the site shall ensure separation of pedestrian and service vehicles, and shall provide easy access to sidewalks on Kalamazoo Avenue and 52nd Street. The site will allow for bicycle access and parking. Service traffic shall be kept as far away from residential uses as possible.

The proposed development requested an extension of the servicing/loading hours to 8:00 AM to 8:00 PM based on the additional landscape buffering and screening to be installed.

Sidewalks shall be constructed on the subject property outside the public right-of-way and public easements will be provided to allow public access to the sidewalks.

The proposed 5,620 square foot building is centrally located with landscaping around the perimeter. At least 25% of green space is provided with the proposed layout. The proposed building is

consistent in size and scale for a single-story commercial building and is further depicted in the included building elevations. Sidewalks provided access from 52nd Street SE and Kalamazoo to the proposed building with designated crosswalks when crossing internal driving lanes. A bicycle rack is provided near the proposed building.

Standard #7: Hours of Operations.

The hours of operation for the businesses within the PUD shall be limited time that are compatible with residential uses, from approximately 7:00 AM to 10:00 PM.

The proposed development requests approval 24 hours of operation. The proposed layout includes landscape and wall or fence screening along the property boundaries that about residential use. Proposed lighting will include shielding and placement to control light intrusion to adjacent residential properties.

Standard #8: Lighting Standards.

The site lighting shall be in compliance with Chapter 20 of the Kentwood Zoning Ordinance. After the businesses are closed daily, the lighting for the site shall be reduced to serve for security purposes only.

The proposed site design shall comply with the requirements of the Kentwood Zoning Ordinance. Should 24 hour operation be approved, lighting design will include consideration for control of light intrusion to adjacent properties.

Standard #9: Landscaping/Parking

Landscaping shall be designed to provide a pleasing, permanent look for this corner. Parking shall be limited in the front setback area along the 52nd and Kalamazoo rights of way. Parking shall be screened by landscaping and berming. The landscaping shall be low enough to provide visibility from the street for both customers and employees. Landscaping shall meet or exceed Zoning Ordinance standards along the western and southern property lines. Landscaping shall be maintained at all times, including replacement of dead and damaged plants and regular to maintain proper plant heights.

The proposed landscaping adheres to the requirements of this section and comply with buffer requirements for commercial properties. Refer to the included landscape plan and details for additional information.

Standard #10: Adjacent uses.

The site shall be designed with the clear intention that commercial uses will not be extended further south and west of the parcel.

The proposed layout includes perimeter landscaping and screening along with curb surround the developed area in a manner with a clear intention the site will not be extended further than proposed.

PROJECT TEAM:

DEVELOPER

KUM & GO
1459 GRAND AVE.
DES MOINES, IA 50309
CONTACT: RYAN HALDER
PHONE: 515-457-6232
EMAIL: ryan.halder@kumandgo.com

ARCHITECT

BRR ARCHITECTURE, INC.
8131 METCALF AVE., SUITE 300
OVERLAND PARK, KS 66204
PHONE: (913) 262-9095

CIVIL ENGINEER

ATWELL, LLC
12745 23 MILE ROAD, SUITE 200
SHELBY TOWNSHIP, MI 48315
CONTACT: MICHAEL McPHERSON
PHONE: (586) 786-9800
EMAIL: mmcpherson@atwell-group.com

PLANNING

KENTWOOD PLANNING COMMISSION
4900 BRETON AVE SE
KENTWOOD, MI 49508
CONTACT: TERRY SCHWEITZER
PHONE: 616-554-0710
EMAIL: schweitt@kentwood.us

BUILDING

KENTWOOD
4900 BRETON AVE SE
KENTWOOD, MI 49508
CONTACT: TERRY SCHWEITZER
PHONE: 616-554-0710
schweitt@kentwood.us

WATER

CITY OF WYOMING PUBLIC WORKS
2660 BURLINGAME AVE SW
WYOMING, MI 49509
CONTACT: KARI MINKUS
PHONE: 616-530-7260
EMAIL: minkusk@wyomingmi.gov

STORMWATER

CITY OF KENTWOOD
1300 MARKET AVE SW
GRAND RAPIDS, MI 49503
CONTACT: JIM KIRKWOOD, P.E.
PHONE: 616-554-0739
EMAIL: kirkwoodj@kentwood.us

ACCESS & ROAD

KENTWOOD PUBLIC WORKS
5068 BRETON RD SE
GRAND RAPIDS, MI 49508
CONTACT: CHAD GRIFFIN
PHONE: 616-554-0825
EMAIL: griffinc@kentwood.us

GAS

DTE
4100 DOERR ROAD
CASS CITY, MI 48726
CONTACT: LISA GREMEL
PHONE: 989-670-4460
EMAIL: lisa.gremel@tteenergy.com

ELECTRIC

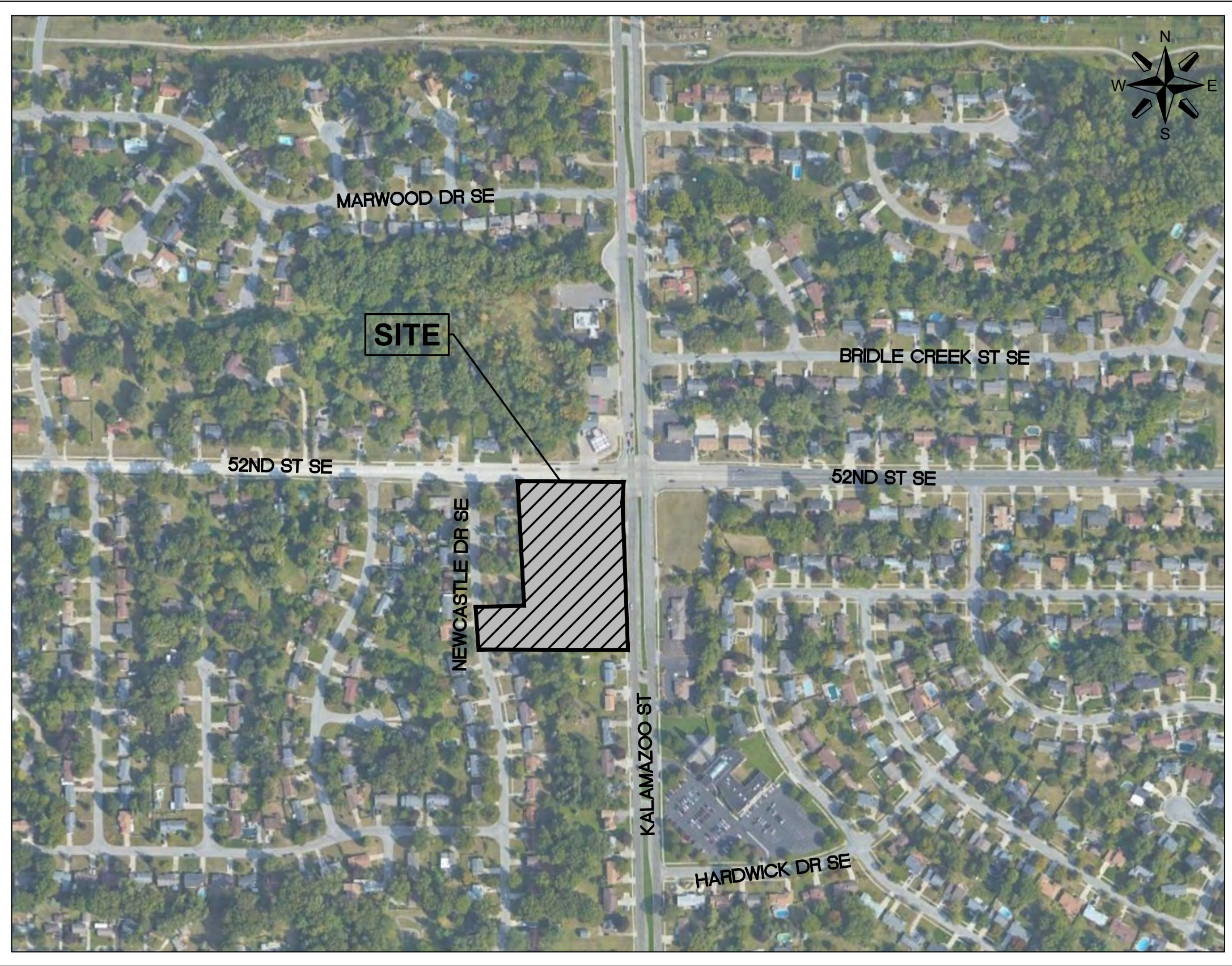
CONSUMERS ELECTRIC
CONTACT: TANYA GILROY
PHONE: 616-251-0574
EMAIL: tanya.gilroy@cmsenergy.com

SIGNAGE

CITY OF KENTWOOD
1300 MARKET AVE SW
GRAND RAPIDS, MI 49503
CONTACT: JIM KIRKWOOD, P.E.
PHONE: 616-554-0739
EMAIL: kirkwoodj@kentwood.us



WHERE MEANS MORE!



VICINITY MAP

NOT TO SCALE

BENCHMARKS

BENCHMARK #1:
THE NORTHWEST FLANGE ON THE FIRE HYDRANT LOCATED ON THE SOUTHEAST CORNER OF THE PARCEL. WEST SIDE OF KALAMAZOO AVENUE.
ELEVATION = 742.34' (NAVD88)

BENCHMARK #2:
THE X LOCATED ON THE NORTHEAST BOLT OF THE LIGHTPOST BASE (NO LIGHTPOST CURRENTLY ATTACHED). LOCATED JUST SOUTH OF THE SWC OF KALAMAZOO AVE AND 52ND ST.
ELEVATION = 740.27' (NAVD88)

LEGAL DESCRIPTION

LAND IN THE CITY OF KENTWOOD, KENT COUNTY, MI, DESCRIBED AS FOLLOWS:

PLEASE SEE PAGES 3 (ALTA SURVEY) FOR LEGAL DESCRIPTIONS OF ALL FOUR PARCELS THAT CONSTITUTE THE PROPOSED SITE.

FLOOD ZONE:

PROPOSED LOCATION DOES NOT LIE WITHIN A FLOODPLAIN (AREA NOT MAPPED).

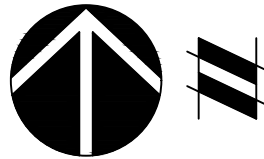
WETLANDS

NO WETLANDS IDENTIFIED ON THE SITE.

SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVD88)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVD88)



0 15' 30'
SCALE: 1"=30'

PRELIM. SITE PLAN DRAWINGS
K&G STORE #2614
5215 KALAMAZOO AVE SE
KENTWOOD, MICHIGAN 49508

Sheet List Table		
REV	Number	Sheet Title
	01	COVER
	02	LAYOUT PLAN
	03	ALTA SURVEY
	04	ALTA SURVEY
	05	DEMOLITION PLAN
	06	PRELIMINARY GRADING PLAN
	07	PRELIMINARY UTILITY PLAN
	08	PRELIMINARY LANDSCAPE PLAN
	09	LANDSCAPE DETAILS
	10	PHOTOMETRIC PLAN
	11	SIGN DETAILS
	12	EXTERIOR ELEVATIONS
	13	TRASH ENCLOSURE ELEVATIONS
	14	CANOPY ELEVATIONS

SITE DATA		
STORE TYPE		MARKET
CANOPY / DISPENSER ARRANGEMENT		6 DISPENSERS (SINGLE)
TYPE OF USE		CONVENIENCE STORE WITH FUEL
HEIGHT	BUILDING	18'-0"
	TOP OF BLADE WALL	22'-8"
	CANOPY	21'-10" (MIN.)
GROSS FLOOR AREA		5,620 S.F.
GROSS CANOPY AREA		3,936 S.F.
GROSS LOT AREA		±109,760 S.F. ±2,519 ACRES
KUM & GO PARCEL		±96,660 S.F. ±2,219 ACRES
EXCEPTED PARCEL		±13,100 S.F. ±0.30 ACRES
FAR (INCLUDING CANOPY)		N/A
BUILDING COVERAGE		5,620 S.F. (5.12%)
LANDSCAPE COVERAGE		53,190 S.F. (48.5%)
PARKING/DRIVE/SIDEWALK COVERAGE		50,950 S.F. (46.4%)
BICYCLE PARKING		2 SPACES
REQUIRED PARKING	STANDARD	23
	ADA	1
	TOTAL	24
	PARKING RATIO = 1 CAR/250 S.F.	
PROVIDED PARKING	STANDARD	24
	ADA	2
	TOTAL	26
	PARKING RATIO = 1.16 CARS/250 S.F.	

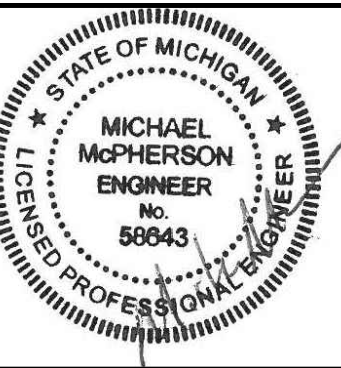
ZONING INFORMATION	
ZONE	C-PUD
MINIMUM LOT AREA	NA
MINIMUM LOT WIDTH (FEET)	100'
SITE AREA PER UNIT (SQUARE FEET)	N/A
FRONT YARD SETBACK (FEET) (NORTH)	
FRONT YARD SETBACK (FEET) (EAST)	
FRONT YARD SETBACK (FEET) (WEST)	
REAR YARD SETBACK (FEET) (NORTH)	
MAXIMUM HEIGHT (FEET)	
MAXIMUM BUILDING COVERAGE	N/A
MAXIMUM IMPERVIOUS COVERAGE	N/A
MAXIMUM FLOOR AREA	N/A



Know what's below.
Call before you dig.

CALL 811 SEVENTY-TWO HOURS PRIOR TO DIGGING, GRADING OR EXCAVATING FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

CONFIDENTIAL DOCUMENT.
INFORMATION CONTAINED IN THIS DOCUMENT IS PROPRIETARY TO KUM & GO, L.C. AND SHALL NOT BE DISTRIBUTED.



1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE
COVER

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISION DESCRIPTION	DATE	BY

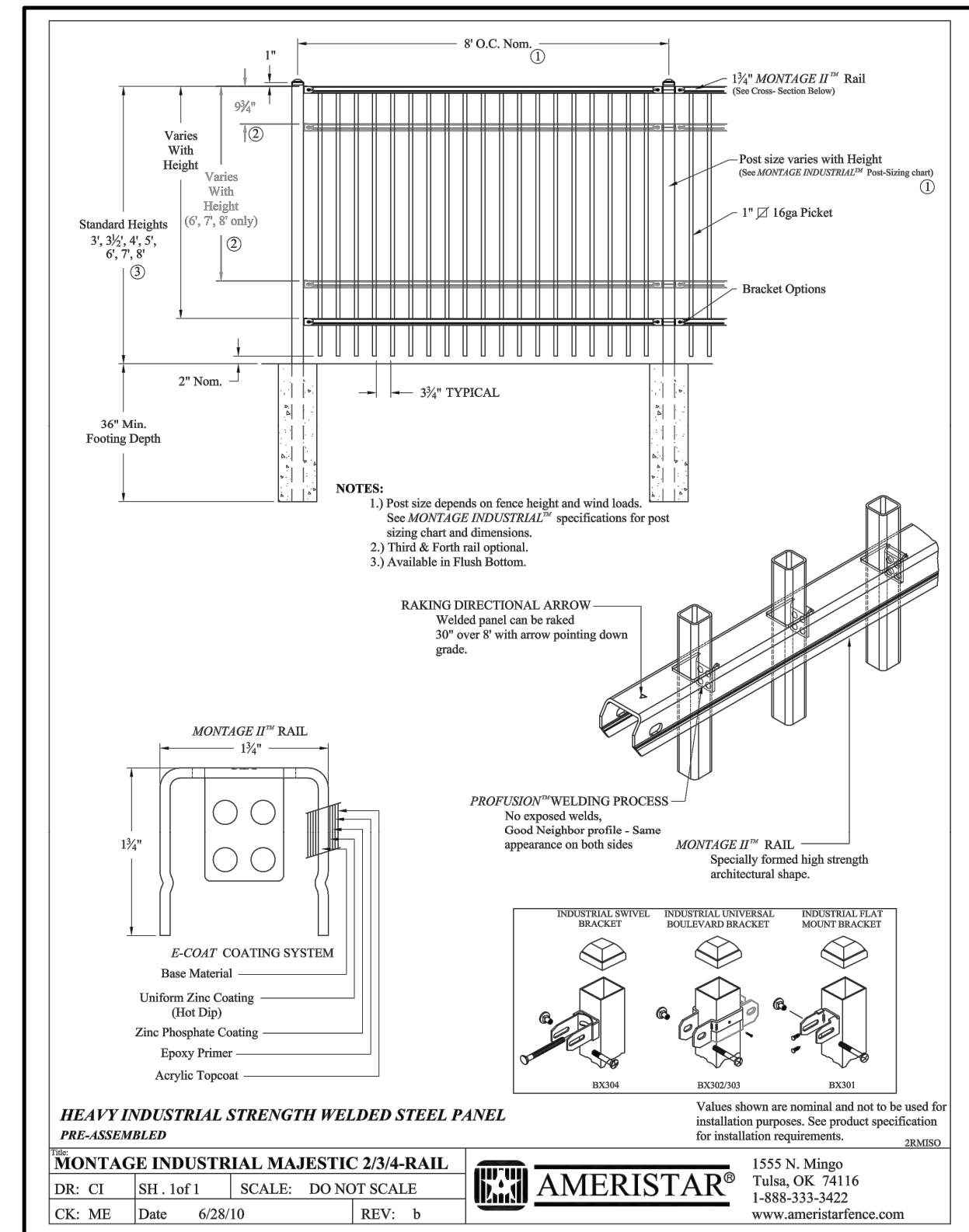
DATE: 05-19-2022

SHEET NUMBER:

01
01 OF 14

LEGEND

	PROPERTY LINE		PAVEMENT MARKINGS
	PROPOSED CURB & GUTTER		STOP BAR & SIGN
	EXISTING CURB & GUTTER		PROPOSED SIGN
	CONCRETE SIDEWALK		ACCESSIBLE PARKING SPACE
	CONCRETE		EX / PR LIGHT POLE
	CONCRETE ROLLOVER CURB		EXISTING UTILITY POLE
	COLOR CONCRETE		
	OVERHEAD ELECTRIC		



FENCE DETAIL DETAIL

NOT TO SCALE

SITE DATA

STORE TYPE	MARKET
CANOPY / DISPENSER ARRANGEMENT	6 DISPENSERS (SINGLE)
TYPE OF USE	CONVENIENCE STORE WITH FUEL
HEIGHT	BUILDING 18'-0"
	TOP OF BLADE WALL 22'-8"
	CANOPY 21'-10" (MIN.)
GROSS FLOOR AREA	5,620 S.F.
GROSS CANOPY AREA	3,936 S.F.
GROSS LOT AREA	±109,760 S.F. ±2,519 ACRES
KUM & GO PARCEL	±96,660 S.F. ±2,219 ACRES
EXCEPTED PARCEL	±13,100 S.F. ±0.30 ACRES
FAR (INCLUDING CANOPY)	N/A
BUILDING COVERAGE	5,620 S.F. (5.12%)
LANDSCAPE COVERAGE	53,190 S.F. (48.5%)
PARKING/DRIVE/SIDEWALK COVERAGE	50,950 S.F. (46.4%)
BICYCLE PARKING	2 SPACES
REQUIRED PARKING	STANDARD 23 ADA 1 TOTAL 24 PARKING RATIO = 1 CAR/250 S.F.
PROVIDED PARKING	STANDARD 24 ADA 2 TOTAL 26 PARKING RATIO = 1.16 CARS/250 S.F.

SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVDB8)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVDB8)



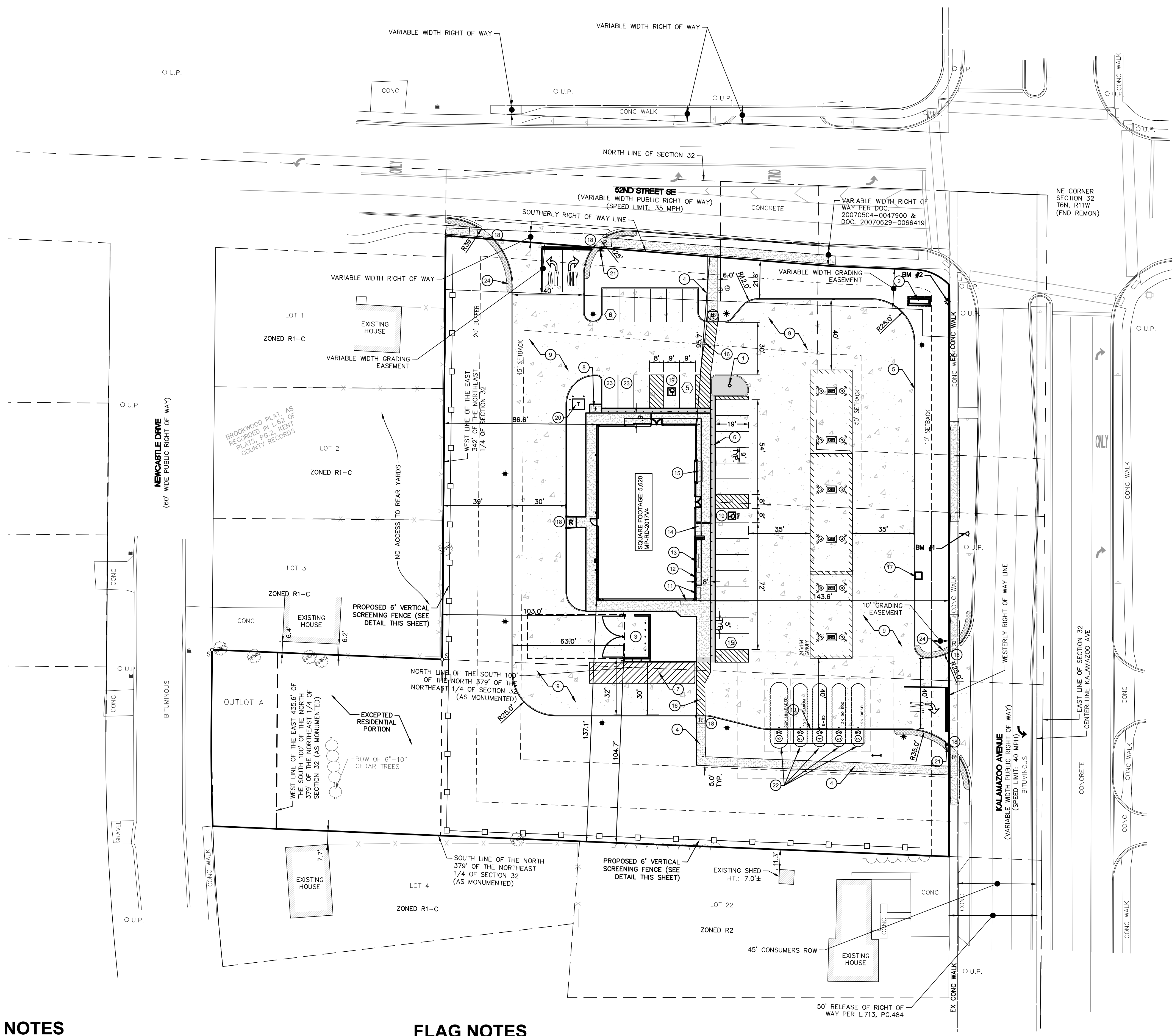
0 15' 30'
SCALE: 1"=30'

NOTES

- REFER TO THE ALTA/ACSM LAND TITLE SURVEY FOR ADDITIONAL EXISTING FEATURES AND PROPERTY BOUNDARY INFORMATION.
- ALL DIMENSIONS ARE TO EDGE OF BUILDING, FACE OF CURB, OR EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED.
- REFER TO ARCHITECTURAL / FOUNDATION PLANS FOR BUILDING DIMENSIONS.
- ALL RADII ARE 5' UNLESS OTHERWISE NOTED. ALL RADII ARE DIMENSIONED FROM BACK OF CURB.
- A SEPARATE SIGN APPLICATION TO THE CITY OF KENTWOOD IS REQUIRED FOR ALL SIGNS.

FLAG NOTES

- | | | |
|------------------------------------------------|--------------------------------------------|------------------------------------------------------------|
| 1. PROPOSED 40' FLAGPOLE | 10. PAINT CURB YELLOW ALONG FRONT OF TANKS | 19. ACCESSIBLE PARKING SPACE WITH BOLLARD MOUNTED ADA SIGN |
| 2. PROPOSED 25' MONUMENT SIGN | 11. PROPANE CAGE | 20. PAD-MOUNTED TRANSFORMER |
| 3. 14'-8" X 24'-8" TRASH ENCLOSURE WITH NICHHA | 12. ICE MERCHANDISE | 21. STOP SIGN |
| 4. PROPOSED SITE SIDEWALK | 13. FIREWOOD | 22. UNDERGROUND FUEL STORAGE TANKS |
| 5. PROPOSED INTEGRAL CURB | 14. MISC. MERCHANDISE | 23. DESIGNATED FUTURE EV STALLS |
| 6. PROPOSED ZERO CURB WITH BOLLARDS | 15. REDBOXE(S) | 24. ROLL OVER CURB |
| 7. PROPOSED LOADING ZONE | 16. PROPOSED CROSSWALK | 25. STANDALONE VENT RACK |
| 8. PROPOSED BIKE RACK - WAVE STYLE | 17. PROPOSED AIR MACHINE ON 4'X4' PAD | |
| 9. PROPOSED CONCRETE PAVEMENT | 18. PROPOSED ADA RAMP | |



1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISION DESCRIPTION	DATE

DATE: 05-19-2022
SHEET NUMBER: 02
02 OF 14

ALTA/NSPS LAND TITLE SURVEY

SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVD88)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVD88)



0 15' 30'
SCALE: 1"=30'

LEGEND

⊕	SECTION CORNER
⊗	FOUND MONUMENT
×	FOUND MAG NAIL
○	FOUND IRON ROD
○	FOUND CAPPED IRON ROD
○	FOUND IRON PIPE
○	SET CAPPED IRON ROD
---	BOUNDARY LINE
---	EASEMENT LINE
---	SECTION LINE
---	BOUNDARY ADJACENT LINE
---	SCHEDULE B, PART II EXCEPTION
(M)	MEASURED
(R)	RECORDED
(P)	PLATTED
P.O.C.	POINT OF COMMENCEMENT
P.O.B.	PLACE OF BEGINNING

SCHEDULE C DESCRIPTION PER ALTA COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY, COMMITMENT NO.: NCS-1111238-OMHA, COMMITMENT DATE: JANUARY 21, 2022:

LAND IN THE CITY OF KENTWOOD, KENT COUNTY, MI, DESCRIBED AS FOLLOWS:

PARCEL 1:
THE NORTH 279 FEET OF THE EAST 342 FEET OF THE NORTHEAST 1/4, EXCEPT COMMENCING AT THE NORTHEAST CORNER OF SECTION; THENCE WEST ALONG THE NORTH SECTION LINE 185.93 FEET; THENCE SOUTHERLY PARALLEL WITH CENTERLINE OF KALAMAZOO AVENUE 183.02 FEET; THENCE EAST PARALLEL WITH THE NORTH SECTION LINE 184.32 FEET TO THE EAST SECTION LINE; THENCE NORTH 183.02 FEET TO BEGINNING AND EXCEPT COMMENCING 185.93 FEET, 90 DEGREES, 0 MINUTES, 0 SECONDS WEST ALONG THE NORTH SECTION LINE FROM THE NORTHEAST CORNER OF THE SECTION; THENCE SOUTH 2 DEGREES, 25 MINUTES, 54 SECONDS EAST 43.21 FEET; THENCE NORTH 87 DEGREES, 53 MINUTES, 40 SECONDS WEST 156.75 FEET; THENCE NORTH 1 DEGREE, 55 MINUTES, 34 SECONDS WEST TO THE NORTH SECTION LINE; THENCE EAST ALONG THE NORTH SECTION LINE TO BEGINNING, SECTION 32, TOWN 6 NORTH, RANGE 11 WEST, CITY OF KENTWOOD, KENT COUNTY, STATE OF MICHIGAN.

PARCEL 2:
PART OF THE NORTHEAST 1/4 COMMENCING AT THE NORTHEAST CORNER OF SECTION; THENCE 90 DEGREES, 0 MINUTES, 0 SECONDS WEST ALONG THE NORTH SECTION LINE 55.96 FEET; THENCE SOUTH 2 DEGREES, 25 MINUTES, 54 SECONDS EAST 53.35 FEET; THENCE NORTHWESTERLY 21.25 FEET ALONG A 27.50 FOOT RADIUS CURVE TO THE LEFT (LONG CHORD BEARS NORTH 67 DEGREES, 52 MINUTES, 5 SECONDS WEST 20.72 FEET); THENCE 90 DEGREES, 0 MINUTES, 0 SECONDS WEST 47.93 FEET; THENCE NORTH 87 DEGREES, 53 MINUTES, 40 SECONDS WEST 63.33 FEET; THENCE SOUTHERLY PARALLEL WITH THE CENTERLINE OF KALAMAZOO AVENUE 106.79 FEET; THENCE EAST PARALLEL WITH THE NORTH SECTION LINE 184.32 FEET TO THE EAST SECTION LINE; THENCE NORTH 183.02 FEET TO BEGINNING, SECTION 32, TOWN 6 NORTH, RANGE 11 WEST, CITY OF KENTWOOD, KENT COUNTY, STATE OF MICHIGAN.

PARCEL 3:
THE EAST 435.6 FEET OF THE SOUTH 100 FEET OF THE NORTH 379 FEET OF THE NORTHEAST 1/4 OF SECTION 32, TOWN 5 NORTH, RANGE 21 WEST, CITY OF KENTWOOD, KENT COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 32; THENCE SOUTH 00 DEGREES 00 MINUTES WEST ALONG THE EAST LINE OF SAID SECTION 279.00 FEET TO THE POINT OF BEGINNING OF THIS DESCRIPTION; THENCE NORTH 88 DEGREES 10 MINUTES WEST ALONG THE NORTH LINE OF SAID SOUTH 100 FEET 341.98 FEET; THENCE NORTH 00 DEGREES 00 MINUTES EAST 0.83 FEET TO THE SOUTHEAST CORNER OF LOT 3 OF BROOKWOOD PLAT, AS RECORDED IN LIBER 62 OF PLATS ON PAGE 2; THENCE NORTH 88 DEGREES 04 MINUTES WEST ALONG THE SOUTH LINE OF SAID LOT 93.60 FEET; THENCE SOUTH 00 DEGREES 00 MINUTES EAST ALONG THE EAST LINE OF SAID LOT 100.00 FEET; THENCE SOUTH 88 DEGREES 04 MINUTES EAST ALONG THE NORTH LINE OF LOTS 4 AND 22 OF SAID PLAT 435.60 FEET TO THE EAST LINE OF SAID SECTION 32; THENCE NORTH 00 DEGREES 00 MINUTES EAST ALONG THE EAST LINE OF SAID SECTION 99.79 FEET TO THE POINT OF BEGINNING.

PARCEL 4
OUTLOT A, BROOKWOOD PLAT, ACCORDING TO THE PLAT THEREOF AS RECORDED IN LIBER 62 OF PLATS, ON PAGE 2, KENT COUNTY RECORDS.

SURVEYOR'S CERTIFICATE

TO: -FIRST AMERICAN TITLE INSURANCE COMPANY
-KUM & GO, L.C., AN IOWA LIMITED LIABILITY COMPANY

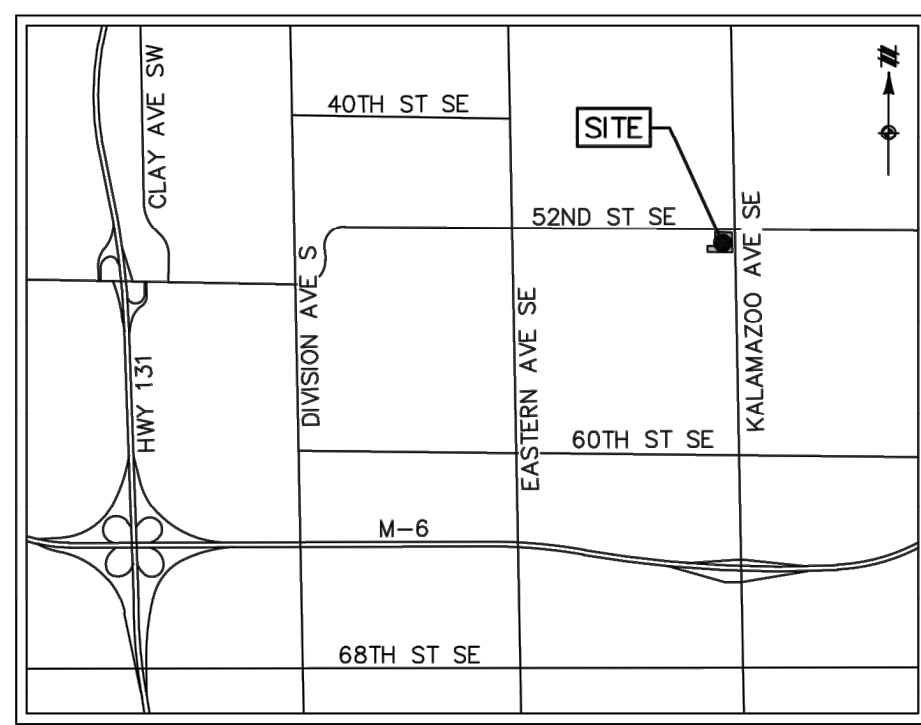
THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 6(a), 6(b), 7(a), 7(b)(1), 7(c), 8, 9, 11(a)(b), 13, 16, 17, 19, 20(a)(WETLANDS) AND 20(b)(SPEED LIMIT) OF TABLE A THEREOF.

FIELD WORK WAS COMPLETED ON FEBRUARY 25, 2022.

9

CHRISTOPHER P. KELLY
PROFESSIONAL SURVEYOR NO. 4001056500
CKELLY@ATWELL-GROUP.COM
TWO TOWNE SQUARE, SUITE 700
SOUTHFIELD, MICHIGAN 48076
248.447.2000

DATE: 03/07/2022



VICINITY MAP
NOT TO SCALE

ZONING:

THE SUBJECT PROPERTY IS CURRENTLY ZONED CPUD (COMMERCIAL PLANNED UNIT DEVELOPMENT) PER CITY OF KENTWOOD ZONING REPORT LETTER DATED FEBRUARY 6, 2003. BUILDING SETBACKS ARE TO BE DETERMINED AT THE TIME OF CITY ENTITLEMENT APPROVAL.

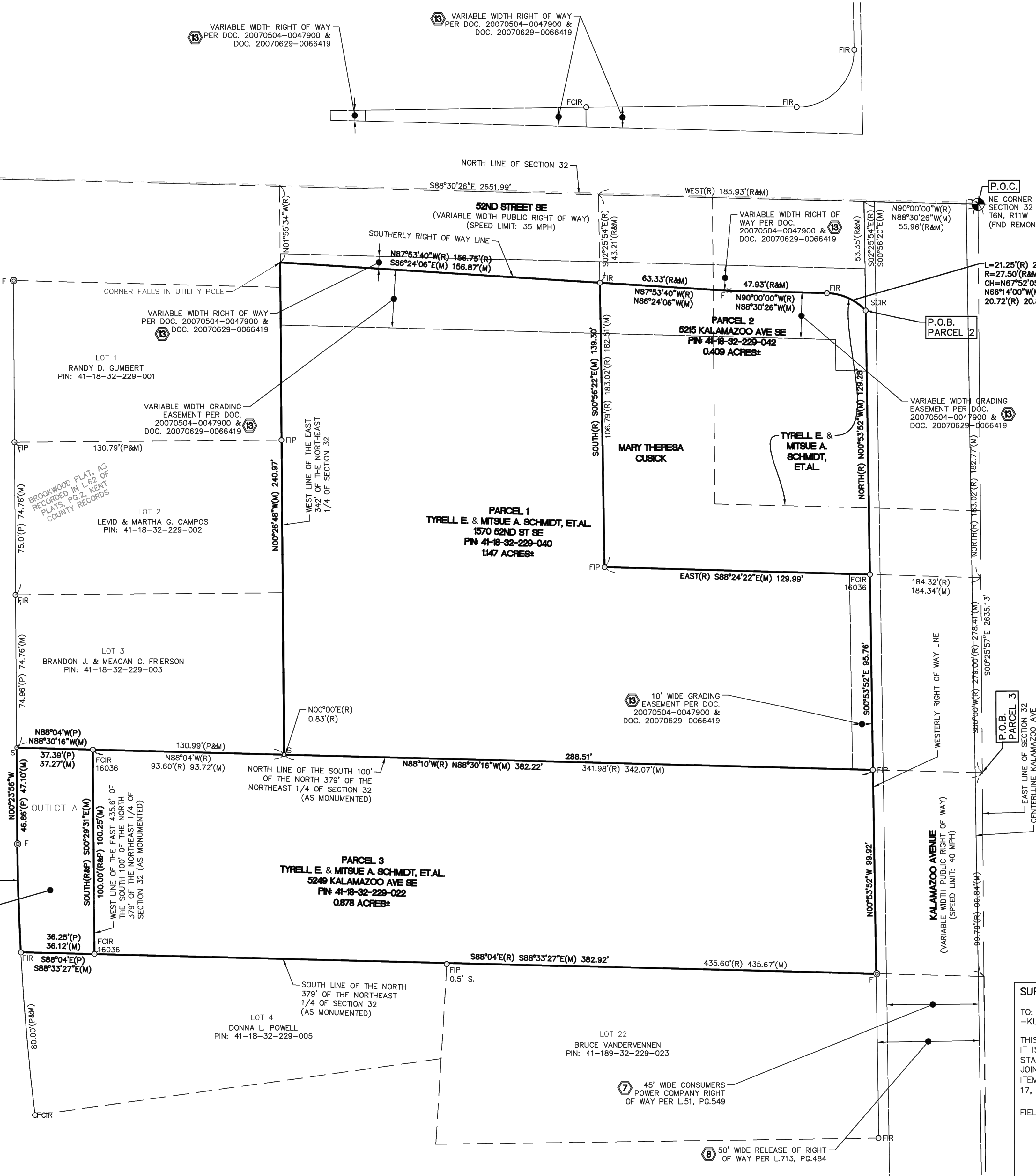
SCHEDULE B, PART II EXCEPTIONS PER ALTA COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY, COMMITMENT NO.: NCS-1111238-OMHA, COMMITMENT DATE: JANUARY 21, 2022:

- THE TERMS, PROVISIONS AND EASEMENT(S) CONTAINED IN THE DOCUMENT ENTITLED "RIGHT OF WAY" RECORDED AUGUST 14, 1930 AS LIBER 51, PAGE 549 OF OFFICIAL RECORDS. (AFFECTS PARCELS 1, 2 AND 3)
RESPONSE: COVERS SUBJECT PROPERTY AND ADDITIONAL LAND, EASEMENT AS SHOWN HEREON.
- THE TERMS, PROVISIONS AND EASEMENT(S) CONTAINED IN THE DOCUMENT ENTITLED "RELEASE OF RIGHT OF WAY" RECORDED AS LIBER 713, PAGE 484 OF OFFICIAL RECORDS. (AFFECTS PARCELS 1, 2 AND 3)
RESPONSE: COVERS KALAMAZOO AVENUE, AS SHOWN HEREON.
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "RESTRICTIVE AGREEMENT" RECORDED MAY 16, 1962 AS LIBER 935, PAGE 145 OF OFFICIAL RECORDS. (AFFECTS PARCEL 2)
RESPONSE: COVERS PARCEL 2.
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "DEED" RECORDED MAY 4, 1976 AS LIBER 2215, PAGE 692 OF OFFICIAL RECORDS. (AFFECTS PARCEL 4)
RESPONSE: COVERS PARCEL 4 AND ADDITIONAL LAND.
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "RESTRICTIVE COVENANT" RECORDED OCTOBER 20, 1998 AS LIBER 4509, PAGE 45 OF OFFICIAL RECORDS. (AFFECTS PARCEL 2)
RESPONSE: COVERS PARCEL 2, CONTAINS USE RESTRICTIONS.
- THE FACT, AS DISCLOSED BY THAT CERTAIN DOCUMENT OR DOCUMENTS RECORDED OCTOBER 20, 1998 AS LIBER 4509, PAGE 45 OF OFFICIAL RECORDS, THAT SOME VIOLATION OF ENVIRONMENTAL PROTECTION LAWS MAY HAVE OCCURRED WHICH MAY AFFECT THE LAND.
RESPONSE: COVERS PARCEL 2, CONTAINS USE RESTRICTIONS.
- THE TERMS, PROVISIONS AND EASEMENT(S) CONTAINED IN THE DOCUMENT ENTITLED "DECLARATION OF TAKING" RECORDED MAY 4, 2007 AS INSTRUMENT NO. 20070504-0047900 OF OFFICIAL RECORDS. (AFFECTS PARCELS 1 AND 2)
RESPONSE: EASEMENTS AS SHOWN HEREON.
- THE TERMS, PROVISIONS AND EASEMENT(S) CONTAINED IN THE DOCUMENT ENTITLED "ORDER VESTING TITLE AND POSSESSION" RECORDED JUNE 29, 2007 AS INSTRUMENT NO. 20070629-0066419 OF OFFICIAL RECORDS. (AFFECTS PARCELS 1 AND 2)
RESPONSE: EASEMENTS AS SHOWN HEREON.

L=53.18'(P) 53.24'(M)
R=1200.00'
D=232'32"
CH=501'48'55"E
53.24'

PARCEL 4
TYRELL E. & MITSUE A. SCHMIDT, ETAL
5250 NEWCASTLE DRIVE SE
PIN: 41-18-32-229-004
0.085 ACRES

NEWCASTLE DRIVE
(60' WIDE PUBLIC RIGHT OF WAY)



866.850.4200 www.atwell-group.com
12745 23 MILE ROAD, SUITE 200
SHELBY TOWNSHIP, MI 48315
(248) 768-9600

P.L.: CLK DRAWN BY: JR JOB #: 22000815

1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SEC OF 52ND ST. SE & KALAMAZOO AVE SE

REVISION DESCRIPTION	DATE

DATE: 02-28-2022

SHEET NUMBER: 03
03 OF 14

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISIONS

ALTA/NSPS LAND TITLE SURVEY

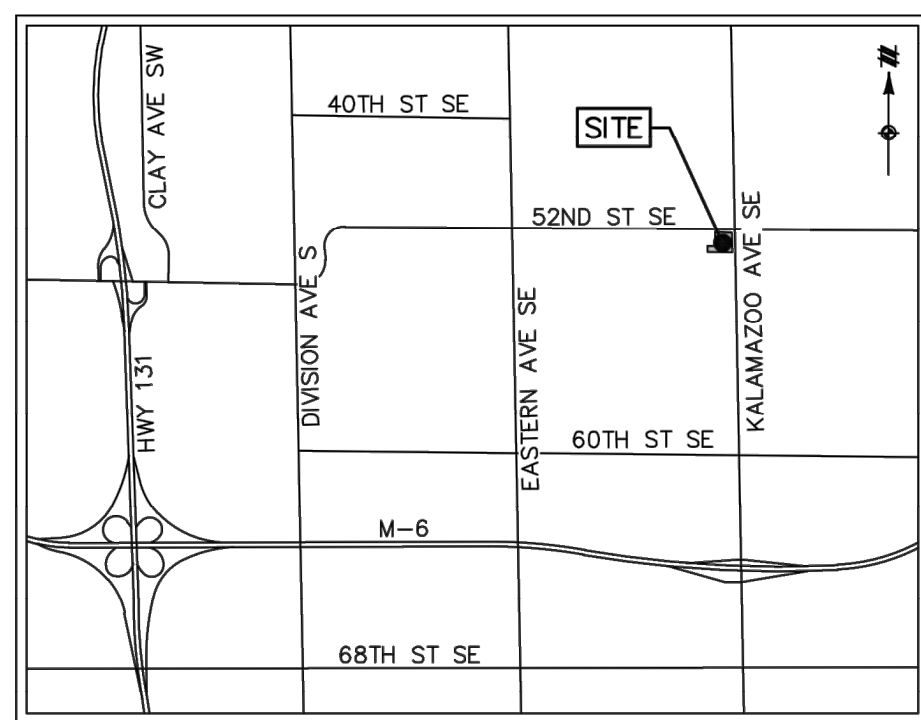
SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVD88)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVD88)



0 15' 30'
SCALE: 1"=30'



VICINITY MAP
NOT TO SCALE

LEGEND

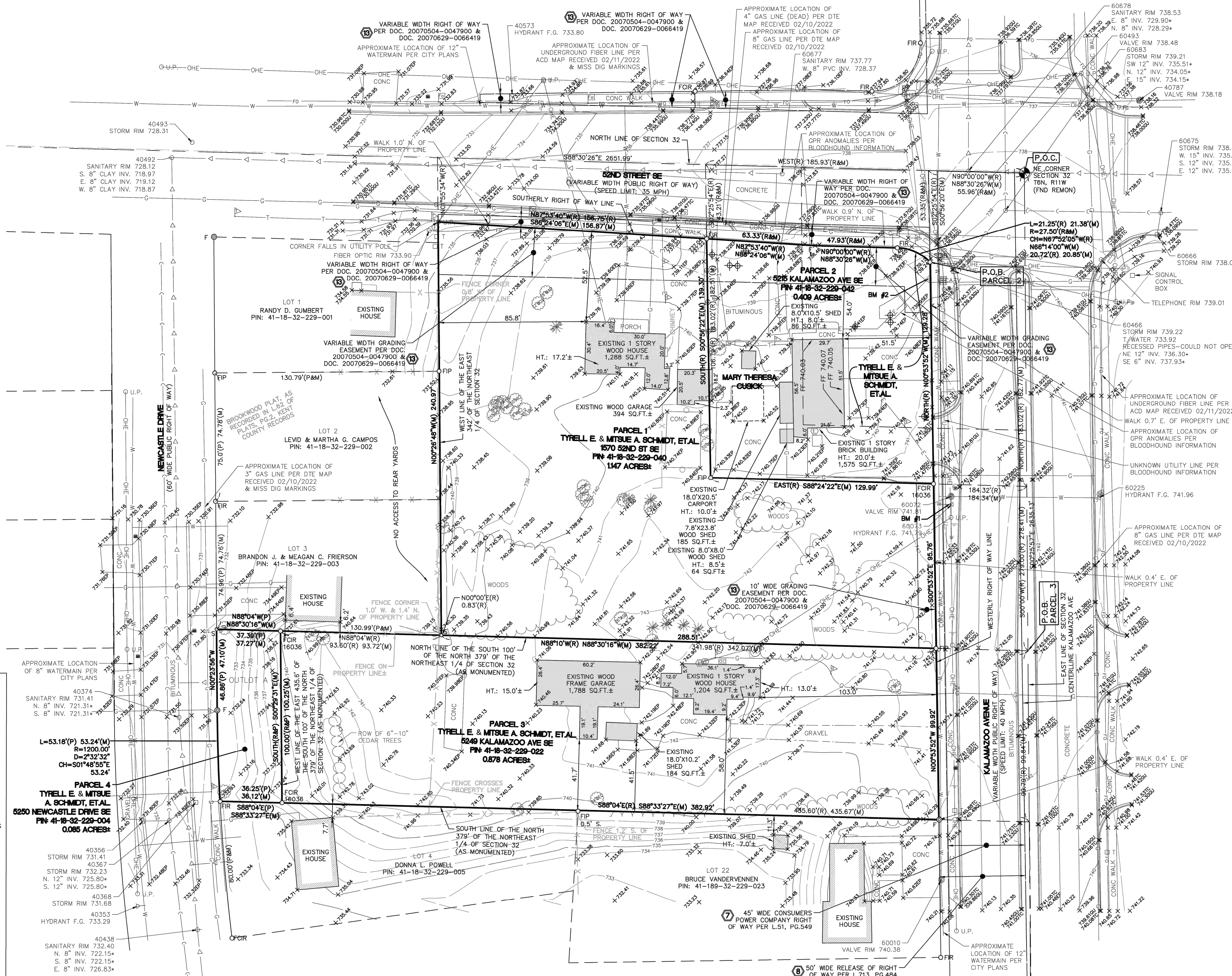
	SECTION CORNER
	FOUND MONUMENT
	FOUND IRON ROD
	FOUND CAPPED IRON ROD
	FOUND IRON PIPE
	SET CAPPED IRON ROD
	EXISTING CABLE RISER
	EXISTING UTILITY POLE
	EXISTING GUY ANCHOR
	EXISTING SIGN
	EXISTING ELECTRIC METER
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE
	EXISTING TREE
	EXISTING SHRUB
	EXISTING LIGHT POLE
	EXISTING TELEPHONE PEDISTAL
	EXISTING GAS METER
	EXISTING MANHOLE/CATCH BASIN
	EXISTING TRAFFIC SIGNAL POLE
	EXISTING AIR CONDITIONING UNIT
	EXISTING UTILITY HANDHOLE
	EXISTING MAILBOX
	EXISTING MONITORING WELL
	UNDERGROUND TELEPHONE MARKER
	UNDERGROUND WATER MARKER
	UNDERGROUND ELECTRIC MARKER
	UNDERGROUND FIBER OPTIC MARKER
	BOUNDARY LINE
	EASEMENT LINE
	SECTION LINE
	BOUNDARY ADJACENT LINE
	EXISTING FENCE
	EXISTING LIMITS OF VEGETATION
	UNDERGROUND FIBER OPTIC LINE
	UNDERGROUND UTILITY LINE
	UNDERGROUND STORM LINE
	OVERHEAD UTILITY LINE
	UNDERGROUND ELECTRIC LINE
	UNDERGROUND TELEPHONE LINE
	UNDERGROUND WATER LINE
	EXISTING CURB AND GUTTER
	EXISTING GROUND CONTOUR
	EXISTING BUILDING
	SCHEDULE B, PART II EXCEPTION
	EXISTING GROUND ELEVATION
	EXISTING TOP OF CURB ELEVATION
	EXISTING GUTTER ELEVATION
	EXISTING EDGE OF PAVEMENT ELEVATION
	MEASURED RECORD
	PLATTED
	POINT OF COMMENCEMENT
	PLACE OF BEGINNING
	PER PLAN

NOTES:

- BEARINGS ARE BASED ON MICHIGAN STATE PLANE COORDINATE SYSTEM (NAD83), SOUTH ZONE, INTERNATIONAL FEET, GROUND DISTANCES. MEASURED BEARINGS DIFFER FROM TITLE. VERTICAL DATUM IS BASED ON NAVD88.
- THE SITE SHOWN HEREON FALL ON PANEL 2601070005B OF THE FLOOD INSURANCE RATE MAP, EFFECTIVE 11/18/1981, PANEL NOT PRINTED, AREA OF MINIMAL FLOOD HAZARDS.
- WATER MAIN, STORM SEWER, SANITARY SEWER AND FRANCHISE UTILITY STRUCTURES HAVE BEEN FIELD LOCATED WHERE VISIBLE, UTILITY AND AS-BUILT MAPS HAVE BEEN REQUESTED AND SOME MAPS HAVE BEEN RECEIVED AT DATE OF THIS SURVEY. FRANCHISE UTILITY MAPS HAVE BEEN REQUESTED FROM THE APPROPRIATE FRANCHISE COMPANIES, BUT NOT ALL MAPS HAVE BEEN RECEIVED AT DATE OF SURVEY.

NOTE: THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN-SERVICE OR ABANDONED.

NOTE TO THE CLIENT, INSURER, AND LENDER - WITH REGARD TO TABLE A, ITEM 11, SOURCE INFORMATION FROM PLANS AND MARKINGS WILL BE COMBINED WITH OBSERVED EVIDENCE OF UTILITIES PURSUANT TO SECTION 5.E.IV. TO DEVELOP A VIEW OF THE UNDERGROUND UTILITIES. HOWEVER, LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY, AND RELIABLY DEPICTED. IN ADDITION, IN SOME JURISDICTIONS, 811 OR OTHER SIMILAR UTILITY LOCATE REQUESTS FROM SURVEYORS MAY BE IGNORED OR RESULT IN AN INCOMPLETE RESPONSE, IN WHICH CASE THE SURVEYOR SHALL NOTE ON THE PLAT OR MAP HOW THIS AFFECTED THE SURVEYOR'S ASSESSMENT OF THE LOCATION OF THE UTILITIES. WHERE ADDITIONAL OR MORE DETAILED INFORMATION IS REQUIRED, THE CLIENT IS ADVISED THAT EXCAVATION AND/OR A PRIVATE UTILITY LOCATE REQUEST MAY BE NECESSARY.
- THERE WAS NO EVIDENCE OF RECENT STREET OR SIDEWALK CONSTRUCTION OR REPAIRS OBSERVED IN THE PROCESS OF CONDUCTING THE FIELDWORK.
- THERE WAS NO EVIDENCE OF RECENT EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS OBSERVED IN THE PROCESS OF CONDUCTING THE FIELDWORK.
- THE SURVEYED PROPERTY CONTAINS NO OBSERVED PARKING STALLS AT THE DATE OF THE FIELDWORK.
- NO UNDELINEATED WETLANDS WERE OBSERVED AT THE DATE OF THE FIELDWORK. ACCORDING TO THE NATIONAL WETLANDS INVENTORY MAPPER, THE SUBJECT PROPERTY CONTAINS NO WETLANDS.
- SNOW AND ICE CONDITIONS EXISTED IN THE FIELD THAT MAY RESULT IN SOME PHYSICAL IMPROVEMENTS NOT BEING VISIBLE AND THEREFORE MAY NOT BE LOCATED AS PART OF THIS SURVEY.



1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SEC OF 52ND ST. SE & KALAMAZOO AVE SE

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

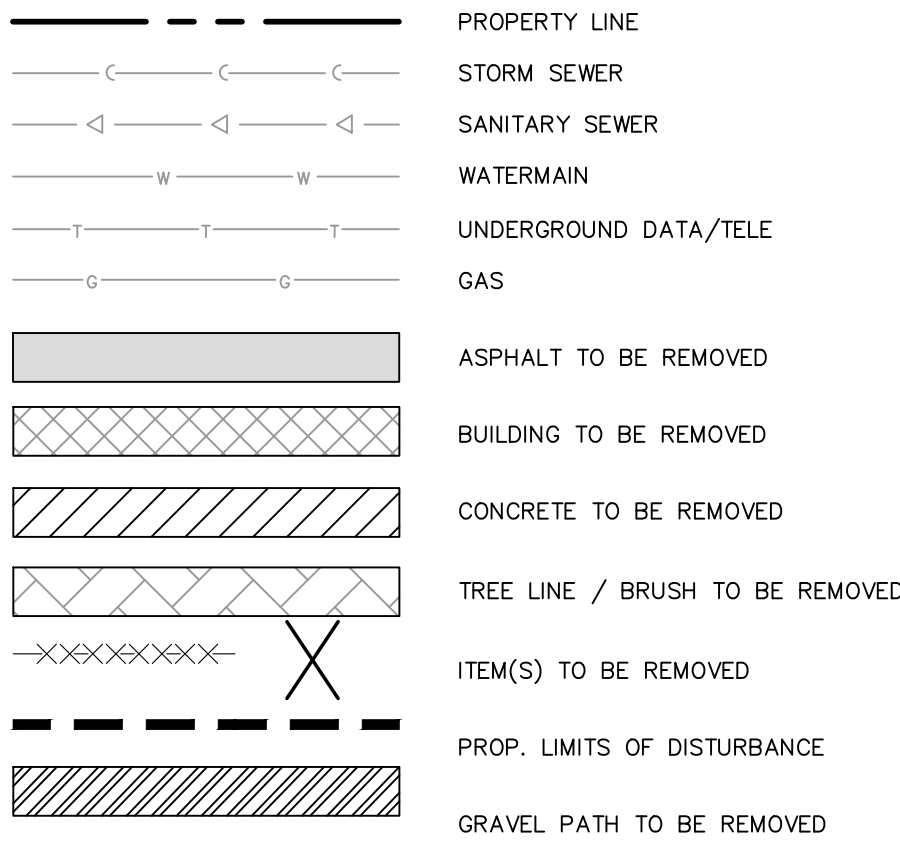
REVISION DESCRIPTION	DATE

DATE: 02-28-2022
SHEET NUMBER: 04
04 OF 14

REVISIONS

ATWELL
866.850.4200 www.atwell-group.com
12745 23 MILE ROAD, SUITE 200
SHELBY TOWNSHIP, MI 48315
(248) 788-8600
P.M.: CPK DRAWN BY: JR JOB #: 22000815

LEGEND



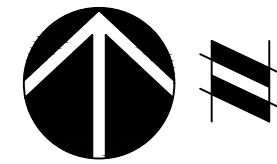
NOTES

1. ANY ITEM NOT INDICATED AS BEING REMOVED SHALL REMAIN.
2. FOR ADDITIONAL INFORMATION REFERENCE THE STANDARD NOTES SHEET, STANDARD DETAILS SHEET(S), AND ANY MUNICIPALITY AND/OR JURISDICTIONAL DETAILS ATTACHED TO THIS PLAN SET.
3. CONTRACTOR SHALL CALL "MISS DIG" AT LEAST THREE (3) WORKING DAYS PRIOR TO CONSTRUCTION/DEMOLITION.
4. CONTRACTOR SHALL COORDINATE ALL UTILITY REMOVAL AND ABANDONMENT ACTIVITIES WITH LOCAL GOVERNING AGENCY OR UTILITY COMPANY PRIOR TO STARTING DEMOLITION TO INSURE COMPLIANCE WITH GOVERNING AGENCY AND UTILITY COMPANY REMOVAL AND ABANDONMENT STANDARDS.
5. CONTRACTOR RESPONSIBLE FOR COORDINATING UTILITY TERMINATIONS & OBTAINING DEMOLITION PERMIT FROM THE CITY OF PORTAGE PRIOR TO BEGINNING CONSTRUCTION.
6. CONTRACTOR SHALL COMPLY AND ADHERE TO, AS APPLICABLE, ANY REQUIREMENTS RELATED TO INTERACTING WITH OR MANAGING ENVIRONMENTAL CONTAMINATION THAT MAY COINCIDE WITH DEMOLITION ACTIVITIES.

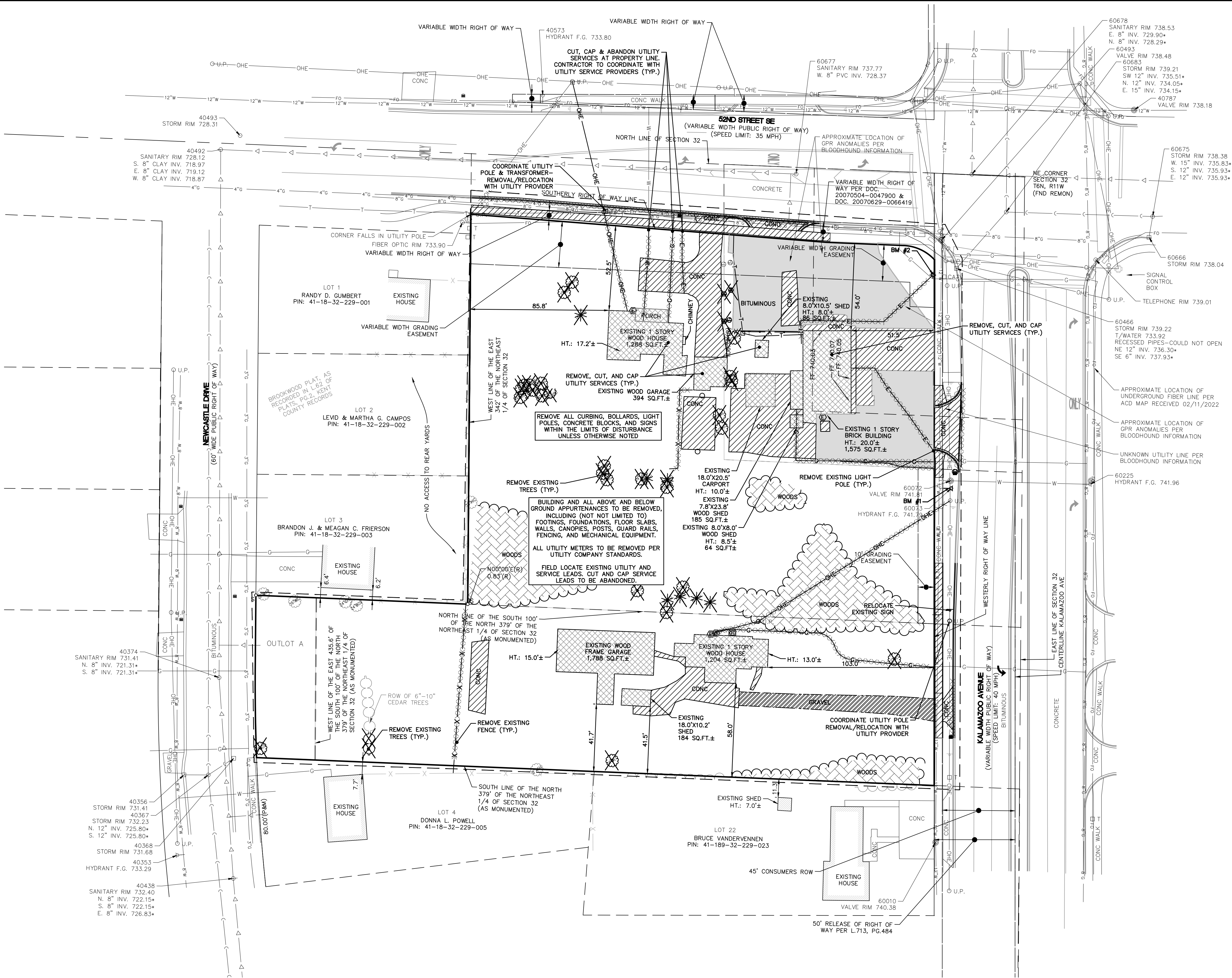
SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVD88)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVD88)



0 15' 30'
SCALE: 1"=30'



ATWELL
866.850.4200 www.atwell-group.com
12745 23 MILE ROAD, SUITE 200
SHELLEYSBURG, MO 64683
(586) 786-5800



MICHAEL
McPHERSON
ENGINEER
No. 56843
MICHIGAN PROFESSIONAL ENGINEER



1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISION DESCRIPTION	DATE

DATE: 05-19-2022

SHEET NUMBER: 05

05 OF 14

REVISIONS

DEMOLITION PLAN

P.M.: MMC
DRAWN BY: JA
JOB #: 22000815

CAD FILE: K:\22000815\DWG\PLAN SETS\SITE-PRELIMINARY\22000815-04-DM

LEGEND

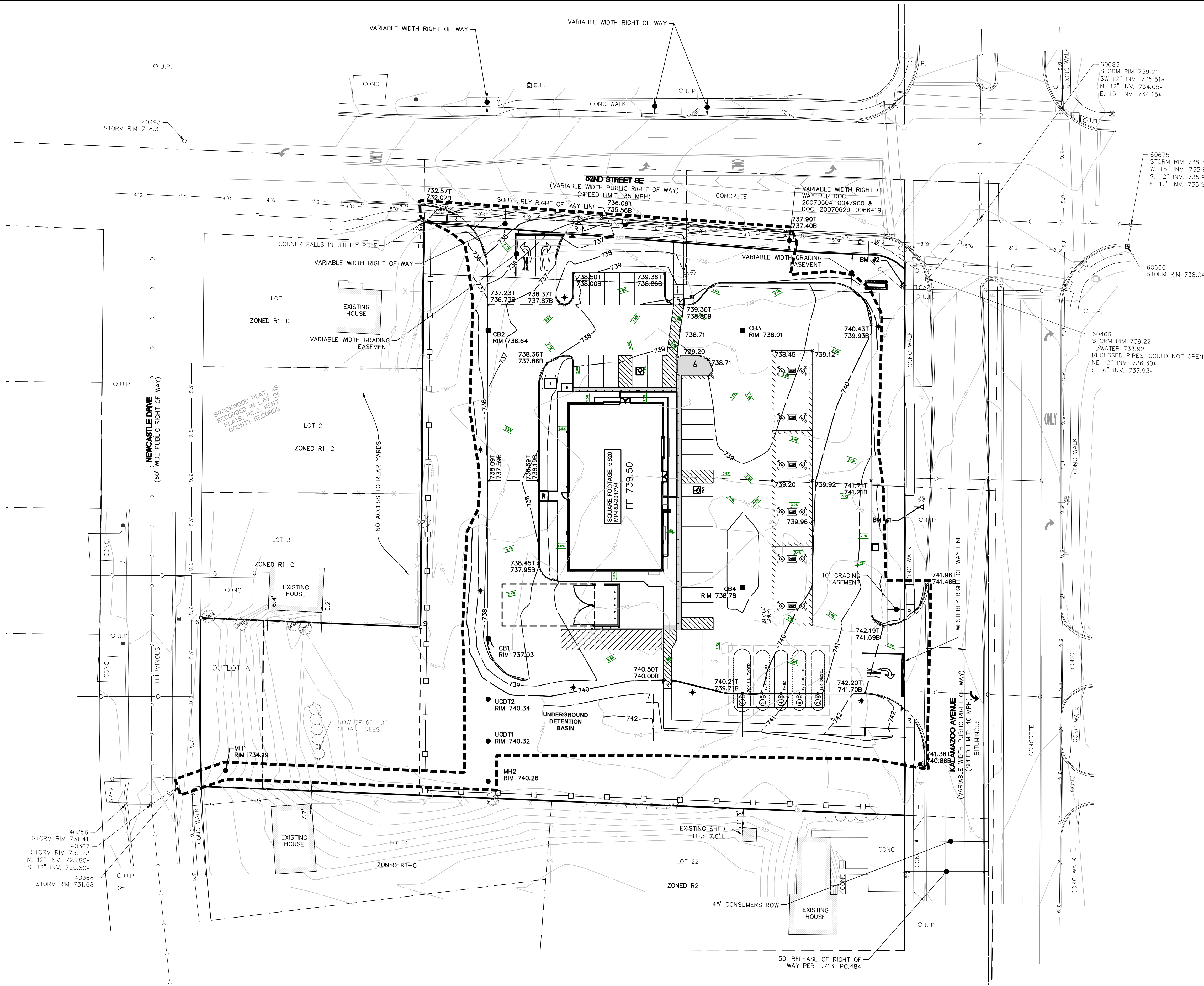
- PROPERTY LINE
---900--- EXISTING CONTOUR
---900--- PROPOSED CONTOUR
- - - - - LIMIT OF DISTURBANCE
- □ EXISTING MANHOLE/CATCH BASIN
● ■ PROPOSED MANHOLE/CATCH BASIN

NOTES

1. ALL ELEVATIONS SHOWN ARE TOP OF PAVEMENT/FINISH GRADE UNLESS OTHERWISE NOTED.

STORMWATER NARRATIVE

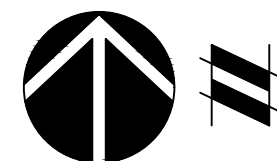
ON-SITE STORMWATER WILL GENERALLY SHEET FLOW TO A SERIES OF CATCH BASINS WHERE IT WILL BE CONVEYED TO THE ON-SITE UNDERGROUND DETENTION BASIN WHICH OUTLETS TO THE CITY STORM SYSTEM VIA A CONNECTION TO AN EXISTING CATCH BASIN TO THE WEST OF THE PROPERTY. THE PROPOSED PAVED AREA IS EQUIVALENT OR LESS THAN THE CURRENT PAVED AREA. STORMWATER FROM THE ROOF TOP AND CANOPY WILL BE CONVEYED TO VIA DOWNSPOUTS TO THE STORMWATER SEWER SYSTEM.



SITE BENCHMARKS

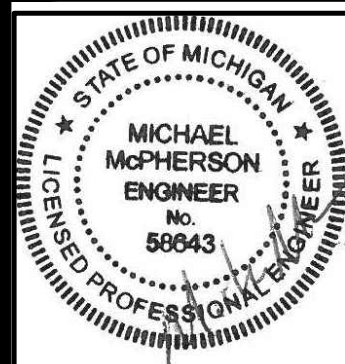
BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVD88)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVD88)



0 15' 30'
SCALE: 1"=30'

ATWELL
866.850.4200 www.atwell-group.com
12745 23 MILE ROAD, SUITE 200
SHELLEYSBURG, MO 64683
(586) 788-5800



1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

PRELIMINARY GRADING PLAN

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISIONS

REVISION DESCRIPTION	DATE

DATE: 05-19-2022

SHEET NUMBER:

06
06 OF 14

CAD FILE: K:\22000815\DWG\PLAN SETS\SITE-PRELIMINARY\22000815-06-0

LEGEND

	PROPERTY LINE		EXISTING / PROPOSED MANHOLE
	EXISTING OVERHEAD LINES		EXISTING / PROPOSED HYDRANT
	STORM SEWER		EXISTING / PROPOSED VALVE OR STOP BOX
	EXISTING SANITARY SEWER		EXISTING / PROPOSED LIGHT POLE
	EXISTING WATER		EXISTING / PROPOSED CATCH BASIN
	PROPOSED SANITARY SEWER		PROPOSED CLEANOUT
	PROPOSED WATER		EXISTING UTILITY POLE
	UNDERGROUND ELECTRIC		PROPOSED TRANSFORMER
	GAS		
	UNDERGROUND TELEPHONE/DATA		
	PROPOSED SAND BACKFILL		

NOTES

- FOR ADDITIONAL INFORMATION REFERENCE THE STANDARD NOTES SHEET, STANDARD DETAILS SHEET(S), AND ANY MUNICIPALITY AND/OR JURISDICTIONAL NOTES/DETAILS INCLUDED WITH THIS PLAN SET.
- ALL 6" DIAMETER SANITARY SEWER SHALL BE PVC SDR 23.5.
- NO FOOTING DRAINS SHALL BE CONNECTED TO THE BUILDING SEWER.
- NO CONNECTION RECEIVING STORM SEWER, SURFACE WATER OR GROUND WATER SHALL BE MADE TO THE SANITARY SEWER.
- ELECTRIC, TELEPHONE, AND DATA CONDUIT SHALL BE SCHEDULE 40 PVC OR TYPE DB DIRECT BURY TELEPHONE DUCT.
- A GALVANIZED PULL WIRE OR PURPLE PROPYLENE POLYETHYLENE ROPE WITH A MINIMUM PULLING TENSION OF 100 POUNDS SHALL BE INSTALLED IN THE CONDUIT.
- PULL BOX SIZE SHALL BE 17"x30"x28" "QUAZITE" OR APPROVED EQUAL.
- THE MINIMUM CONDUIT BURIAL DEPTH SHALL BE 24".
- AN ORANGE DETECTABLE HAZARD TAPE WITH INTEGRAL "BURIED CABLE" WORDING, SHALL BE INSTALLED 12" ABOVE CONDUIT.

SANITARY NARRATIVE

THE PROPOSED DEVELOPMENT WILL CONNECT TO AN EXISTING SANITARY LOCATED NORTH OF THE PROPERTY. ON-SITE SANITARY WILL CONSIST OF A 1,000 GALLON GREASE INTERCEPTOR AND 8" PVC SDR23.5 PIPE. CLEANOUTS WILL BE INSTALLED AT ALL CHANGES IN DIRECTION & ON RUNS OVER 100'.

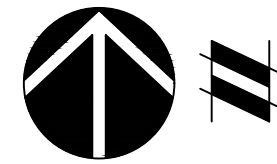
WATER NARRATIVE

ON-SITE WATER SERVICE WILL BE SUPPLIED VIA 2" TYPE "K" COPPER LEAD CONNECTED TO THE NEAREST ALLOWED TAP LOCATION. THE TAP IS CURRENTLY PROPOSED TO A WATER LINE TO THE EAST OF THE PROPERTY.

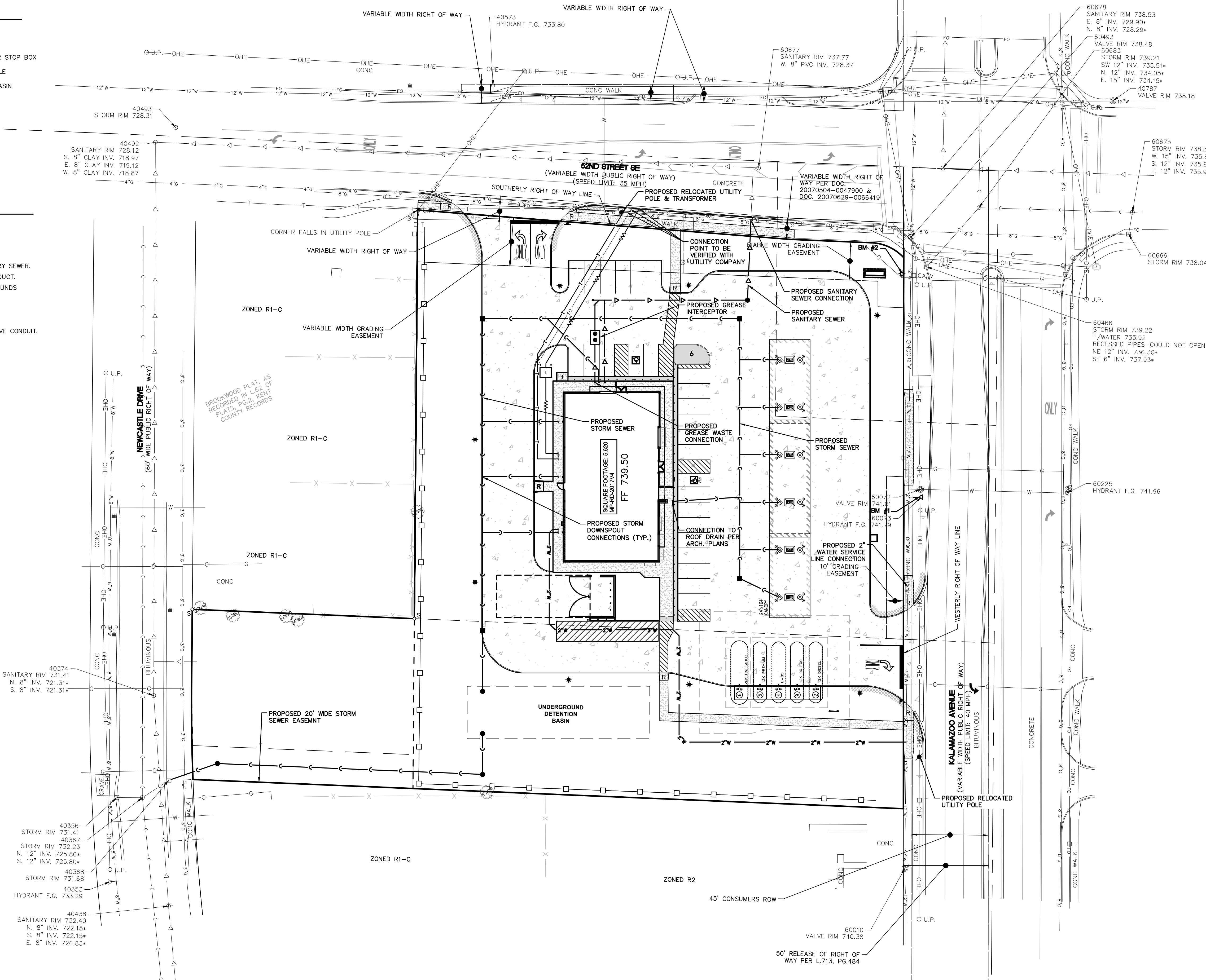
SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVD88)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVD88)



0 15' 30'
SCALE: 1"=30'



888.850.4200 www.atwell-group.com
12745 23 MILE ROAD, SUITE 200
SHELLEYSBURG, MO 64683
(580) 788-5800

P.M.: MMC

DRAWN BY: JA

JOB #: 22000815

STATE OF MISSOURI

MICHAEL McPHERSON
ENGINEER
No. 58643

REGISTERED PROFESSIONAL ENGINEER

1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI

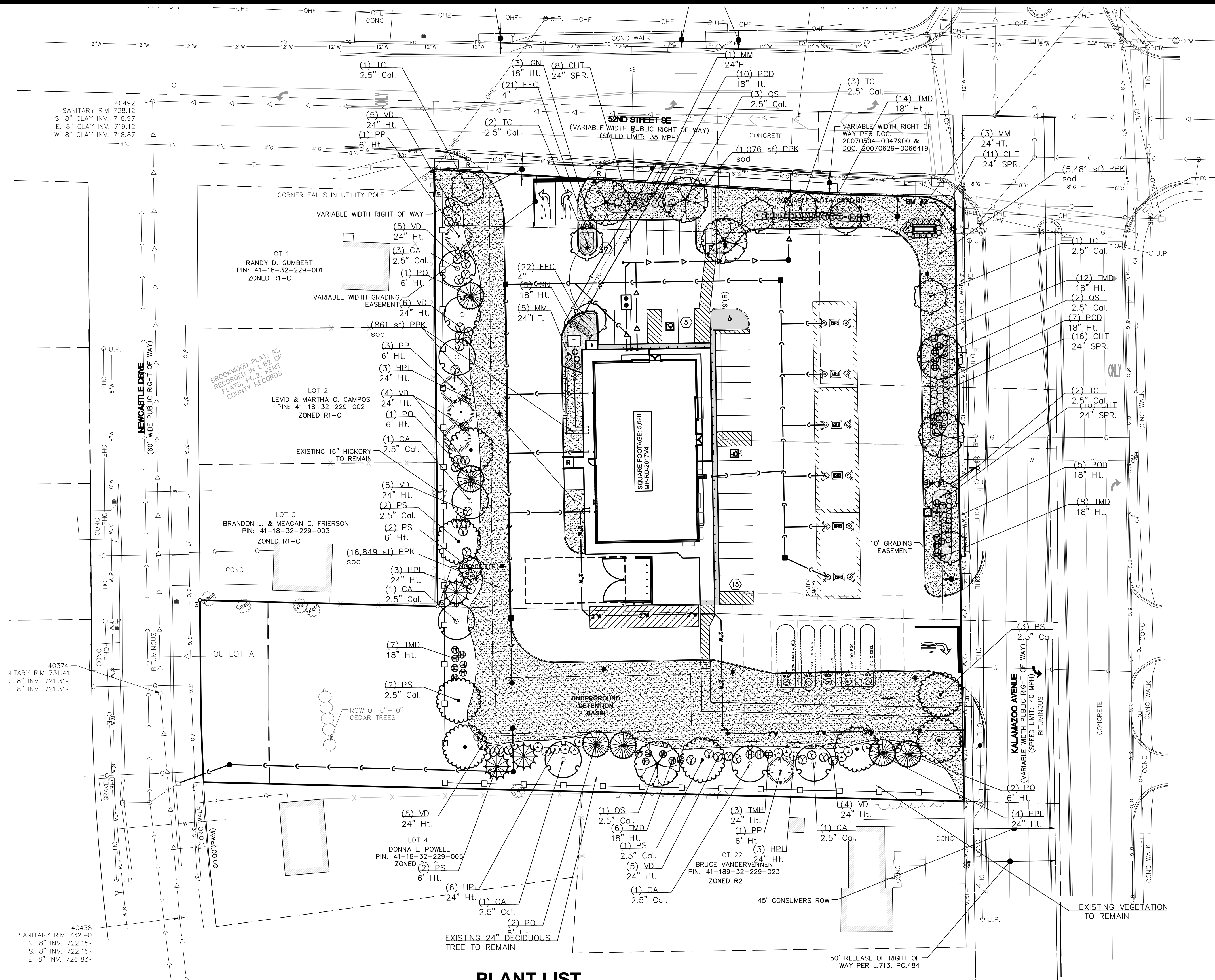
SWC OF 52ND ST SE & KALAMAZOO AVE SE

PRELIMINARY UTILITY PLAN

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISIONS			
REVISION DESCRIPTION	DATE		
△			
DATE: 05-19-2022			
SHEET NUMBER: 07			
07 OF 14			

CAD FILE: K:\22000815\DWG\PLAN SETS\SITE-PRELIMINARY\22000815-06-1



NOTES TO CONTRACTOR

- 1. ALL DISEASED, DAMAGED, OR DEAD PLANTING MATERIALS SHALL BE REMOVED IN ACCORDANCE WITH STANDARDS OF THE CITY OF KENTWOOD ZONING ORDINANCE.
- 2. ALL PLANTS SHALL CONFORM TO THE CURRENT ISSUE OF THE AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN AND SHALL HAVE PASSED INSPECTIONS REQUIRED UNDER STATE REGULATIONS.
- 3. MULCH MATERIAL: ALL MULCH TO BE 4"-6" ROUNDED RIVER BED GRAVEL WITH 8" MIN. DEPTH WITH WEED PREVENTION FABRIC. MULCH ALL PLANTING BEDS, 2'-0" STRIP ADJACENT TO LANDSCAPED AREA AROUND BUILDING AND TRASH ENCLOSURE. ALL EVERGREEN AND DECIDUOUS TREES TO RECEIVE 8" DEEP STONE MULCH WITH NO STONE IN DIRECT CONTACT WITH TREE TRUNK. EXTENT OF MULCH TO BE 4'-0" DIA. AT TREES.
- 4. SITE IRRIGATION TO BE PROVIDED FOR ALL PROPOSED LANDSCAPE IMPROVEMENT AREAS AND LAWN AREAS

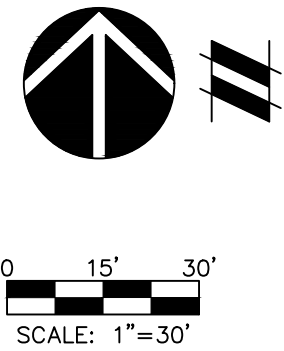
LEGEND

- PROPERTY LINE
- PROPOSED PROPERTY BOUNDARY
- EXISTING CURB & GUTTER
- PROPOSED CURB & GUTTER
- 4'-6" GRAVEL IN PLANTING BEDS MIN. DEPTH OF 8"
- PROPOSED TOPSOIL AND SOD LOCATIONS
- TRAFFIC FLOW ARROWS
- PROPOSED SIGN
- PARKING ROW COUNT
- ACCESSIBLE PARKING SPACE
- EXISTING / PROPOSED LIGHT POLE
- PROPOSED RAMP

SITE BENCHMARKS

BENCHMARK #1:
NORTHWEST FLANGE BOLT ON FIRE HYDRANT
ELEVATION: 742.34 (NAVDB8)

BENCHMARK #2:
"X" ON NORTHEAST BOLT ON CONCRETE
LIGHT POLE BASE, NO POLE ON BASE
ELEVATION: 740.27 (NAVDB8)



PLANT LIST

Quantity	Botanical	Common	Size	Container	Remarks	Percentage
8	Carpinus caroliniana	American Hornbeam	2.5" Cal.	B&B or Cont.	Full, well balanced canopy	3%
6	Picea omorika	Serbian Spruce	6" Ht.	B&B	Full to ground	3%
5	Picea pungens	Colorado Spruce	6" Ht.	B&B	Full to ground	2%
4	Pinus strobus	White Pine	6" Ht.	B&B or Cont.	Full to ground	2%
8	Platanus occidentalis	American Sycamore	2.5" Cal.	B&B	Straight trunk, full crown	3%
6	Quercus shumardii	Shumard Red Oak	2.5" Cal.	B&B or Cont.	Straight trunk, even crown	3%
9	Tilia cordata	Littleleaf Linden	2.5" Cal.	B&B	Well balanced head, Straight trunk	4%
Quantity	Botanical	Common	Size	Container	Spacing	Remarks
45	Cotoneaster horizontalis 'Tom Thumb'	Tom Thumb Cotoneaster	24" SPR.	Pot	36" o.c.	Full pot
19	Hydrangea paniculata 'Limelight' TM	Limelight Hydrangea	24" Ht.	Pot	60" o.c.	Full Pots Specimens
8	Ilex glabra 'compacta'	Compact Inkberry	18" Ht.	Pot	36" o.c.	Full
9	Miscanthus sinensis 'Morning Light'	Eulalia Grass	24" Ht.	Pot	48" o.c.	Full pot
22	Physocarpus opulifolius 'Donna May' TM	Little Devil Dwarf Ninebark	18" Ht.	Pot	48" o.c.	Full
47	Taxus x media 'Densiformis'	Dense Yew	18" Ht.	Pot or Cont.	48" o.c.	Full even form
3	Taxus x media 'Hicksii'	Hicks Anglo-Japanese Yew	24" Ht.	Pot or B&B	60" o.c.	Full and even
40	Viburnum dentatum 'Arrowwood'	Arrowwood Viburnum	24" Ht.	Pot	60" o.c.	Full even form
239						Total 100%
Quantity	Botanical	Common	Size	Container	Spacing	Remarks
43	Euonymus fortunei 'Coloratus'	Purple Wintercreeper	4"	Pot	24" o.c.	Full pot
24267	Poa pratensis	Kentucky Bluegrass	sod			

LANDSCAPE REQUIREMENTS SUMMARY

ITEM	REQUIREMENT	QTY. REQUIRED	QTY. PROVIDED	FORMULA
GREEN BELT	52ND ST. = 6 TREES (4 LINDEN, 2 OAK) AND 52 SHRUBS KALAMAZOO AVE. = 272 LF/40=6.8 TREES, 49 SHRUBS	6 TREES, 42 SHRUBS	52ND ST. = 210 LF/40=5.25 TREES, 42 SHRUBS KALAMAZOO AVE.=272 LF/40=6.8 TREES, 49 SHRUBS	=6 TREES, 6 X 7 = 42 SHRUBS KALAMAZOO AVE.=272 LF/40=6.8 TREES, 7 X 7 = 49 SHRUBS
BUFFER	30' WIDE - (B) 1 CANOPY TREE, 1 EVERGREEN TREE AND 5 SHRUBS PER 40 LF	WEST=8 CANOPY & EVERGREEN TREES AND 40 SHRUBS SOUTH=7 CANOPY & EVERGREEN TREES AND 35 SHRUBS	WEST=8 CANOPY & EVERGREEN TREES AND 40 SHRUBS SOUTH=7 CANOPY & EVERGREEN TREES AND 35 SHRUBS	WEST=8 CANOPY & EVERGREEN TREES AND 40 SHRUBS SOUTH=7 CANOPY & EVERGREEN TREES AND 35 SHRUBS
PARKING LOT LANDSCAPE	1 CANOPY TREE AND 100 SF OF LANDSCAPE AREA FOR EVERY 10 SPACES	3 TREES AND 300 SF OF LANDSCAPING	3 TREES (2 HORNBEAM, 1 OAK) AND 545 SF OF LANDSCAPING	21 SPACES =21/10=2.1=3 TREES 3 X 100 SF=300SF LANDSCAPING
PARKING PERIMETER LANDSCAPE	PARKING SHALL BE SCREENED BY LANDSCAPING AND BERING	SEE PLAN	SEE PLANS	N/A
POND PERIMETER	1 CANOPY OR EVERGREEN TREE AND 9 SHRUBS FOR EVERY 50 LF OF POND PERIMETER	N/A	N/A	N/A

888.850.4200 www.atwell-group.com
12745 23 MILE ROAD, SUITE 200
SHELBYVILLE, KY 40381
(502) 786-5800

STATE OF MICHIGAN
MICHAEL McPHERSON
ENGINEER
56643
LICENSED PROFESSIONAL ENGINEER

1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

PRELIMINARY LANDSCAPE PLAN

KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

REVISION DESCRIPTION	DATE

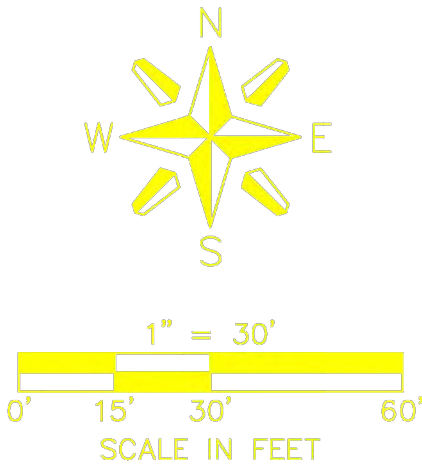
DATE: 05-19-2022

SHEET NUMBER: 08

08 OF 14

REVISIONS

CAD FILE: K:\2200815\DWG\PLAN SETS\SITE-PRELIMINARY\2200815-08-US



SCV



TLFL



SLM

PHOTOMETRIC EVALUATION
NOT FOR CONSTRUCTION

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

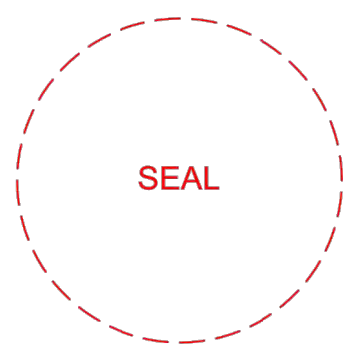
This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted. Fixture nomenclature noted does not include mounting hardware or poles. This drawing is for photometric evaluation purposes only and should not be used as a construction document or as a final document for ordering product.

Luminaire Schedule									
Symbol	Qty	Label	Arrangement	Description	LLD	LDD	LLF	Arr. Lum. Lumens	Arr. Watts
	12	A	SINGLE	SCV-LED-23L-SCFT-50-15.5' MT HGT	1.000	1.000	1.000	23101	188
	5	A1	SINGLE	SCV-LED-23L-SCFT-50-17.5' MT HGT	1.000	1.000	1.000	23101	188
	7	B	SINGLE	WST LED P2 40K V/W MVOLT - 11' MH - FIXTURE BY LITHONIA LIGHTING	1.000	1.000	1.000	3512	25
	1	C	SINGLE	TLFL-LED-20L- 4' MH	1.000	1.000	1.000	18768	187.91
	17	D	SINGLE	DNR52609 LED6-40K - 9' MH - FIXTURE BY ATLANTIC LIGHTING	1.000	1.000	1.000	1579	23.8
	6	G	SINGLE	SLM-LED-18L-SIL-FT-50-70CRI-SINGLE-16'POLE+2.5'BASE	1.000	1.000	1.000	18904	135
	2	H	D180	SLM-LED-18L-SIL-FT-50-70CRI-BLK-D180-16'POLE+2.5'BASE	1.000	1.000	1.000	37808	270

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ALL CALC POINTS	Illuminance	Fc	3.65	72.5	0.0	N.A.	N.A.
CANOPY	Illuminance	Fc	45.47	72.5	30.5	1.49	2.38
INSIDE CURB	Illuminance	Fc	7.82	42.9	1.2	6.52	35.75



10000 ALUMINE RD. • CINCINNATI, OHIO 45242 USA
PH (513) 793-1000 • FAX (513) 793-1002



6400 Westown Parkway
West Des Moines, Iowa
50266
P: 515-226-0128
F: 515-223-9873

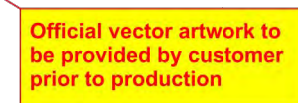
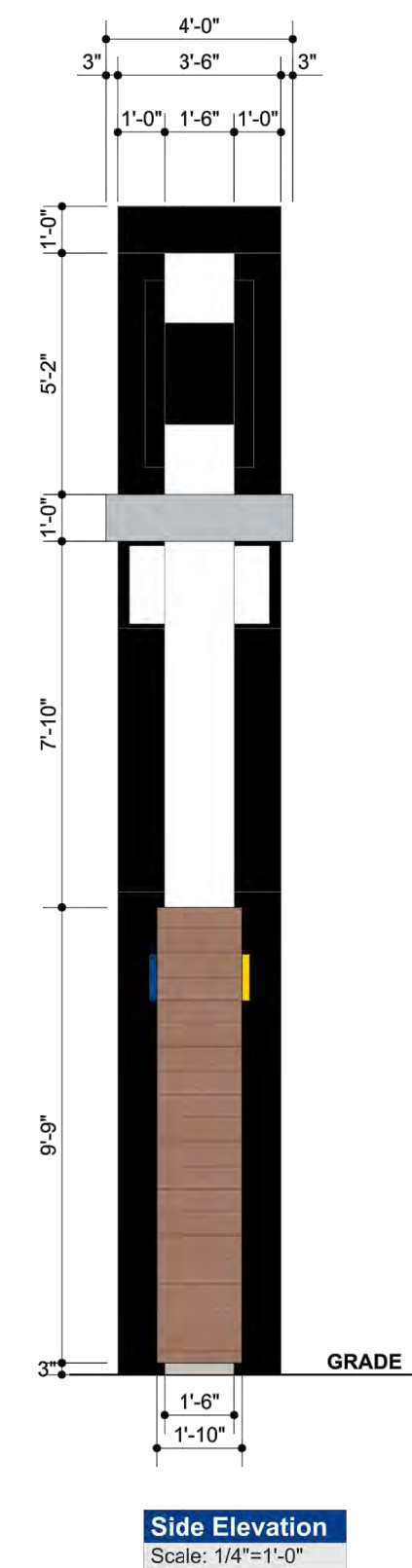
#2614 - KENTWOOD, MI
ADDRESS OR INTERSECTION
LO-155818 PHOTOMETRIC PLAN

KG PROJECT TEAM:
RDR:
SDM:
CPM:

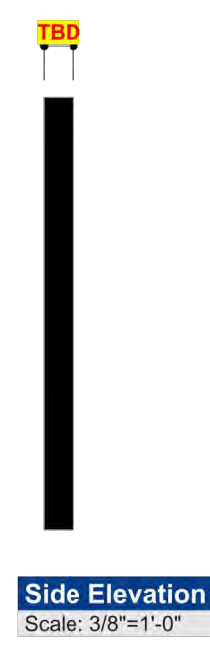
REVISION DESCRIPTION		DATE	
Δ			

DATE: 04-28-22

SHEET NUMBER:



18 Sq Ft



72 Sq Ft



Face:	3M Panagraphics III
Graphics:	1st surface applied Black and Red vinyl
Returns:	5" deep .080" Aluminum painted White
Backs:	.080" thk. Aluminum
Illum:	White LEDs as required by manufacturer, Remote Power Supplies
Install:	Sign to be installed using min. 3/8" all thread fasteners (or approved equivalent) and wood blocking as required
Quantity:	(1) ONE BUILDING SIGN REQUIRED

Alum. Ball, Gold Anodized, Dia. to Match Pole Butt Size

Stationary Truck, Heavy Duty, Cast Alum., with 1 1/4" Dia. Pulley on 1/4" Pin.

Halyard, #10 (5/16" Dia.), Braided Rope, Polypropylene

Two Snaphooks, swivel, bronze, chrome plated

Roller Sling with plastic coated Counterweight

Tapered Alum. Tube "G" Base Wall, Alloy 6063-T6 Satin Ground Finish

Flush Access Door with Cylinder Lock, Continuous Piano Hinge, and Manually Operated Cam Cleat

"E" Butt Diameter

Spun Alum. Collar (Caulk collar perimeter by installer)

Grade

Concrete (by Installer)

Cement or Waterproof compound (by Installer)

Dry Sand (by Installer)

Ground Sleeve Assembly 16 Ga. Corrugated Galv. Steel Tube

Lighting Spike (3/4" dia., steel rod)

"A" Exposed "C" Height Total (Nom.) Length

"D" Taper (Nom.)

"B" Setting Depth

6"

12"

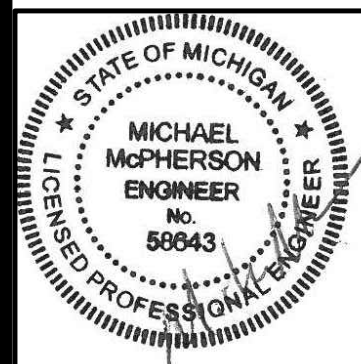
4' - 6"

ECX40IH INTERNAL WITH WINDOW

A	40 ft.
B	4 ft.
C	44 ft.
D	19'-3 in.
E	7 in.
F	3.5 in.
G	.156 in.

CUSTOMER NAME: Kum & Go DATE: _____
POLE SECTIONS: 2 QUANTITY 1 FINISH Satin Finish

ECX40IH INTERNAL WITH WINC	
A	40 ft.
B	4 ft.
C	44 ft.
D	19'-3 in.
E	7 in.
F	3.5 in.
G	.156 in.



1459 Grand Ave
Des Moines, IA 50309
P: 888-458-6646

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

SIGN DETAILS

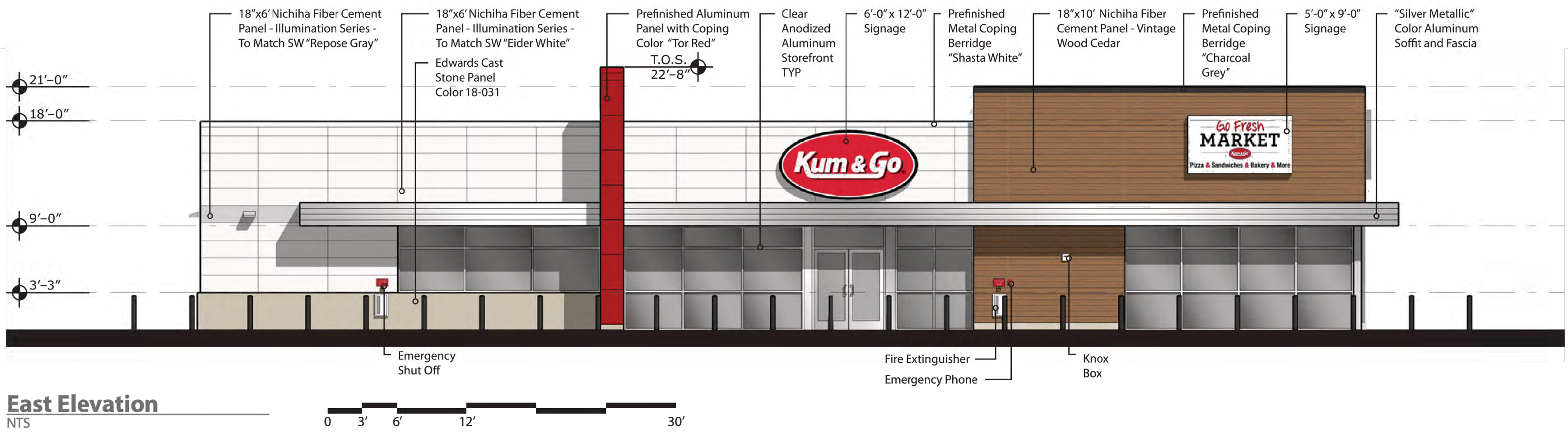
KG PROJECT TEAM:
RDM: DAVE LEMONS
SDM: RYAN HALDER
CPM: SCOTT NEWBURY

[illegible]

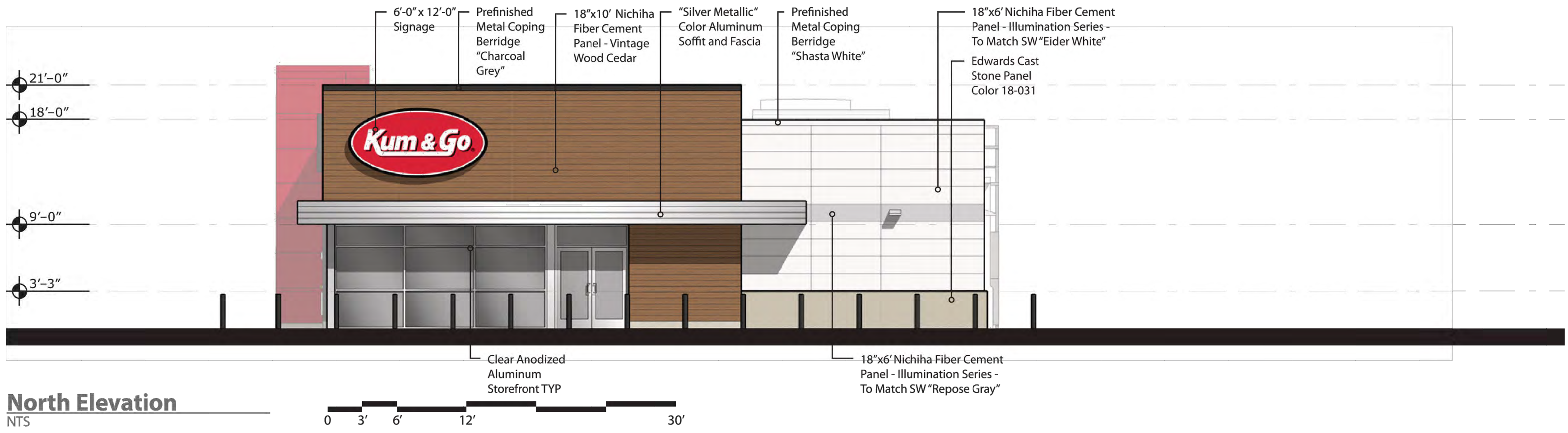
DATE: 05-19-2022

SHEET NUMBER:
11
11 OF 14

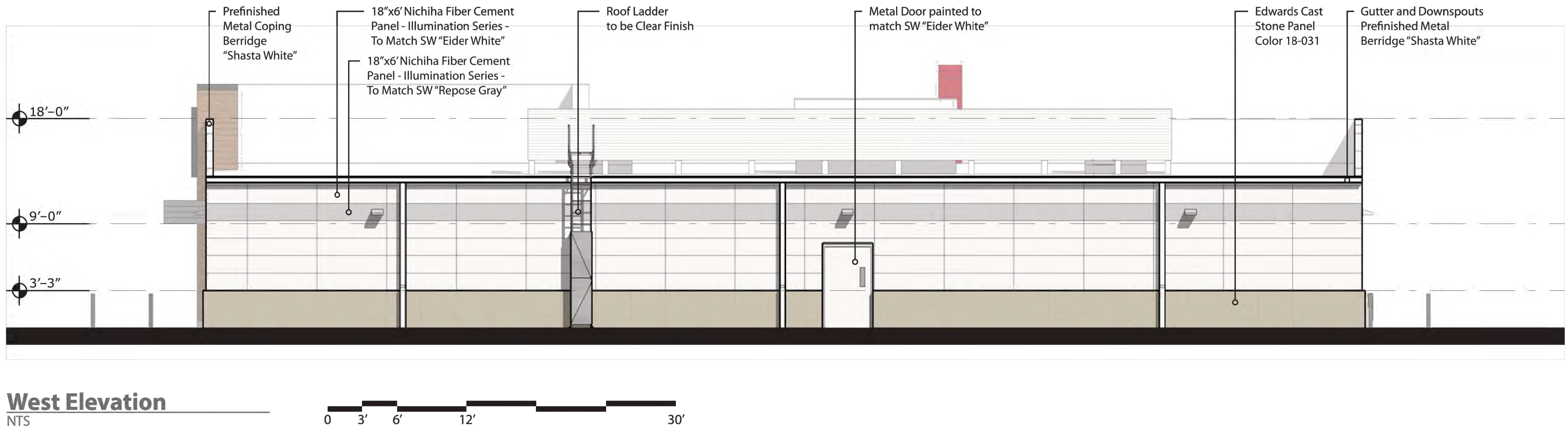
Proposed Building Signage			
Location	Sign	Size	Area
East Elevation	"Kum & Go" Sign	6' x 12'	72 SF
	"Go Fresh Market" Sign	5' x 9'	45 SF
North Elevation	"Kum & Go" Sign	6' x 12'	72 SF
West Elevation	No Signage	---	0 SF
South Elevation	No Signage	---	0 SF
Total			189 SF



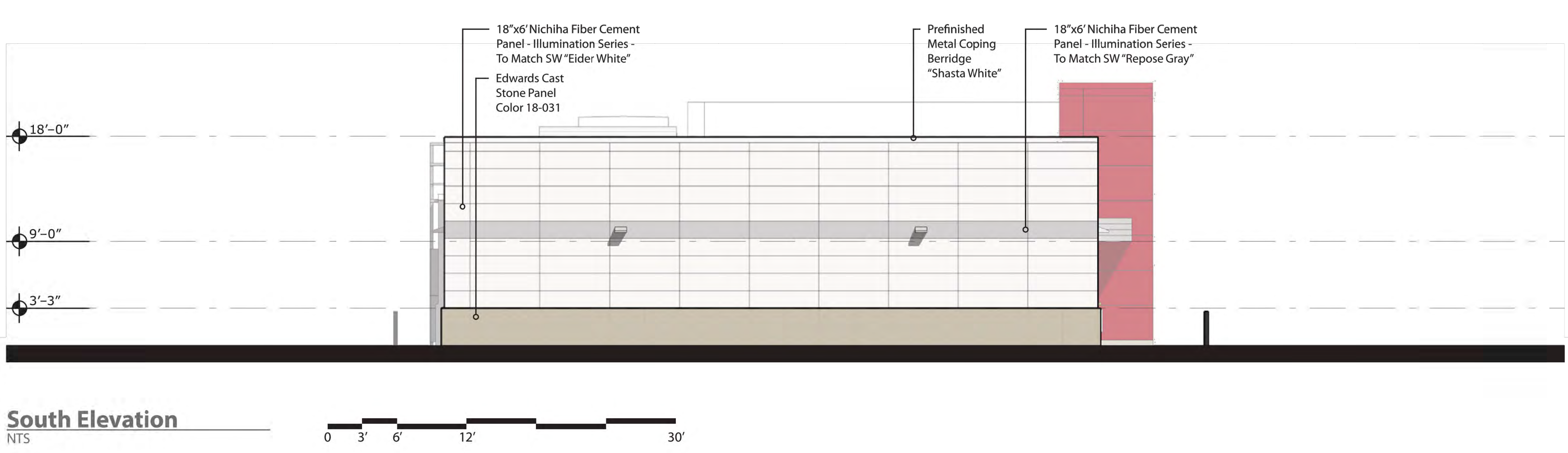
East Elevation
NTS



North Elevation
NTS



West Elevation
NTS



South Elevation
NTS

brr

ARCHITECT OF RECORD:
BRR ARCHITECTURE, INC
4131 METCALF AVENUE
SUITE 300
OVERLAND PARK, KS 66204

www.brrarch.com
TEL: 913-262-9095
FAX: 913-262-9044

SEAL

1459 Grand Avenue
Des Moines, Iowa
50309
P:515-457-6247

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

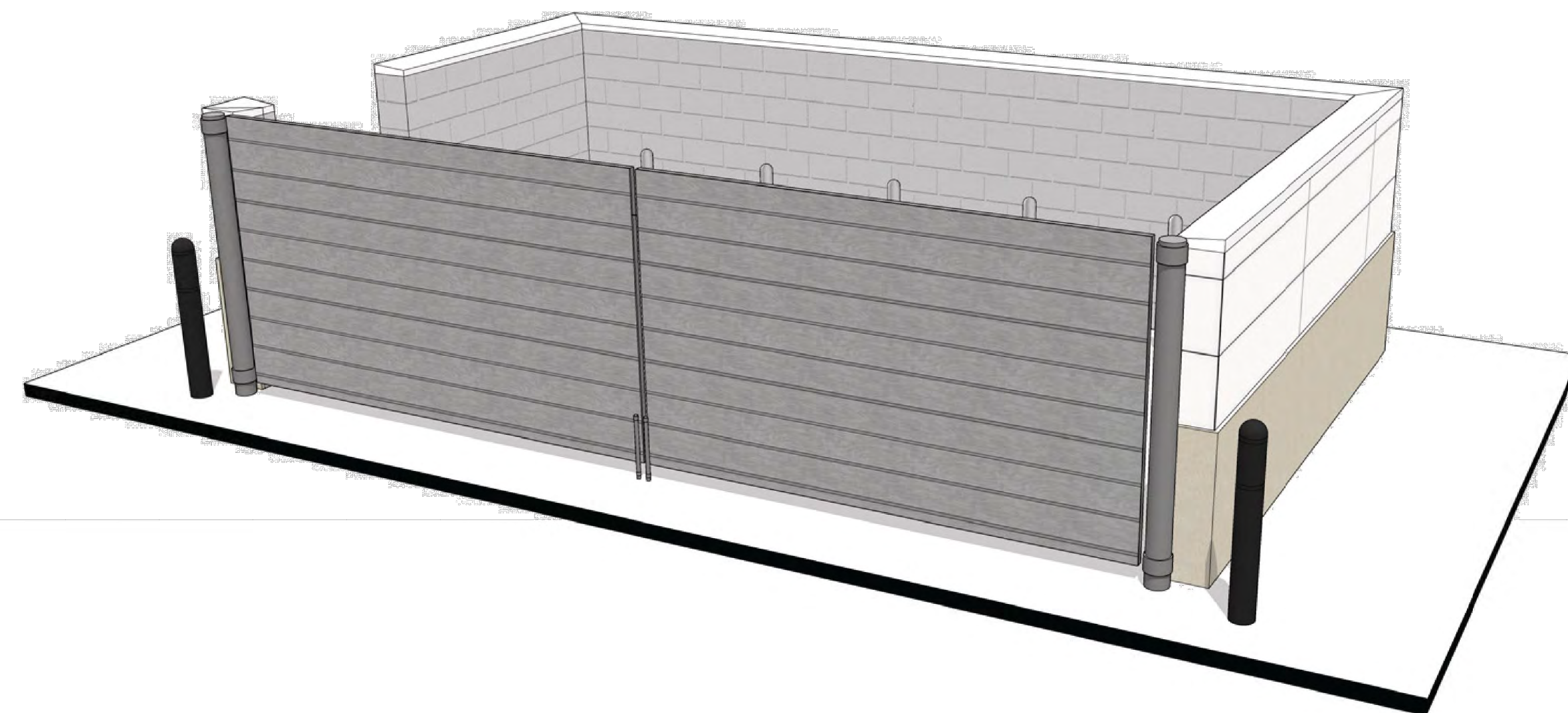
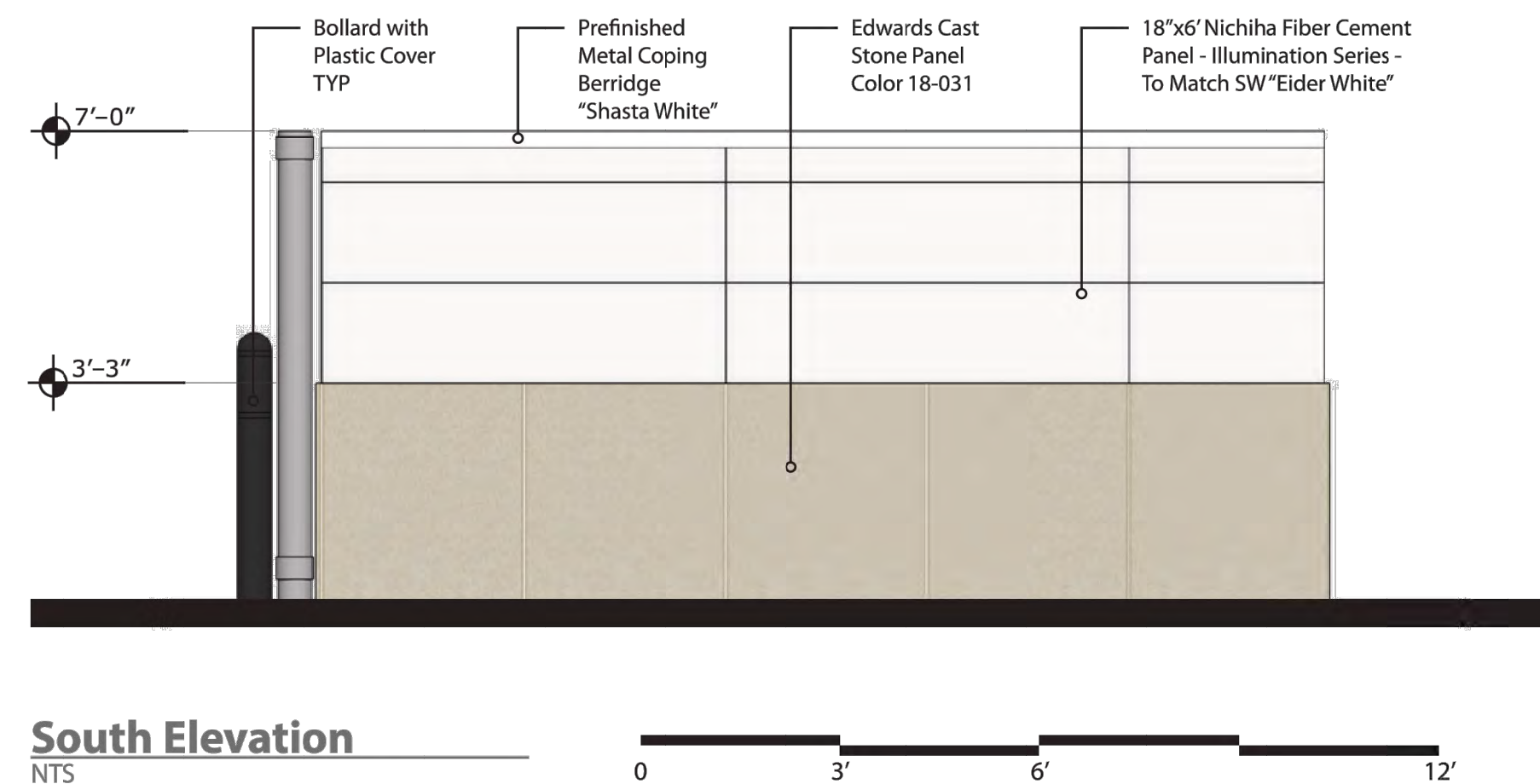
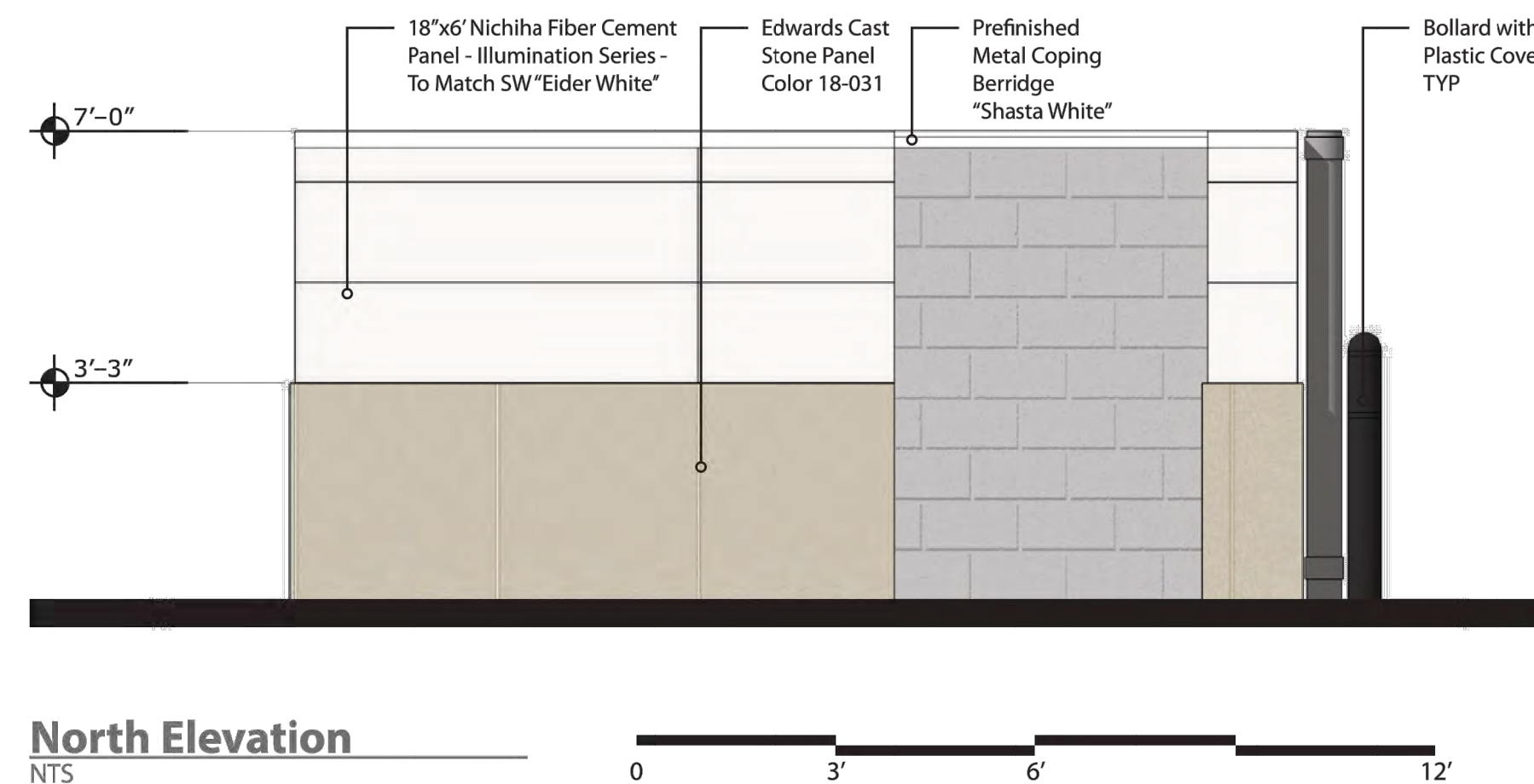
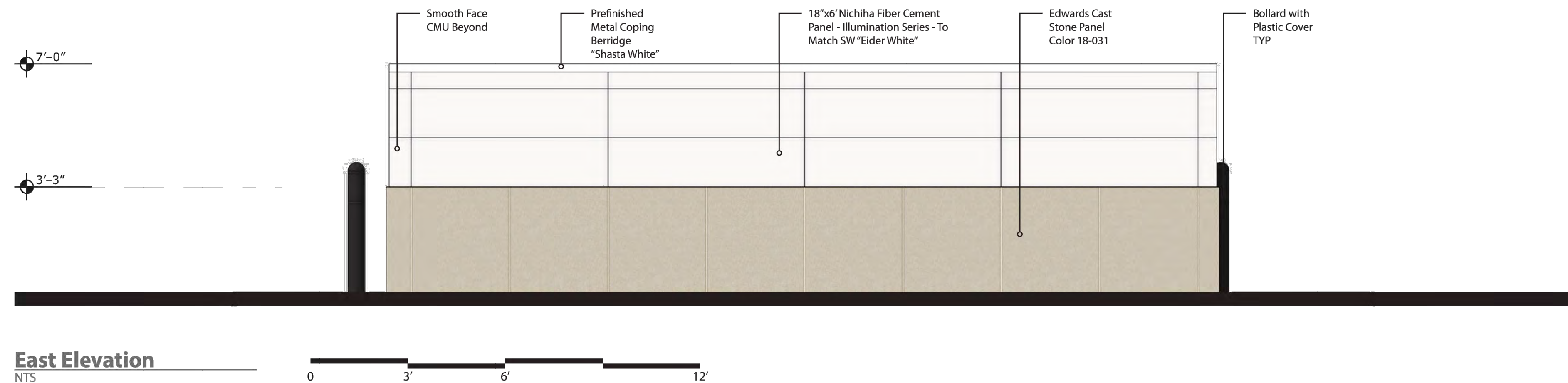
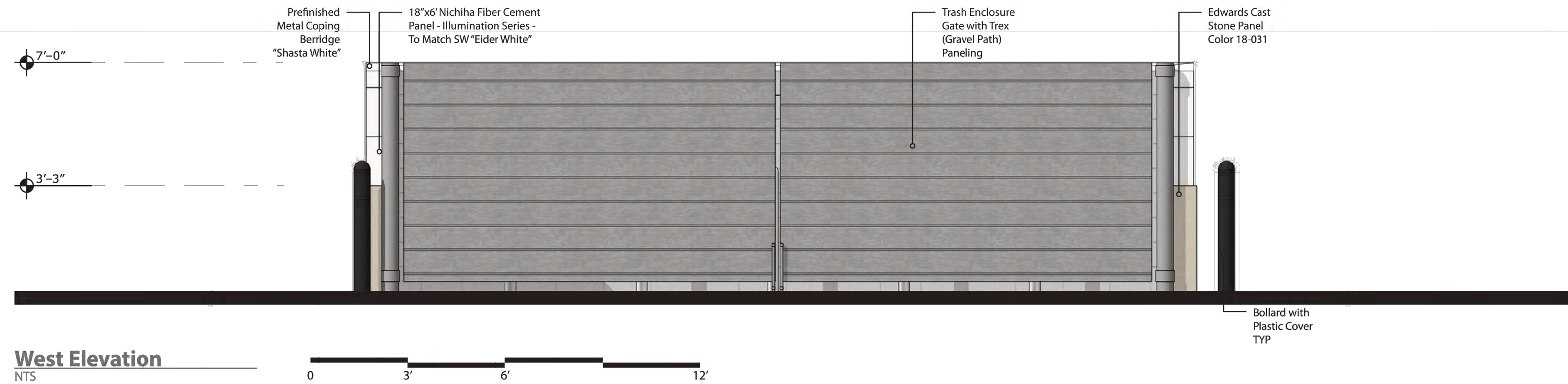
EXTERIOR ELEVATIONS

KG PROJECT TEAM:
RDM:
SDM:
CPM:

REVISION DESCRIPTION	DATE

DATE: 04/28/2022

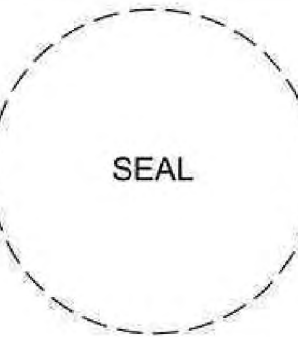
SHEET NUMBER:
12
12 OF 14



Perspective
NTS

brr

ARCHITECT OF RECORD:
BRR ARCHITECTURE, INC.
8131 MITCHELL AVENUE
SUITE 300
OVERLAND PARK, KS 66204
www.brrarch.com
TEL: 913-262-9095
FAX: 913-262-9044



1459 Grand Avenue
Des Moines, Iowa
50309
P:515-457-6247

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE
TRASH ENCLOSURE ELEVATIONS

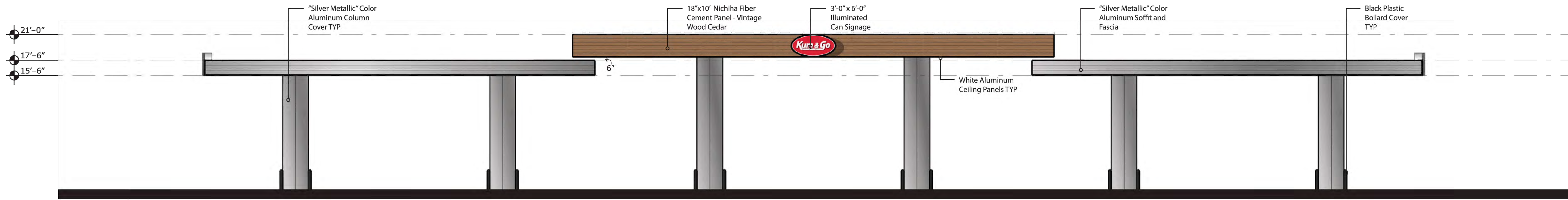
KG PROJECT TEAM:
RDM:
SDM:
CPM:

REVISION DESCRIPTION	DATE	△

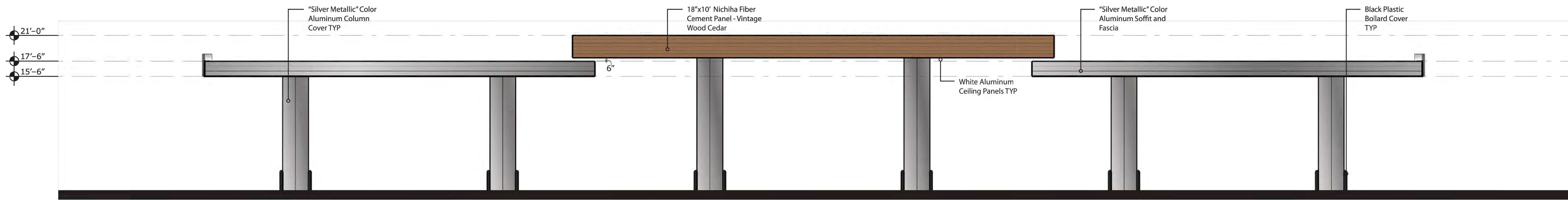
DATE: 04/28/2022

SHEET NUMBER:
13
13 OF 14

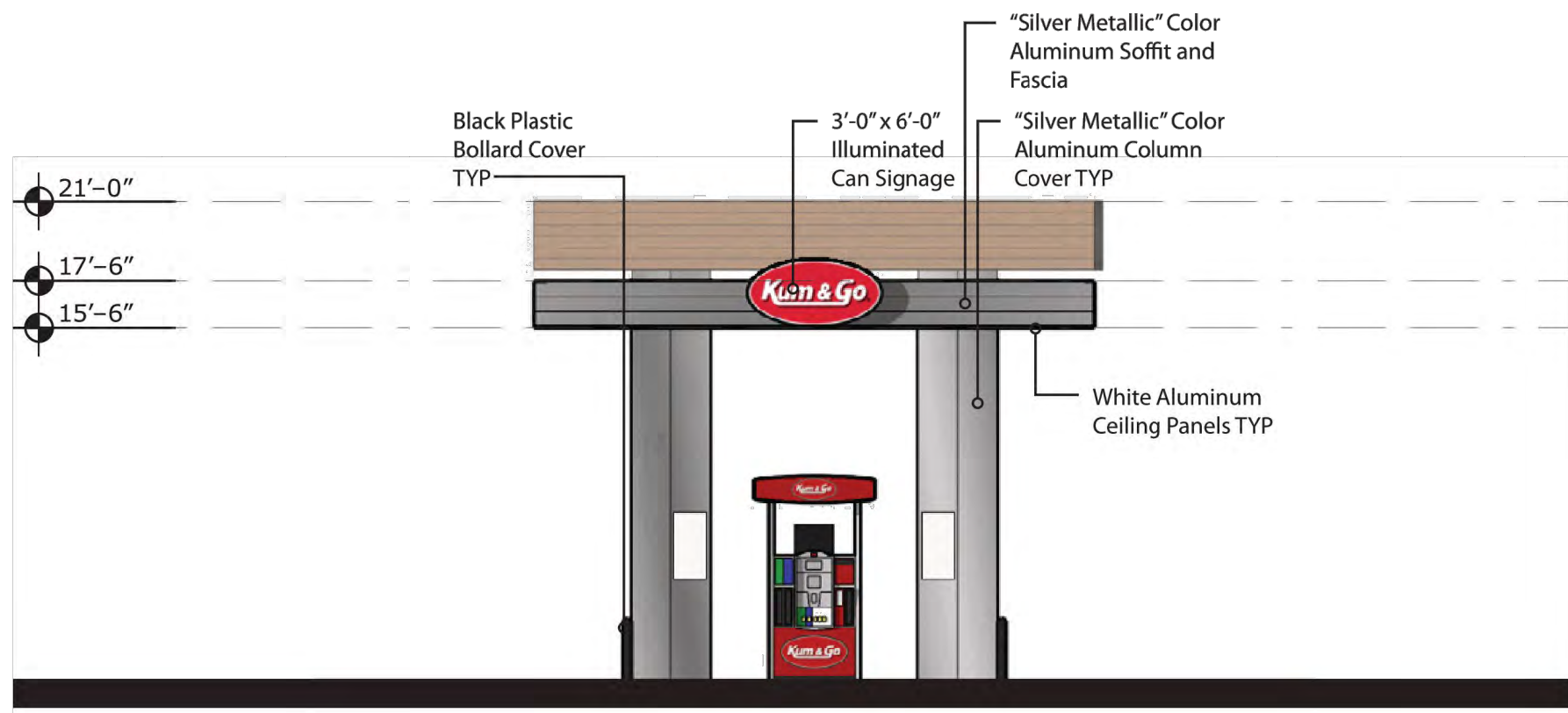
Proposed Canopy Signage			
Location	Sign	Size	Area
East Elevation	"Kum & Go" Sign	3'x 6'	18 SF
West Elevation	No Signage	---	0 SF
South Elevation	"Kum & Go" Sign	3'x 6'	18 SF
North Elevation	"Kum & Go" Sign	3'x 6'	18 SF
Total			54 SF



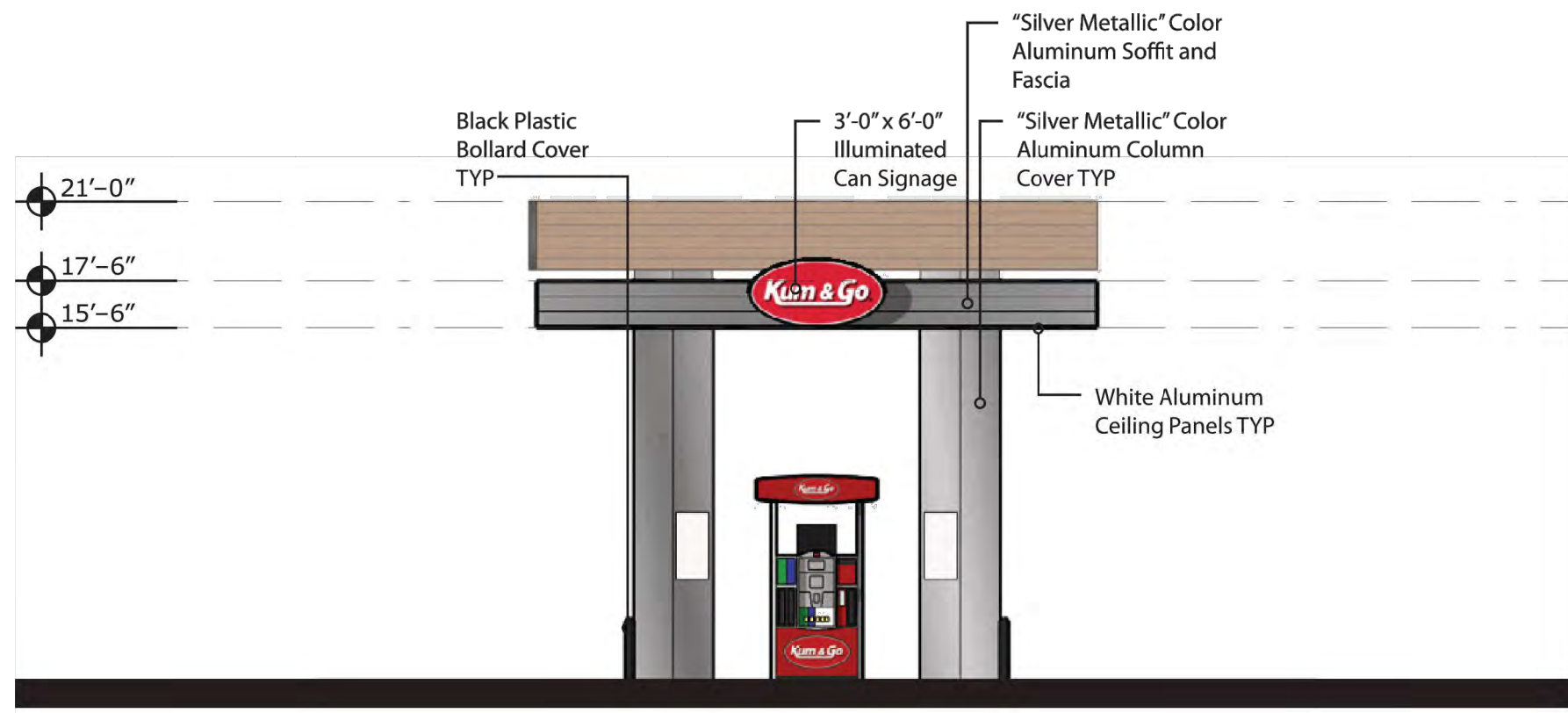
East Elevation
NTS



West Elevation
NTS



South Elevation
NTS



North Elevation
NTS

brr

ARCHITECT OF RECORD:
BRR ARCHITECTURE, INC
813 METCALF AVENUE
SUITE 300
OVERLAND PARK, KS 66204
www.brrarch.com
TEL: 913-262-9095
FAX: 913-262-9044

SEAL

1459 Grand Avenue
Des Moines, Iowa
50309
P:515-457-6247

2614 - KENTWOOD, MI
SWC OF 52ND ST SE & KALAMAZOO AVE SE

CANOPY ELEVATIONS

KG PROJECT TEAM:
RDM:
SDM:
CPM:

REVISION DESCRIPTION	DATE				

REVISIONS

DATE: 04/28/2022

SHEET NUMBER:
14
14 OF 14



PUD STATEMENT FOR THE SOUTHWEST CORNER OF 52nd STREET AND KALAMAZOO AVENUE

February 6, 2003, Revised February 24, 2003

The property proposed for rezoning to a Commercial Planned Unit Development (CPUD) is located at the southwest corner of Kalamazoo Avenue and 52nd Street. The property is 3 acres in size and is currently zoned C-2 Community Commercial, C-4 Office and R-2 Two Family Residential. The purpose of the proposed CPUD is to allow for the commercial use of the property while minimizing the impact on the adjacent residential neighborhood. The PUD statement for this property outlines the expectations the city has for the future development of the property while acknowledging the waivers from the PUD standards that are required in order to make the property usable.

Proposed Zoning:

Commercial Planned Unit Development CPUD

Compliance with the Master Plan

The Master Plan recommends commercial and office use for the site. The proposed CPUD zone permits uses in both the C-4 Office zone as well as the C-2 Community Commercial zone. The use of the Commercial Planned Unit Development will ensure that the impact of any commercial use is mitigated. The Master Plan also recommends neighborhood commercial uses that are walkable from the surrounding residential area.

Use Restrictions:

It is the intent of the City of Kentwood to exercise flexibility in the review and analysis of any proposed use of the property. The uses proposed for the CPUD should be permitted or Special Land Uses that are mid to low traffic generators that do not create high traffic volumes during peak hours for the road system (7:00-8:30A.M. and 4:00-6:00P.M.) The City will require a traffic analysis to determine the impact of the use on the surrounding road system, to evaluate traffic flow, and to determine the changes to the intersection that may be required to accommodate the use. The city will not preclude the use of the existing building.

Site Requirements:

All three parcels must be combined into one parcel under a single ownership. The westerly portion of the southernmost parcel shall be split and combined with the small residential lot on Newcastle Avenue to create a usable residential lot on that street.

Driveways:

All existing driveways on Kalamazoo and 52nd Street will be removed. One driveway will be permitted on Kalamazoo and one driveway will be permitted on 52nd Street. The driveway on 52nd Street shall be located on the westernmost side of the development, and is anticipated to allow both left and right turns in and out of the driveway. The driveway on Kalamazoo Avenue shall either align with the driveway for the credit union (or another future use), or shall be located further to the south than the credit union driveway. A traffic impact analysis will be required in accordance with Section 13.02 of the Kentwood Zoning Ordinance. This analysis will determine whether the Kalamazoo driveway will be a full service driveway, or whether access will be restricted. This traffic analysis shall also take into account the impact of the Southbelt on Kalamazoo Avenue, shall determine the need for additional intersection improvements, evaluate traffic flow and final driveway location/alignment on both Kalamazoo Avenue and 52nd Street.

Architectural features:

Building roof elevations shall be in a low profile residential style with peaked roof or another roof style approved by the Planning and City Commissions. The buildings shall not overpower the residential uses to the south and west. The building shall be primarily of brick on all sides, with architectural features that create interest and break up large expanses of wall area.

Site Design:

The building(s) shall be designed to avoid the appearance of overcrowding on the site and the adjoining homes. Although the CPUD regulations require 30% green space provided on the site, less green space will be considered if other site amenities area provided. The main building shall be limited to approximately 11,000 square feet in area, with an additional 6,000 square foot office building permitted along the south side of the site. The Police and Fire Departments must review and approve the building location for the safety and security of the building occupants and area residents.

The building must be residential is scale and design. Parking shall be held to a minimum for the planned or potential uses; deferred parking shall be encouraged as long as it can be shown that the deferred parking can be provided at a later date on site needed without impacting the total required green space. Servicing/loading within the buildings shall occur between the hours of 9:00A.M. to 3:00P.M. Circulation around the site shall ensure separation of pedestrian and service vehicles, and shall provide easy access to sidewalks on Kalamazoo Avenue and 52nd Street. The site will allow for bicycle access and parking. Service traffic shall be kept as far away from residential uses as possible.

Sidewalks shall be constructed on the subject property outside the public right-of-way and public easements will be provided to allow public access to the sidewalks.

Section 12.05 C 4 g requires that provision be made to accommodate improvements to public transit service. Section 12.05 C 5 f requires an applicant to make provision for a pedestrian circulation system.

Sections of the PUD Ordinance that will require a waiver:

The CPUD regulations allow for deviations from certain requirements of the zoning ordinance. The following ordinance sections may require a waiver:

- Section 12.02 C 3: Waiver from the minimum lot size of 5 acres
- Section 12.05 C 5 a: Waiver from the requirement of an average greenbelt of 30' and no less than 20' along public streets
- Section 12.05 C 5 h: Waiver from the requirement of 30% open space exclusive of areas of deferred parking
- Section 12.05 C 5 d: Waiver from the requirement of extensive gathering and seating plazas, greenways and tree-lined drives within parking lots.

Hours of Operation:

The hours of operation for the businesses within the PUD shall be limited times that are compatible with residential uses, from approximately 7:00 A.M. to 10:00 P.M.

Lighting Standards:

The site lighting shall be in compliance with Chapter 20 of the Kentwood Zoning Ordinance. After the businesses are closed daily, the lighting for the site shall be reduced to serve for security purposes only.

Landscaping/Parking:

Landscaping shall be designed to provide a pleasing, permanent look for this corner. Parking shall be limited in the front setback area along the 52nd and Kalamazoo rights of way. Parking shall be screened by landscaping and berming. The landscaping shall be low enough to provide visibility from the street for both customers and employees. Landscaping shall meet or exceed Zoning Ordinance standards along the western and southern property lines. Landscaping shall be maintained at all times, including replacement of dead and damaged plants and regular trimming to maintain proper plant heights.

Adjacent uses:

The site shall be designed with the clear intention that commercial uses will not be extended further south and west of the parcel.

April 1, 2003

CITY OF KENTWOOD
ORDINANCE NO. 8-03

AN ORDINANCE TO AMEND THE CITY OF KENTWOOD ZONING ORDINANCE BEING ORDINANCE NO. 9-02, AS AMENDED, TO REZONE APPROXIMATELY THREE ACRES FROM C-2 COMMUNITY COMMERCIAL, C-4 OFFICE AND R-2 TWO FAMILY RESIDENTIAL TO CPUD COMMERCIAL PLANNED UNIT DEVELOPMENT (SOUTHWEST CORNER 52ND STREET & KALAMAZOO AVENUE)

THE CITY OF KENTWOOD ORDAINS:

SECTION 1.

The Zoning Ordinance, Ordinance No. 9-02, is hereby amended to change the following described area of land from R1-C Single Family Residential to OS Open Space viz:

Part of the NE ¼ of Section 32, T6N, R11W, City of Kentwood, Kent County, Michigan, being further described as beginning at the NE corner of said Section; thence South 01°33'46" West 379.06 feet along the East Section line; thence North 86°30'28" West 351.26 feet; thence North 01°33'46" East 100.06 feet; thence South 86°30'28" East 9.25 feet; thence North 01°33'46" East 279.00 feet, to the North Section line; thence South 86°30'28" East 342.00 feet, along said line, to the Point of Beginning, containing 3.00 acres, more or less.

Section 2.

This Ordinance shall be effective ten (10) days following its publication.

The foregoing Ordinance was offered by Commissioner McGookey, supported by Commissioner Penning, the vote being as follows:

YEAS: Commissioners: Brinks, Cummings, McGookey, Penning, Vander Laan and Mayor Root.

NAYS: Commissioner Clanton.

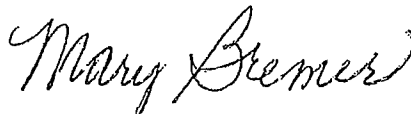
ABSTAIN: None.

ABSENT: None.



Mary Bremer
Deputy City Clerk

I hereby certify the foregoing to be a true copy of an ordinance adopted at a regular meeting of the Kentwood City Commission held March 18, 2003.



Mary Bremer
Deputy City Clerk

**KENTWOOD CITY COMMISSION
APPROVED
FINDINGS OF FACT
APRIL 15, 2003**

PROJECT: 52nd Street and Kalamazoo PUD

APPLICATION: 2-03

REQUEST: Rezoning of 3 acres of land from C-2 Community Commercial, C-4 Office, and R-2 Two Family Residential to CPUD Commercial Planned Unit Development

LOCATION: Southwest corner of 52nd Street and Kalamazoo Avenue

HEARING DATE: April 1, 2003

MOTION:

Motion by McGookey, supported by Penning, to approve Ordinance 8-03 to rezone three acres of land from C-2 Community Commercial, C-4 Office and R-2 Two Family Residential to CPUD Commercial Planned Unit Development at the southwest corner of 52nd Street and Kalamazoo Avenue with condition 1 and basis points 1 through 6 as stated in the Planning Commission Findings of Fact dated March 11, 2003.

Roll Call Vote: Yeas: Brinks, Cummings, McGookey, Penning, Vander Laan and Mayor Root.
Nays: Clanton. Absent: None

Ordinance Adopted.

CONDITIONS:

1. The development of the 52nd Street and Kalamzoo Planned Unit Development shall be consistent with the PUD Narrative dated February 6, 2003, and revised February 24, 2003.

BASIS:

1. The property proposed for the rezoning is comprised of three parcels of land under three different zoning districts. Given the high traffic volumes of the abutting arterial streets, it would be difficult to develop these parcels separately. The use of the Planned Unit Development allows the development of the parcels with greater flexibility than traditional zoning.
2. The zoning is consistent with the Master Plan, which recommends commercial and office use for the site. The proposed CPUD permits uses in both the C-4 and the C-2 zones.

3. There are residential uses to the west and south. Use of the CPUD zone will ensure that these residential uses are protected from the impact of the future commercial use.
4. The CPUD statement is written to allow maximum flexibility for the future redevelopment of the property. The PUD statement will also require that the PUD be tied to specific development requirements that will create a safe and attractive development that will not have a negative impact on the area.
5. The proposed rezoning meets the requirements of Section 13.03 C of the Kentwood Zoning Ordinance.
6. Discussion during the work session and public hearings.

**KENTWOOD CITY COMMISSION
APPROVED
FINDINGS OF FACT
APRIL 15, 2003**

PROJECT: 52nd Street and Kalamazoo Commercial Planned Unit Development

APPLICATION: 2-03

REQUEST: Preliminary Site Plan for a Commercial Planned Unit Development

LOCATION: Southwest corner of 52nd Street and Kalamazoo Avenue

HEARING DATE: April 1, 2003

MOTION:

Motion by McGookey, supported by Cummings, to approve the Preliminary Site Plan for a Commercial Planned Unit Development at the southwest corner of 52nd Street and Kalamazoo Avenue with conditions 1 and 2 and basis points 1 through 4 as stated in the Planning Commission Findings of Fact dated March 11, 2003.

- Motion Carried.

CONDITIONS:

1. The development of the Commercial Planned Unit Development shall be consistent with the PUD Narrative dated February 6, 2003, and revised February 24, 2003.
2. Applicant shall initiate a land division to formally separate the R1-C portion of the applicant's property from the proposed CPUD to create a legal lot on Newcastle Avenue.

BASIS:

1. The CPUD statement is written to allow maximum flexibility for the future redevelopment of the property. The PUD statement will also require that the PUD be tied to specific development requirements that will create a safe and attractive development that will not have a negative impact on the area.
2. The PUD statement indicates the general site requirements and design, architectural features and anticipated waivers from the PUD regulations of the Kentwood Zoning Ordinance.

3. A land division and combination will be required to formally divide the R-2 parcel and combine it with the adjacent parcel on Newcastle Avenue. If the land division does not take place the parcel may not have access to a public street.
4. Discussion during work session and public hearings.

April 1, 2003

CITY OF KENTWOOD
ORDINANCE NO. 8-03

AN ORDINANCE TO AMEND THE CITY OF KENTWOOD ZONING ORDINANCE BEING ORDINANCE NO. 9-02, AS AMENDED, TO REZONE APPROXIMATELY THREE ACRES FROM C-2 COMMUNITY COMMERCIAL, C-4 OFFICE AND R-2 TWO FAMILY RESIDENTIAL TO CPUD COMMERCIAL PLANNED UNIT DEVELOPMENT (SOUTHWEST CORNER 52ND STREET & KALAMAZOO AVENUE)

THE CITY OF KENTWOOD ORDAINS:

SECTION 1.

The Zoning Ordinance, Ordinance No. 9-02, is hereby amended to change the following described area of land from R1-C Single Family Residential to OS Open Space viz: .

Part of the NE ¼ of Section 32, T6N, R11W, City of Kentwood, Kent County, Michigan, being further described as beginning at the NE corner of said Section; thence South 01°33'46" West 379.06 feet along the East Section line; thence North 86°30'28" West 351.26 feet; thence North 01°33'46" East 100.06 feet; thence South 86°30'28" East 9.25 feet; thence North 01°33'46" East 279.00 feet, to the North Section line; thence South 86°30'28" East 342.00 feet, along said line, to the Point of Beginning, containing 3.00 acres, more or less.

Section 2.

This Ordinance shall be effective ten (10) days following its publication.

The foregoing Ordinance was offered by Commissioner McGookey, supported by Commissioner Penning, the vote being as follows:

YEAS: Commissioners: Brinks, Cummings, McGookey, Penning, Vander Laan and Mayor Root.

NAYS: Commissioner Clanton.

ABSTAIN: None.

ABSENT: None.



Mary Bremer
Deputy City Clerk

I hereby certify the foregoing to be a true copy of an ordinance adopted at a regular meeting of the Kentwood City Commission held March 18, 2003.



Mary Bremer
Deputy City Clerk

PLANNING STAFF RECOMMENDATION

Golder 10-19-04

PROJECT Thomson's Automotive Repair

APPLICATION: 35-04

REQUEST: Special Land Use Approval for an Automotive Repair Establishment

HEARING DATE: October 26, 2004

RECOMMENDATION: Recommend conditional approval of the Special Land Use automotive repair as described in Case No. 35-04.
Approval is conditioned on the following:

CONDITIONS:

1. Operation shall be consistent with that as described in the operations description dated September 9, 2004, with the removal of reference to the rock garden and the split rail fence around the perimeter of the site.
2. City and property owner shall enter into a development agreement incorporating representations made regarding the operation and the nature of the proposed lease with the applicant.
3. No outside storage of parts of any kind on the site.
4. All vehicle work shall be done within the existing building.
5. All vehicles shall be parked in designated parking spaces as indicated on the site plan. All vehicles on the site must be licensed and operable.
6. Applicant shall complete the restoration work on the building as described in the Special Land Use application prior to starting the automotive repair operation on the site.

BASIS:

1. The operation description for the project refers to hours of operation, types of repairs to be completed, and improvements to be made to the building and the site. These standards are necessary to ensure that the property does not become a nuisance to the neighborhood or create public safety issues.
2. The proposed use is not consistent with the CPUD statement for this site or the recommendations of the Planning and City Commissions with respect to the site. It is an interim use of the property, and as such is not intended to be located at this site long term. The proposed lease allows the owners of the property to terminate the lease with a 90 day notice to accommodate a use that is more consistent with the standards set forth in the PUD statement. The city has not received an executed copy of the lease. The representations made regarding this lease must be recorded to ensure that it applies to current and future property owners.
3. The site for the proposed automotive repair establishment is small and it is located on a visible corner of two arterial streets. Automobile parts or inoperable vehicles on the site will defeat the purpose of keeping the site clean and attractive.
4. The applicant has committed to the improving the appearance of the building. The improvement of the building is key to permitting a use that is otherwise inconsistent with the PUD statement adopted for the site.
5. Discussion at work session and public hearing.

PLANNING STAFF RECOMMENDATION

Golder 10-18-04

PROJECT Thompson's Automotive
APPLICATION: 35-04
REQUEST: Site Plan Review an Automotive Repair establishment
HEARING DATE: October 26, 2004
RECOMMENDATION: Recommend conditional approval of the site plan dated October 19, 2004 for Thomson Automotive Repair as described in Case No. 35-04. Approval is based on the following:

CONDITIONS:

1. Staff approval of the landscaping plan for the site.
2. Compliance with the City Engineer's memo dated October 14, 2004.
3. Compliance with the Fire Department review memo dated October 5, 2004.
4. The applicant shall work with staff on the additional lighting proposed for the site.
5. Approval by the Planning Commission of the Special Land Use Automotive Repair.

BASIS:

1. The applicant will need to provide additional detail on the landscaping proposed for the site. The landscaping plan shall include, but not be limited to, information on the types and quantity of landscape material in the planting strip between the sidewalk and street pavement, the proposed green area at the corner of 52nd and Kalamazoo, moveable planters, and the dumpster enclosure.

2. The review of the lighting plan will ensure that the lighting proposed is consistent with the Zoning Ordinance.
3. While the site plan does not meet all the requirements of the Zoning Ordinance, it is located within a Commercial Planned Unit Development which gives the city flexibility in applying the ordinance standards. The plan will bring the building and site closer to conformance with city ordinances.
4. Discussion at work session and public hearing.



Figure 10: Division Avenue Sub Area Schematic Plan

SUB AREA 2: KALAMAZOO AVENUE CORRIDOR

Introduction:

Kalamazoo Avenue is a vital transportation corridor in Kentwood, linking the city to Grand Rapids and the South Beltline (M-6), and connecting residences, employment centers, and commerce throughout the corridor. With the opening of the South Beltline in 2004, traffic along this stretch of road increased significantly. As traffic increased, the city became concerned about the impact of the traffic on existing residential uses, especially as the Kent County Road Commission considered expanding the roadway to five lanes. The city negotiated with the Kent County Road Commission to assume control of the roadway and has redeveloped the road into a 4-lane boulevard cross section. The creation of the boulevard along this primarily residential area has helped to retain the value of the street for residential use.

The recommendations for this corridor will relate primarily to the intersections at 52nd and 60th Streets. In 2005 the land use at 52nd and Kalamazoo included a vacant gas station on the southwest corner and a credit union on the southeast corner. The credit union has since been closed and the building demolished. The vacant gas station is now an automotive repair establishment. In conjunction with Lighthouse Communities (LINC), the city completed a charrette process in late 2010 to obtain neighborhood input into the future redevelopment of the

intersection. The charrette process resulted in the development of an illustrative plan for the 52nd and Kalamazoo intersection that give developers and property owners an idea of how the redevelopment of the intersection could look.

At 60th Street and Kalamazoo Avenue, vacant land still exists on the northeast corner, and an existing retail center stands partially vacant on the northwest side of the intersection. Meanwhile, commercial development has occurred south of 60th Street in Gaines Township. It is apparent that the northwest corner of the Kalamazoo and 60th intersection may be difficult to develop due to poor visibility and access into the site.

The northeast corner of 60th and Kalamazoo may also be difficult to develop, given the need for land assembly (seven parcels owned by 5 different entities) and cooperation between the property owners in order to provide adequate space for development.

Application of Planning Principles

Principle 1. Open Space and Green Infrastructure:

52nd Street and Kalamazoo Avenue:

The intersection of Kalamazoo and 52nd Street was discussed at the 2010 charrette. The charrette designs for the intersection included corners of green space/plaza at the intersection itself. Walkability was also considered an important element for the area. Since the southwest quadrant of the intersection is zoned Commercial Planned Unit Development (CPUD), a certain amount of open space will be required, which also supports the Open Space concepts portrayed in the charrette sketch plans.

On the southeast corner of 52nd and Kalamazoo, the city's acquisition of property for road improvements has resulted in agreements to reduce the required setbacks from the 52nd Street and Kalamazoo Avenue rights of way. However, an open plaza or green space will still be required at the corner to allow for clear vision at the intersection. The recommendation for this corner allows both neighborhood

excerpt from
Master Plan re:
Kalamazoo/52nd St

commercial and office uses—although these uses will be limited to those that would have low impact on the neighborhood and generate only a limited amount of traffic. Heavy screening will be required for the area of the site adjacent to the Princeton Estates housing development.

60th Street and Kalamazoo Avenue:

There is a potential for a green gateway at the northeast corner of the intersection of Kalamazoo and 60th Street. As a PUD development a landscape greenbelt area is required, as well as sensitivity to the residential uses to the north and east.

Open space could also be incorporated into the plan for the development of the northwest corner of the intersection. Again, sensitivity is required between commercial uses and to the apartments located to the north.

Principle 2: Mobility

A. Arterial Streets

Kalamazoo Avenue is an arterial street. The city made the decision to reconstruct Kalamazoo Avenue as a four-lane boulevard, making the street safer and more livable while accommodating the volume of traffic in the corridor. In order to minimize the impact on adjacent property owners, the city did not acquire much additional right-of-way along the Kalamazoo corridor. Therefore, there is not room within the right-of-way for all of the elements of Complete Streets. Bike paths are only available from the 5000 block north to Pickett Street. However, sidewalks are available along both sides of the remaining Kalamazoo Avenue frontage. The city should strive to make crossing the major arterial streets easier through the incorporation of better crosswalk demarcation at the four constructed mid-block refuge island locations as well as at major intersections along Kalamazoo Avenue.

When Kalamazoo Avenue was developed into a boulevard street, the construction did not include street trees because the width of the parkway (between the street pavement and the sidewalk) is insufficient to plant trees. However, trees have since been planted within the boulevard medians along Kalamazoo Avenue from 44th Street through 60th Street.

B. Primary Intersections

As previously noted, the Kalamazoo Avenue corridor is very distinctive given: its interchange with the M-6 limited access freeway less than one mile south of 60th Street; its classification as a commercial truck route; the significant presence of residential neighborhoods along and within the square miles off the corridor; and little or no allowance for direct left turn movements within the major arterial intersections at 60th Street, 52nd Street and 44th Street. Subsequently the boulevard cross section of Kalamazoo Avenue is designed to preserve and protect the adjoining residential development as well as safely optimize the movement of traffic along the major arterial street while affording reasonable access to frontage properties. These circumstances place a premium on cross/shared access between frontage parcels as well as locating driveways away from the major intersections and median crossovers. As properties at the primary intersections redevelop, a traffic analysis will be required to assure the proposed point of access into the property complements the boulevard and safely accommodates the nature and volume of traffic associated with the new use.

Minimizing the number of driveways, locating driveways further from the 52nd and 60th/Kalamazoo intersections and cross access between all parcels will be critical. At the northeast corner of 60th and Kalamazoo, access will be limited to one driveway on the Kalamazoo street frontage. Similar limitations will be applied to the 52nd and Kalamazoo intersection as it redevelops, consistent with the adopted PUD concept plan on the southwest quadrant.

C. Non-Motorized Facilities

Sidewalks are available along both sides of Kalamazoo Avenue. In order to minimize the impact on adjacent property owners, the city did not acquire much additional right-of-way along the Kalamazoo corridor. Bike paths are only available from the 5000 block north to Pickett Street. The city should strive to make crossing the major arterial streets easier through the incorporation of better crosswalk demarcation at the constructed mid-block locations as well as at major

intersections along Kalamazoo Avenue. In addition, connections to adjacent non-motorized paths should be made, along with wayfinding signs to direct people to attractions, stores, and parks nearby. For example, the non-motorized plan identifies existing and future connections to the Paul Henry Trail, parks, and a historic home. Wayfinding signs could direct people to these resources.

D. Transit Services

Transit is currently offered within the Kalamazoo Avenue corridor from 52nd Street to 60th Street and from 44th Street south to the Kent County Health Department facility at 4700 Kalamazoo Avenue, SE. Although the extent of the service is limited, there exists opportunity to walk and bike along Kalamazoo Avenue to the transit stops between 44th Street and 60th Street on Kalamazoo Avenue.

E. Air, Rail, and Truck Transport

Kalamazoo Avenue is a truck route that will continue to connect M-6 and other freeways to the community. Traffic counts from 2018 suggest that there are 24,000 vehicle trips daily on Kalamazoo Avenue near 60th Street and 20,000 vehicle trips near the intersection of Kalamazoo Avenue and 52nd Street. As commercial properties develop in Kentwood at these intersections, more truck traffic will be apparent. Since residences are located along Kalamazoo, median landscaping and street trees are critical to minimize noise and impact on adjacent residential properties.

Principle 3: Place Strengthening

A. Gateway:

The Kalamazoo Avenue corridor offers an opportunity for gateways into Kentwood at the intersections with 60th Street and 44th Street. A likely location for a significant gateway would be at the northeast corner of 60th and Kalamazoo. The gateway feature could be incorporated into the future commercial development planned for the intersection, possibly maintained by the owner of the development. This type of gateway is less likely at the 44th and Kalamazoo intersection because of limited space for a gateway feature; welcome signage could be incorporated instead.

B. Sense of Place:

The Kalamazoo corridor may also provide some opportunity to provide a distinct look for the city. At 52nd and Kalamazoo, the redevelopment of the southeast and southwest corners of the intersection may provide a revitalization that can provide a sense of place. At these locations, a PUD zone could be used to establish parking, setbacks, and to specify building materials. The C-5 Neighborhood Commercial zone may also be utilized by developers as this zone allows reduced setbacks and parking. Since Kalamazoo Avenue traffic is increasing, the redevelopment of the intersection is inevitable, and demand for the redevelopment of the intersection is likely.

The opportunity exists for a unique development on the northeast corner of Kalamazoo and 60th Street. Since the adoption of the 2012 Master Plan, the City rezoned the property to PUD and adopted a general plan that sets standards for a unified development. The overall intent is to ensure that the uses in the PUD are reasonably compatible with the adjacent neighborhood and does not have a negative impact on the function of the street system or intersection.

Principle 4: Partnerships and Organization

The redevelopment of the 60th Street and Kalamazoo Avenue intersection will require cooperation between Gaines Charter Township, the Kent County Road Commission and the City of Kentwood. In addition, Kentwood and Gaines Township will need to cooperate regarding bike trails, school traffic, event planning, and traffic issues on the Kalamazoo corridor.

Principle 5: Sustainability

The existence of vacant buildings along Kalamazoo Avenue detract from the sustainability of the corridor. The city may want to consider incentives to encourage the redevelopment of vacant buildings and under-developed sites before undertaking the development of green fields such the northeast corner of 60th and Kalamazoo Avenue. At various locations along the corridor, properties may exist that have environmental limitations which must be addressed to

allow their sale and re-use. The redevelopment plans for these properties should include connections to the surrounding neighborhoods to encourage walking, biking and shopping locally.

While some vacant buildings exist, some residential properties along Kalamazoo Avenue appear to be falling into disrepair. The city will make an effort to educate property owners on the availability of funds for repairs and upgrades to maintain the value of these properties. In addition, it would be desirable to re-locate the off-street parking to the rear yard of the exceptionally deep properties with large paved front yards.

Principle 6: Commercial Development/Redevelopment

As noted earlier, some vacant buildings and properties exist along the Kalamazoo Avenue corridor. The city should do what it can to make the Kalamazoo corridor more attractive by continuing efforts to landscape along the corridor. Incentives such as the Commercial Rehabilitation Act (PA 210 of 2005) should be utilized when possible to encourage developers to make an investment in these commercial areas.

52nd and Kalamazoo Avenue

Current land use at the Kalamazoo Avenue and 52nd Street intersection includes an automotive repair shop, a convenience store/gas station, office buildings and undeveloped property. This intersection is nestled in an established neighborhood and ideally situated to serve a somewhat under-retailed area of Kentwood.

This area should be developed with the surrounding neighborhood in mind, keeping development to a small scale. The emphasis should be on quality development that serves the immediate residents and allows for some convenience for commuters or people living within walking distance.

The 2010 design charrette assisted in the development of recommendations for the intersection based on input from neighbors, property owners, designers, and the city. The process allowed for the formulation of Guiding Development

Concepts to be used to review any proposed development for the 52nd and Kalamazoo Intersection, as follows:

Guiding Development Concepts for 52nd and Kalamazoo Avenue

- Establish open space (green or hard scape plaza) at the intersection corners.
- Provide parking behind buildings, if possible.
- No expansion of land area for commercial or office uses.
- Landscaped buffers/screens for residential uses adjacent to commercial or office uses.
- Area must be walkable.
- Shared access must be provided between buildings.
- Ensure that access to public streets works with boulevard design of Kalamazoo Avenue. A traffic analysis will be required for any proposed development.
- Commercial uses to serve the immediate area or commuters.
- Commercial uses will be permitted at the southeast corner of 52nd and Kalamazoo Avenue but will be required to be those that will have minimal impact on the surrounding neighborhood. Uses must also be well-screened from the adjacent residences.
- Plan is for the long term and must be marketable.



Figure 11: Kalamazoo and 60th Street development concepts

60th Street and Kalamazoo Avenue

This area is located in a segment of the greater Grand Rapids market that has enjoyed success for retail development, especially south of the city of Kentwood. The development of the South Beltline (M-6) and interchange with Kalamazoo Avenue is a logical location for retail development. Within Kentwood, at the northwest corner of the Kalamazoo and 60th Street intersection is an underdeveloped shopping center. Given the difficulty to develop the site, the city has considered amendment of the Master Plan to allow residential use within the undeveloped portion of the commercial center.

The Master Plan recommends commercial use for the northeast corner of 60th Street and Kalamazoo Avenue, although sensitivity needs to be employed between commercial uses and the adjacent residential neighborhood. The city has rezoned and adopted a PUD plan for this intersection to address ingress and egress, storm water detention, cross access, signage, and restricted land uses.

Guiding Development Concepts for 60th and Kalamazoo Avenue (Northeast Corner)

- The area shall be developed as one integrated planned unit development, even if properties within the overall site may not be in common ownership.
- Development will be sensitive to adjacent residential uses to the north and northeast.
- Quality architecture and landscaping will be a requirement for the PUD development at this intersection. Buildings will be predominantly brick with asphalt shingled sloped roofs.
- A traffic impact study will be required for the site at the time of the application for the first proposed use. Access to Kalamazoo will be limited to no more than one full movement driveway; access onto 60th Street will be limited to no more than one full movement driveway and one right-turn in, right-turn out driveway.
- The PUD plan shall integrate vehicular and pedestrian circulation throughout the overall site including reciprocal parking, maintenance agreements and cross access.
- An identity feature will be required at the corner of 60th and Kalamazoo but can be incorporated into the development. Clear vision will need to be maintained at the intersection.

STAFF REPORT: June 6, 2022
PREPARED FOR: Kentwood Planning Commission
PREPARED BY: Lisa Golder
CASE NO.: 17-22 Kum and Go SLU and Site Plan Review Vehicle Fuel Station

GENERAL INFORMATION

APPLICANT: Kum & Go L.C. rep by: Michael McPherson, Atwell LLC
Ryan Halder 12745 23 Mile Road Suite 200
1459 Grand Ave Shelby Twp MI 48315
Des Moines IA 50309

STATUS OF APPLICANT: Owner and owner's representative

REQUESTED ACTION: Applicant is requesting Special Land Use approval and Site Plan Review of a vehicle fuel station located at 5215 Kalamazoo Avenue.

EXISTING ZONING OF SUBJECT PARCEL: CPUD Commercial Planned Unit Development

GENERAL LOCATION: 5215 Kalamazoo Avenue, 1570 52nd Street and 1549 Kalamazoo Avenue

PARCEL SIZE: 2.28 Acres

EXISTING LAND USE ON THE PARCEL: Existing automotive repair, two single family residential homes

ADJACENT AREA LAND USES: N: 52nd Street and gas station
S: Duplex
E: Kalamazoo Avenue, vacant land
W: Single family homes

ZONING ON ADJOINING PARCELS: N: C-2 Commercial
S: R-2 Two Family Residential
E: C-4 Office
W: R1-C Single Family Residential

Compatibility With Master Plan

The Master Plan recommends commercial use for this location. The 2010 Master Plan incorporated a charrette process to obtain neighborhood input into the development of the major intersections along the Kalamazoo corridor. Neighborhood commercial uses or office uses were recommended. The 2020 Master Plan recommends small-scale development to serve the immediate residents as well as convenience for commuters. The 2020 Master Plan also establishes some development concepts for the intersection of 52nd Street and Kalamazoo Avenue, as follows:

- Establish opens space (green or hardscape plaza) at the intersection corners
- Provide parking behind buildings, if possible
- No expansion of commercial or office
- Landscaped buffers/screens for residential uses adjacent to commercial or office uses
- Area must be walkable
- Shared access must be provided between buildings
- Ensure that access to public streets works with boulevard design of Kalamazoo Avenue
- A traffic analysis will be required with the development.
- Commercial uses to serve the immediate area or commuters
- Plan is long term and marketable.

Relevant Zoning Ordinance Sections

Chapter 15.04 D lists the Special Land Use requirements for Vehicle Fuel Stations. General standards for Special Land Uses are found in Section 15.02. Site plan review standards are found in Section 14.05.

Zoning History

The site was initially developed as a gas station, and then approved as an automotive repair establishment called Don and Sons in 1995. At one time the site was also considered for a pharmacy. In 2003 the Planning and City Commissioners approved a city-sponsored rezoning of the site to Commercial Planned Unit Development (CPUD). The CPUD approval was based on a PUD Statement dated February 6, 2003 and February 24, 2003 (See attached PUD Statement and City Commission approval). The rezoning was for property of approximately 3 acres in size; this is larger than the acreage proposed for the Kum and Go development because the rezoning included the 52nd Street and Kalamazoo Avenue rights of way (extended to the centerline). Excluding the rights of way, the proposed site is 2.28 acres in area.

The 2003 PUD Statement specified a preference for Permitted or Special Land Uses that would not generate significant traffic in the peak hour of operation for the intersection. The CPUD also specifies that the hours of operation for any use be compatible with residential uses, from approximately 7:00AM-10:00PM.

The 2003 PUD statement was intended to be utilized in the review of future proposed plans for the corner. A plan for Thompsons Auto Repair was approved by the city in 2004. The use was

seen as an interim measure prior to a development project that would incorporate all of the land zoned Commercial Planned Unit Development.

The proposed Kum and Go plan was reviewed by the Land Use and Zoning Subcommittee in January of 2022; the Committee was generally in favor of the concept.

TECHNICAL INFORMATION

Street and Traffic

The site is located at the southwest corner of 52nd Street and Kalamazoo Avenue. Kalamazoo Avenue at the 52nd Street intersection is a 4-lane road that includes two northbound and two southbound lanes and a right turn lane at 52nd Street. Lanes for indirect left turns are located north and south of the intersection; only the north indirect left crossover is suitable for truck traffic. A grass median prohibits left turns into and out of the proposed Kum and Go development from Kalamazoo Avenue. 52nd Street is a two-lane road with a center turn lane. At the intersection with Kalamazoo Avenue, 52nd Street allows for direct left hand turns for both northbound and southbound traffic. There are dual left turns onto Kalamazoo from west-bound 52nd Street.

The Kum and Go development proposes two driveways. The Kalamazoo driveway allows right-in and right-out movements only. The width of the driveway is 50 feet. The applicant shall provide turning templates to confirm the need for this width. Pavement markings may also be employed to ensure that vehicles entering and exiting the site remain in their designated lanes; a deceleration lane may be necessary to allow traffic to get out of the through lanes and make a turn into the site at a safe speed. Slower speeds are also safer for pedestrians walking through the area.

The 52nd Street driveway allows full movement of traffic in and out of the site. The applicant must address any issues this may create with left turns into Newcastle Drive. The proposed 52nd Street driveway is also 55 feet in width. Again, the applicants shall provide information to justify the need for this width.

Trip Generation and Traffic Analysis

Applicant has submitted a Traffic Impact Study as required by the Zoning Ordinance and by the conditions of the CPUD approval for this site. The traffic analysis was based on 2018 traffic volumes of 20,065 vehicles per day on Kalamazoo Avenue and 2016 traffic volume of 16,469 vehicle trips per day on 52nd Street. The analysis reviewed current conditions, and then estimated future conditions, given the proposed gas station use. The study concluded that all intersection approaches continue to operate at an acceptable level of service (LoS D or better). The study also identified between 16% and 35% of the traffic volume as “pass-by” trips, or trips by vehicles that were already on the roadway and therefore not created by the new development. In addition, the study concluded that there were adequate gaps in through traffic to allow turns left into and out of the 52nd Street driveway without excessive queueing. A summary of the traffic analysis is attached. The city is working with a traffic consultant to review the conclusions of the traffic study and to

determine whether the Commission should require the developer to make additional improvement or changes to the proposed plan.

Staff Review

1. The Kum and Go development is proposed on a 2.28 acre property on the southwest corner of 52nd Street and Kalamazoo Avenue. The property owned by the applicant also includes an attached residentially-zoned property with frontage on Newcastle Drive (not part of the 2.28 acre parcel). The parcel is intended to be split from the CPUD parcel and developed as single family residential.
2. The proposed Kum and Go store is a gas station with 6 pumps (12 pumping stations). The proposed floor plan includes food preparation and sales, a seating area, and a retail area for convenience items. The applicant has indicated their desire to sell beer and wone at the site. The applicant shall provide a floor plan for city review.

Special Land Use

3. Fuel stations are permitted in the CPUD zone with Special Land Use, meeting the general requirements and the requirements of Section 15.04D, as follows:

1. A vehicle fuel station building and its accessory uses and buildings shall be located not less than fifty (50) feet from any right-of-way line or from any side or rear lot line abutting a Residential District. This setback requirement shall not apply to accessory parking.

The proposed fuel station is adjacent to a residential district. The convenience store is located 87' from the common lot line. The building it set back at least 100' from the 52nd Street and Kalamazoo Avenue rights of way.

2. Where adjoining a residential use or Residential District, Buffer Zone "B" is required (see Chapter 19; Landscaping). In addition, the Planning Commission may require a solid wall or solid fence along the lot line having a maximum height of six (6) feet.

Buffer zone "B" requires a minimum width of 20' and one canopy tree, 1 evergreen tree, and 5 shrubs per each 40 feet along the property line. There is 620 feet of the property that adjoins a residential use. Therefore, 16 canopy trees, 16 evergreen trees, and 80 shrubs must be provided along these areas that are adjacent to the residential uses. The applicant shall indicate how these standards are met by the proposed landscaping plan. It is possible that existing trees may help to meet the ordinance requirements.

3. The minimum frontage shall be one hundred fifty (150) feet and the minimum lot area one (1) acre.

The fuel station meets this requirement.

4. Accessory auto related facilities located on the premises such as wash facilities, vehicle rental and vehicle repair are allowed however must obtain separate special land use permits as provided for in this Chapter.

These accessory uses are not proposed.

5. The lot shall be located so that at least one (1) side abuts an arterial street.

The lot abuts two arterial streets.

6. The site shall be limited to no more than one (1) driveway for each street on which it has frontage.

The applicant is proposing one driveway on Kalamazoo Avenue and one on 52nd Street. driveways on Kalamazoo Avenue. .

7. All storage of material, merchandise and equipment shall be within the building.

The applicant's statement of operation indicates the desire to sell merchandise outside of the building. The location of the outdoor sales shall be indicated on the site plan as the storage of these materials could be considered contrary to this requirement.

8. Gasoline or other flammable mixtures shall not be used to wash down the premises.

9. In the event that a Vehicle Fuel station has been abandoned or not used as a Vehicle Fuel station for a period of more than one (1) year, any application to operate the premises as a Vehicle Fuel station shall be considered as an application for a new Vehicle fuel station.

10. The applicant shall comply with Michigan Department of Environmental Quality (EGLE) requirements.

General Special Land Use Standards

3. As a Special Land Use, the proposed fuel station must also comply with the standards of Section 15.02, as follows:

- A. Be designed, constructed, operated and maintained so as to be harmonious and appropriate in appearance, with the existing or intended character of the general vicinity and that a use will not change the essential character of the area in which it is proposed.

The area has been rezoned to a CPUD in 2003. The CPUD Statement from that time set certain specific requirements for the PUD that go beyond the Special Land Use Standards of the Zoning Ordinance. The City, in its review of the CPUD must come to terms with these standards: to either waive the 2003 standards, or insist that the developer comply with the standards. The Special Land Use standards within Section 15.02 A requires that the development is harmonious and

appropriate with the existing or intended uses of the general vicinity. The proposed use is a gas station that is low in profile and includes a green buffer between the gas station and the existing single family homes to the west. Other commercial buildings and a gas station exist to the north, and commercial/office are recommended for the property to the east.

- B. Be served adequately by essential public facilities and services such as highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewage facilities or schools.

The use is adequately served by public facilities

- C. Not create excessive additional requirements at public cost for public facilities and services.

The use will not create additional requirements at public cost.

- D. Not involve uses, activities, processes, materials, and equipment or conditions of operation that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke, fumes, glare, electrical or electromagnetic interference or odors.

The applicant's traffic analysis indicates that the level of service projected for the use would not be impacted by the introduction of the gas station. It is possible that traffic through the site could be further reduced by eliminating the driveway on the west side of the gas station, thereby keeping all vehicles east of the proposed building. In addition, the Commission could also reduce service/loading hours proposed for the development or reduce operating hours.

The lighting proposed for the site does not meet city ordinances; pump islands for gasoline stations are limited to a maximum of 22 footcandles; the applicant is proposing readings in some locations under the canopy in excess of 70 footcandles. In addition, the average to minimum standard is exceeded. The applicant must also provide detail regarding the height of the light poles proposed; the maximum height when adjacent to a residential development is 20'.

- E. Be compatible and in accordance with the goals, objectives and policies of the Master Plan and promote the Intent and Purpose of the zoning district in which it is proposed to locate.

The use is generally consistent with the Master Plan.

- F. Be subject to stipulations by the Planning Commission of additional conditions and safeguards deemed necessary for the general welfare, for the protection of individual property rights, and for ensuring that the intent and objectives of this Ordinance will be observed. The breach of any condition, safeguard, or requirement shall automatically invalidate the granting of the Special Land Use.

G. Comply with all applicable licensing ordinances

Site Plan Review

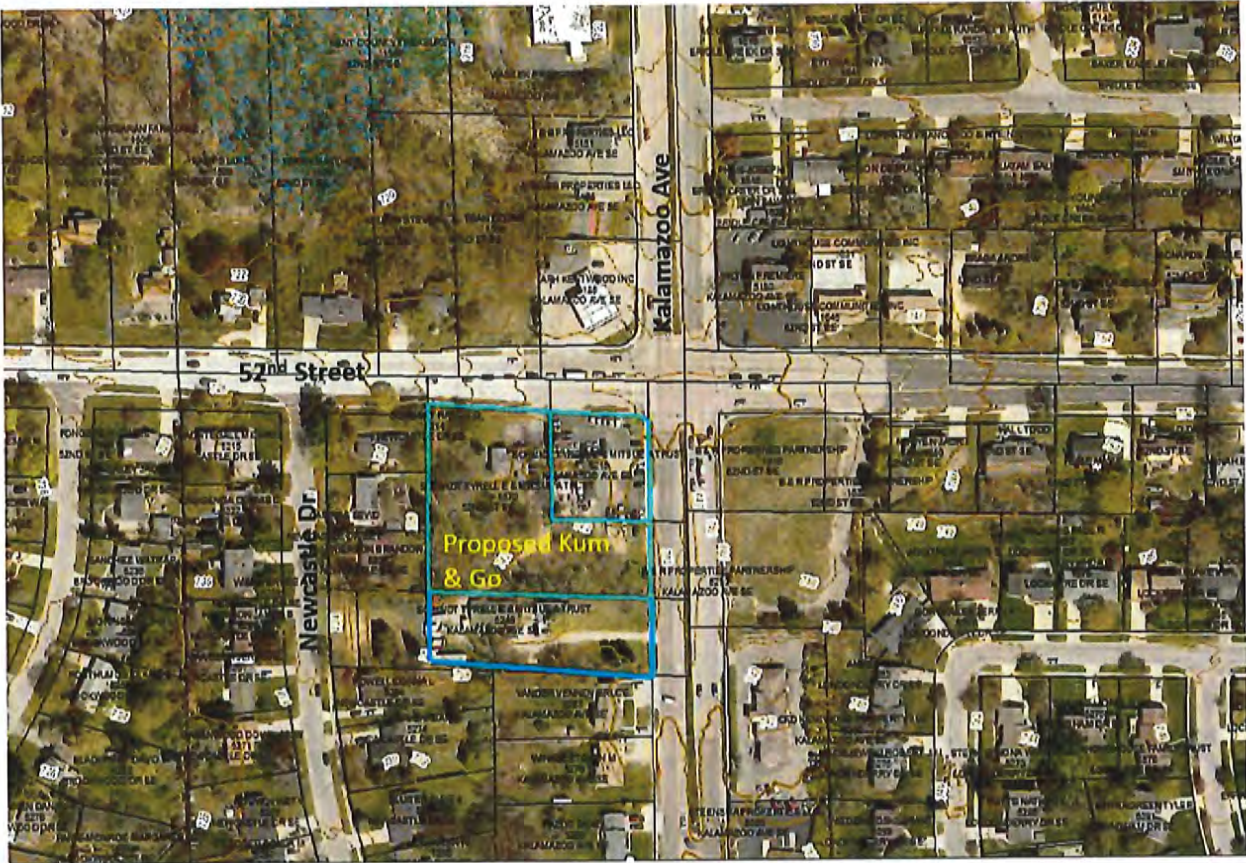
5. Access to the site for fuel trucks will be from Kalamazoo Avenue only. Since 52nd Street is not a truck route, commercial trucks or fuel trucks delivering to the site would have to travel south to the site from 44th Street, or north to the site from the M-6 area. A crossover exists north of the Kalamazoo/52nd Street intersection to allow northbound vehicles make an indirect left to access the Kalamazoo driveway. The crossover south of the proposed gas station is about 2000 feet to the south.
6. The applicant shall provide information on truck circulation through the site.
7. The proposed convenience store building will be 5,620 square feet in area. The use will require 24 parking spaces, provided as per the proposed plan.
8. The applicant meets the open space requirements for the CPUD zone.

Attributes:

- Meets open space requirements
- Meets parking requirements
- Eliminates interim use that has been challenging to keep in line with property maintenance
- Low profile building

Issues:

- Lighting standard not met
- Merchandise being sold outside building
- Some inconsistencies with CPUD Plan from 2003—brick façade, hours of operation
- Driveways too wide
- Traffic study to be reviewed—gaps for left turns in



Proposed Kum and Go Parcels



Existing Conditions



To: Kentwood Planning Department
From: Brad Boomstra, P.E.
City Engineer
Date: June 20, 2022
Re: **Kum & Go**
5215 Kalamazoo Avenue SE
41-18-32-229-042 (Also 41-18-32-229-040 & 41-18-229-022)
Review of Site Plan Received 5-19-2022

We have completed our review of the proposed site plans dated 5-19-2022 and received in our office on 6-1-2022 for the above referenced project.

Kentwood Engineering has the following comments that will need to be addressed before this office can grant site plan / construction plan approval:

Overall Plan Comments:

1. The multiple parcels at this site must be combined into a single parcel. This is required whenever new development spans across adjacent parcels. Please contact the Kentwood Assessor, regarding this procedure.

Street / Sidewalk / Parking Lot:

1. A pavement section details/cross sections must be shown on the plan for all paving types proposed.
2. Concrete curb and gutter per MDOT standard plan R-30 series (Detail F4, as modified for the tilt of the gutter pan), and a commercial drive opening per MDOT standard plan R-29 series (Detail M) are required for the proposed drive approach or the private street entrance within the 52nd Street and Kalamazoo Avenue right-of-way. Let us know if you need these details. They must be referenced in the plan notes or included on your detail sheet.
3. The pavement within the commercial drive approach between the sidewalk and the gutter pan must be concrete at least 6 inches thick.
4. No single curb cut shall be less than 10 feet in width, nor more than thirty (30) feet in width.
5. For any curb cut which terminates less than five (5) feet from a construction joint, the contractor shall remove and replace the existing curb to the next joint.
6. The sidewalk must be carried across the commercial drive. Where it crosses, it must be at least 6" thick.

7. Include the City of Kentwood General Notes on the plan(s). These notes are required for the work being performed in the 52nd Street and Kalamazoo Avenue right-of-way. Highway permit conditions, specifications and required general plan notes are posted on the City's website, and can be accessed at <http://www.ci.kentwood.mi.us> (hover over "CITY SERVICES" and "DEPARTMENTS" then click "ENGINEERING", then click the "RIGHT-OF-WAY" link near the top of the page).
8. Additional comments regarding the street and/or sidewalk may be made upon receipt of construction plans.

Grading:

1. Indicate and label the 1% chance (100-year) flood elevation on the site plan or add a note that none exists on this site.
2. Indicate and label the wetland delineation line or add a note that none exist on this site.
3. In the area of the former building, there may be disturbed/loose soil remaining from the previous construction. Be sure the specifications require that an independent testing laboratory be on site to perform compaction testing during site grading. We want to be sure that all backfill soils are properly placed and compacted prior to new foundation construction.
4. Show storm sewers, pipe sizes, pipe slopes, invert elevations and rim elevations on the grading plan.
5. Additional comments regarding grading will be made upon receipt of construction plans.

Storm Sewer / Drainage:

1. Provide a tributary area map and calculations to verify the capacity of the proposed and existing storm pipes to carry a 10-year storm (or a 100-year event if no overland floodway is provided). Use the Kentwood Storm Sewer Design Standards available on the City's website; they contain a Kentwood-specific IDF curve and time of concentration (T_c) nomograph. Use a minimum initial T_c of 10 minutes. Maintain a minimum cleansing velocity of 2.5 ft/sec in the pipes. Calculations must be sealed by a registered engineer.
2. Please indicate the type of material being used for the storm pipe in various areas (i.e., under the roadway, under lawn areas, etc).
3. For each of the storm pipes, indicate the size, slope, length and invert elevations.
4. Additional comments regarding drainage will be made upon receipt of construction plans.

Detention Basin:

1. On-site stormwater detention will be required for this parcel. Section 78-123 of the City of Kentwood Ordinance requires that the new detention facility be sized for the entire parcel in a *fully developed condition*. We will need to see these calculations.

2. The amount of detention volume required may be based on an estimate of the percentage of impervious surface area based on the Kent County Subdivision Drainage Rules (short method #1), on the basis of 0.1 acre-ft per acre for the entire parcel (short method #2) or, alternately, a tabulation of actual reservoir routing (long method). Routing calculations usually result in the lowest required volume. The detention sizing must be based on a 25-year storm. We will need to see detention sizing and release calculations.
3. Kentwood allows a maximum detention release rate of 0.33 cfs/acre, based on the total parcel size.

Soil Erosion and Sediment Control:

1. We will need to see some additional soil erosion control measures as part of the plan review. Section 78-62 of the City of Kentwood Ordinance (posted on the City's website) contains minimum requirements for information that shall be included on the plan. Go to: <http://www.ci.kentwood.mi.us> (hover over "CITY SERVICES" and "DEPARTMENTS" then click "ENGINEERING", then click the "SOIL EROSION AND STORMWATER" link near the top of the page). Please review this ordinance carefully.
2. Per the requirements of Part 91 of Public Act 451, all proposed grading and soil erosion controls must be shown on a sheet(s) titled, at least in part, "SOIL EROSION AND SEDIMENTATION CONTROL PLAN". This plan should show surface features related to grading and soil erosion and sediment control. The SESC Plan may be combined with other plans and improvements as long as clarity is retained. Two (2) copies of this sheet, once approved, will need to accompany the application for an earth change.
3. Include a written description of the soil types of the exposed land area contemplated for an earth change.
4. Place a note on the soil erosion control plan to indicate that existing and new catch basins shall be protected with an inlet filter drop (silt sack). Straw bales or fabric placed under the grate are NOT acceptable, and sediment traps alone are not sufficient to provide adequate sediment filtration. Such a note might read, "EXISTING AND NEW CATCH BASINS SHALL BE PROTECTED WITH AN INLET FABRIC DROP (SILT SACK)." Include a simple detail of the proposed silt sack on the Soil Erosion and Sediment Control plan. We can provide you with an acceptable CAD detail upon request.
5. Place the following notes on the soil erosion control plan:
 - ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL COMPLY WITH ARTICLE 2 OF CITY OF KENTWOOD ORDINANCE 78 AND PART 91 OF PUBLIC ACT 451.
 - ALL SOIL EROSION AND SEDIMENT CONTROLS SHALL BE INSPECTED AND MAINTAINED ON A DAILY BASIS AND IMMEDIATELY FOLLOWING EVERY SIGNIFICANT RAINFALL EVENT.
 - ALL EXCESS SPOILS ARE TO BE REMOVED FROM THE SITE. OTHERWISE, STOCKPILES MUST BE PROVIDED WITH TEMPORARY AND PERMANENT STABILIZATION MEASURES.
 - EXCESS DIRT IS NOT TO BE PLACED ON ANY AREAS ON OR ADJACENT TO THE SITE WHERE THE PLAN DOES NOT SHOW THE AREA BEING DISTURBED. (This area of disturbance, or grading limits, must be clearly shown on the site plan.)

- SILT FENCING IS REQUIRED ALONG ALL DOWNSTREAM EDGES OF THE GRADING LIMITS AND MUST REMAIN IN PLACE UNTIL VEGETATION IS UNIFORMLY RE-ESTABLISHED. THE SILT FENCE MUST BE TOED IN A MINIMUM OF 6 INCHES ALONG ITS BASE. (Remember that silt fences are intended to intercept *sheet flow* only and must always be installed *parallel* with the ground contours. Silt fences must not cross ravines, overland floodways, ditches, swales, etc. where concentrated flows occur.)
 - ALL DISTURBED BANKS EQUAL TO OR GREATER THAN 4:1 AND THE DETENTION BASIN BANKS AND BOTTOM MUST BE COVERED WITH TOPSOIL, SEED AND NORTH AMERICAN GREEN S-150 (OR APPROVED EQUAL) EROSION CONTROL BLANKET. THIS BLANKET, ALONG WITH THE NECESSARY STAPLES OR WOOD PEGS, SHALL BE PLACED PER MANUFACTURER'S RECOMMENDATIONS. SEAMS SHALL BE PLACED PARALLEL TO THE DIRECTION OF SURFACE RUNOFF. (Indicate such areas with shading or hatching on the plan.)
 - ALL SOIL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED UNTIL VEGETATION IS UNIFORMLY RE-ESTABLISHED AND THE SITE IS PERMANENTLY STABILIZED.
 - NO SEDIMENT SHALL BE TRACKED ONTO THE ADJACENT PUBLIC STREET AND IF IT DOES OCCUR, IT SHALL BE CLEANED DAILY.
 - THE STORMWATER DETENTION BASIN IS TO BE THE FIRST ITEM OF EARTH MOVING AND SHALL BE CONSTRUCTED IN CONJUNCTION WITH THE INSTALLATION OF SOIL EROSION CONTROL MEASURES.
6. Per the requirements of Part 91 of P.A. 451, place on the soil erosion control plan a bar graph or chart showing the proposed timing and sequence of each proposed earth change. It must include the installation of both permanent and temporary soil erosion and sedimentation controls, as well as the removal of temporary controls. It must also show the sequence of any construction phases.
7. A stone construction exit must also be included as part of the soil erosion control plan. Indicate the location of this exit on the plan, along with a detail. The length of the rock construction exit shall be at least 50 feet and shall consist of a 6-inch minimum layer crushed rock or stone on top of non-woven geosynthetic fabric (MDOT "Heavy Geotextile Liner"). The size of the stone shall be selected so that it cannot get caught between dual truck tires.
8. Additional soil erosion and sediment control comments may be made by Kentwood staff upon further review.

Utilities (Sanitary & Water):

1. A cleanout will be needed along the sanitary lateral, as the proposed lead is longer than 100 feet (BOCA requirement).
2. The fire hydrants should be moved so that they are in line with the property lines and therefore should not interfere with future driveways.
3. Sanitary sewer, sanitary manhole and watermain construction shall follow the City of Kentwood specifications. Specify EJIW Type 1040 castings. All main line sanitary sewer pipe shall be PVC composite pipe (truss pipe). PVC solid wall (SDR-26 or SDR-

- 35) is not allowed. This is based on our experience with the performance of PVC solid wall pipe.
4. Show the grade and the material of the proposed sanitary lateral. The Michigan Plumbing Code requires 1/8" per foot (or 1%) slope for both 6-inch and 4-inch service laterals.
 5. Additional comments regarding utilities will be made upon receipt of construction plans.

Required Permits, Bonds, Fees [and Escrow]:

1. Because this parcel is adjacent to a public street, over one (1) acre is being disturbed, or is within 500 feet of a lake or stream, a Permit for an Earth Change (fee \$400) and a \$5,000 soil erosion control performance bond or an irrevocable letter of credit using the City of Kentwood format will be required through Kentwood Engineering. The Owner/Developer must sign the Permit for an Earth Change. Both the Owner/Developer and the Contractor must be named on the bond. Please contact us if you need a permit and/or a bond template form, or one can be accessed on the City's website at <http://www.ci.kentwood.mi.us> (hover over "CITY SERVICES" and "DEPARTMENTS" then click "ENGINEERING", then click the "SOIL EROSION AND STORM WATER" link near the top of the page). There is a 365-day limit to complete the work under this permit.
2. Your contractor will need a Drive Permit from Kentwood Engineering for the proposed commercial drive into the 52nd Street and Kalamazoo Avenue right-of-way. A \$10,000 annual ROW bond or an irrevocable letter of credit using the City of Kentwood format and a certificate of insurance (with the City of Kentwood named as additional insured) will also be required. We will need to see a plan of the proposed commercial driveway at a plan scale no greater than 1" = 50'. Contact us if you need a permit and/or bond form, or they can be accessed on the City's website. Highway specifications, general conditions and required plan notes are also posted on the City's website.
3. A highway permit (\$800) may also be needed for any utility work in the 52nd Street and Kalamazoo Avenue right-of-way. A \$20,000 right-of-way bond or an irrevocable letter of credit using the City of Kentwood format and a certificate of insurance (with the City of Kentwood named as additional insured) will also be required. We will also require that a traffic plans be submitted to us for road closure, signage and detours. Kentwood Engineering must be notified at least two (2) working days prior to making the open street cut so that Kentwood Police and the Kentwood Public Schools can be properly notified.
4. A \$30,000 Site Grading and Stormwater Management Bond or an irrevocable letter of credit using the City of Kentwood format and an administrative fee of \$600 will be required through Kentwood Engineering. This bond is posted to assure that the plan, once approved, is constructed in the field according to that plan. Both the Owner/Developer and the Contractor must be named on the bond. Let us know if you need our bond template, or it can be accessed on the City's website.

5. Please note that all required bonds and permit applications and fees must be submitted to, and accepted by, Kentwood Engineering *before any permits can be issued!* There will be no “partial” or “conditional” permits issued.

Miscellaneous / Reminders:

1. Please be advised that at the completion of construction, a civil engineer or surveyor will need to provide an as-built plan to the City of Kentwood with a certification by a registered engineer stating that the site grading and the stormwater system were constructed in accordance with the approved plans. A copy of the certification form is available upon request or on the City's website.
2. Remember that, for a Building Permit to be issued, other City departments (fire, assessor, treasurer, water, planning) may have comments regarding this plan. Contact Kentwood Inspections (Renee Hargrave, 554-0781) regarding building permit application procedures, fees, plan requirements and approval status.
3. Once final approval by all departments has been granted, make sure the contractor has the latest approved set of plans before beginning construction!

Should you have any questions regarding this department's review, please feel free to contact our office.

cc: Kentwood Engineering Permit Staff

City of Kentwood



Occupancy: **KUM & GO**
Occupancy ID: **23333**
Address: **5215 Kalamazoo AVE SE**
KENTWOOD MI 49508

Inspection Type: **BUILDING PLAN-NEW BUILDING**

Inspection Date: **6/8/2022**

By: **QUICK, PATRICK A (0281)**

Time In: **09:36**

Time Out: **00:00**

Authorized Date: **06/08/2022**

By: **QUICK, PATRICK A (0281)**

Form: **PLAN REVIEW - IFC**
2015

Inspection Topics:

INTRODUCTION

REVIEW REQUIREMENTS.

We have reviewed your plans for the above facility. The following items are noted for compliance with code requirements. Code references to the International Fire Code 2015, as amended, are noted IFC; references to the Michigan/International Building Code are noted MBC; references to the City of Kentwood Code of Ordinances are noted KC; references to National Fire Protection Association codes and standards are noted NFPA. Please review the appropriate code for further details. All construction and processes must meet applicable code provisions.

Status: **PLAN REVIEW**

Notes: **Preliminary Plan review for new building on existing site.**

GENERAL; CONSTRUCTION AND DEMOLITION

SMOKING.

Smoking shall be prohibited except in approved areas. "No Smoking" signs shall be posted in a conspicuous location. In approved areas where smoking is permitted, approved noncombustible ashtrays shall be provided on each table and at other appropriate locations. (IFC 310; 3304.1)

Status: **PLAN REVIEW**

Notes:

WASTE DISPOSAL/WASTE ACCUMULATION.

All combustible debris, rubbish and waste material must be removed daily, and shall not be disposed of by burning. (IFC 304.1, 3304.2)

Status: **PLAN REVIEW**

Notes:

SPONTANEOUS IGNITION.

Materials susceptible to spontaneous ignition, such as oily rags, shall be stored in a listed disposal container. (IFC 3304.2.4)

Status: **PLAN REVIEW**

Notes:

REQUIRED ACCESS.

Approved vehicle access for fire fighting shall be provided to all construction or demolition sites prior to and during the time of construction. Access road(s) (20 feet wide, designed and maintained to support a vehicle weighing 20 tons on a single axle with dual wheels and standard road tires) shall be constructed as required. This road may be gravel, but vehicle access must be maintained and unobstructed at all times. (IFC 501.4, 503.1.1, 503.2.3, 503.4, 3310.1)

Status: **PLAN REVIEW**

Notes:

FIRE LANE(S).

Provide fire lane(s) on the _____ side(s) of the building. Each side of the building where a fire lane is required must have an entrance as close to the middle as possible. The fire lane must be a minimum of 10 feet and a maximum of 30 feet from the building, and within 100 feet of all fire department connections. (IFC 503.1.1)

Status: **PLAN REVIEW**

Notes: **One side of building required.**

"NO PARKING - FIRE LANE" SIGNS.

"No Parking – Fire Lane" signs shall be placed no closer than 25 feet apart or further than 75 feet apart. (IFC 503.1.1) The bottom of the sign must be 6'4" from grade level.

Status: **PLAN REVIEW**

Notes:

ADDRESS NUMBERS.

Address numbers. Install _____ inch Arabic numerals on the front of the building in a color that contrasts with the immediate background on which the numerals are mounted and that will be readily visible during normal daylight. (IFC 505.1 – 505.1.7)

Status: **PLAN REVIEW**

Notes: **6 inch numbers on address side of building**

GAS METERS/IMPACT PROTECTION.

Gas meters, regulators and piping shall not be placed where they will be subjected to damage, or shall be protected by approved physical barriers. (IFC 603.9, 312; NFPA 54)

Status: **PLAN REVIEW**

Notes:

MAINTENANCE OF MEANS OF EGRESS.

Required means of egress shall be maintained during construction and demolition, remodeling or alterations and additions to any building. (IFC 3311.2)

Status: **PLAN REVIEW**

Notes:

USE OF MOTORIZED CONSTRUCTION EQUIPMENT.

Internal-combustion-powered construction equipment shall be used in accordance with the following conditions: 1. Equipment shall be located so that exhausts do not discharge against combustible material. 2. Exhausts shall be piped to the outside of the building. 3. Equipment shall not be refueled while in operation. 4. Fuel for equipment shall be stored in an approved area outside of the building. (IFC 3316.1)

Status: **PLAN REVIEW**

Notes:

FIRE PROTECTION AND UTILITY EQUIPMENT IDENTIFICATION AND ACCESS

LOCK BOX/KEY BOX REQUIRED.

Install a Knox Box in a location approved by the Fire Marshal. (IFC 506.1, 506.7)

Status: **PLAN REVIEW**

Notes: **Knox Box can be ordered at knoxbox.com**

Provide keys at completion of project

ROOM/EQUIPMENT IDENTIFICATION.

Rooms containing controls for air-conditioning systems, sprinkler risers and valves, or other fire detection, suppression or control elements shall be identified for the use of the fire department. Approved signs required to identify fire protection equipment and equipment location shall be constructed of durable materials, permanently installed and readily visible. (IFC 509.1)

Status: **PLAN REVIEW**

Notes:

ELECTRICAL EQUIPMENT, WIRING AND HAZARDS

LABELING.

Doors into electrical control panel rooms shall be marked with a plainly visible and legible sign stating ELECTRICAL ROOM or similar approved wording. The disconnecting means for each service, feeder or branch circuit originating on a switchboard or panel-board shall be legibly and durably marked to indicate its purpose unless such purpose is clearly evident. (IFC 605.3.1)

Status: **PLAN REVIEW**

Notes:

MECHANICAL REFRIGERATION

DOCUMENTATION OF MECHANICAL REFRIGERATION COMPLIANCE.

Provide documentation that mechanical refrigeration units and systems comply with the requirements of Section 606 of the International Fire Code.

Status: **PLAN REVIEW**

Notes:

PENETRATIONS.

FIRE-RESISTANCE RATED WALLS.

Penetrations into or through fire walls, fire barriers, smoke barrier walls and fire partitions shall comply with Sections 714.3.1 through 714.3.3. Penetrations in smoke barrier walls shall also comply with Section 714.4.4 of the Michigan Building Code. (MBC 714.3)

Status: **PLAN REVIEW**

Notes:

FIRE PROTECTION SYSTEMS

FIRE PROTECTION CONSTRUCTION DOCUMENTS.

Construction documents and calculations for fire protection systems shall be submitted for review and approval prior to system installation. (IFC 901.2)

Status: **PLAN REVIEW**

Notes:

FIRE PROTECTION INSTALLATION ACCEPTANCE TESTING.

Fire detection and alarm systems, fire-extinguishing systems, fire hydrant systems, fire standpipe systems, fire pump systems, private fire service mains and all other fire protection systems and appurtenances thereto shall be subject to acceptance tests as contained in the installation standards and as approved by the fire code official. The fire code official shall be notified before any required acceptance testing. (IFC 901.5)

Status: **PLAN REVIEW**

Notes:

OCCUPANCY AFTER REQUIRED TESTS.

It shall be unlawful to occupy any portion of a building or structure until the required fire detection, alarm and suppression systems have been tested and approved. (IFC 901.5.1)

Status: **PLAN REVIEW**

Notes:

PORTABLE FIRE EXTINGUISHERS (3A:40BC).

Install fire extinguishers rated 3A:40BC so there is no more than 75 feet of travel to any extinguisher. (IFC 906; NFPA 10)

Status: **PLAN REVIEW**

Notes:

FIRE ALARM AND DETECTION SYSTEM DOCUMENTS.

Construction documents and shop drawings for fire alarm systems shall be submitted for review and approval prior to system installation. (IFC 907.1.1, 907.1.2)

Status: **PLAN REVIEW**

Notes:

GROUP M FIRE ALARM SYSTEM.

Install a manual fire alarm system that activates the occupant notification system. (IFC 907.2.7)

Status: **PLAN REVIEW**

Notes:

OCCUPANT NOTIFICATION SYSTEMS.

A fire alarm system shall annunciate at the fire alarm control unit and shall initiate occupant notification upon activation, in accordance with Sections 907.5.1 through 907.5.2.3.3. Where a fire alarm system is required by another section of the code, it shall be activated by: 1. Automatic fire detectors. 2. Automatic sprinkler system waterflow devices. 3. Manual fire alarm boxes. 4. Automatic fire-extinguishing systems. (IFC 907.5)

Status: **PLAN REVIEW**

Notes:

AUDIBLE ALARM APPLIANCE SILENCING.

Provide a means at the fire alarm control panel to silence audible alarm devices without resetting the panel. Where audible alarm devices are provided in connection with visual alarm devices, there shall be a means provided to silence the audible alarm devices without disabling the visual alarm devices or resetting the panel. (IFC 907.5.2.1.3)

Status: PLAN REVIEW

Notes:

SIGNS.

Where fire alarm systems are not monitored by a supervising station, an approved permanent sign shall be installed adjacent to each manual fire alarm box that reads: WHEN ALARM SOUNDS—DIAL 9-1-1. (IFC 907.5.2.4)

Status: PLAN REVIEW

Notes:

SPRINKLER PLANS/HYDROSTATIC TEST.

Before any sprinkler work is begun, plans must be approved by the Kentwood Fire Department. Hydrostatic test must be witnessed by the Kentwood Fire Department. (NFPA 13)

Status: PLAN REVIEW

Notes:

LOCATION OF FIRE DEPARTMENT CONNECTIONS.

With respect to hydrants, driveways, buildings and landscaping, fire department connections shall be so located that fire apparatus and hose connected to supply the system will not obstruct access to the buildings for other fire apparatus. The location of the fire department connections shall be approved by the fire code official. (IFC 912.2)

Status: PLAN REVIEW

Notes:

VISIBLE LOCATION OF FIRE DEPARTMENT CONNECTIONS.

Fire department connections shall be located on the street side of buildings, fully visible and recognizable from the street or nearest point of fire department vehicle access or as otherwise approved by the fire code official. (IFC 912.2.1)

Status: PLAN REVIEW

Notes:

MEANS OF EGRESS

HEADROOM.

Protruding objects are permitted to extend below the minimum ceiling height required provided a minimum headroom of 80 inches shall be provided for any walking surface, including walks, corridors, aisles and passageways. Not more than 50 percent of the ceiling area of a means of egress shall be reduced in height by protruding objects. Exception: Door closers and stops shall not reduce headroom to less than 78 inches. A barrier shall be provided where the vertical clearance is less than 80 inches high. The leading edge of such a barrier shall be located 27 inches maximum above the floor. (IFC 1003.3.1)

Status: PLAN REVIEW

Notes:

FLOOR SURFACE.

Walking surfaces of the means of egress shall have a slip-resistant surface and be securely attached. (IFC 1003.4)

Status: PLAN REVIEW

Notes:

OUTDOOR AREAS.

Outdoor areas. Yards, patios, courts and similar outdoor areas accessible to and usable by the building occupants shall be provided with means of egress. The occupant load of such outdoor areas shall be assigned by the fire code official in accordance with the anticipated use. Where outdoor areas are to be used by persons in addition to the occupants of the building, and the path of egress travel from the outdoor areas passes through the building, means of egress requirements for the building shall be based on the sum of the occupant loads of the building plus the outdoor areas. (IFC 1004.5)

Status: PLAN REVIEW

Notes:

ILLUMINATION REQUIRED.

Install sufficient emergency lights to illuminate all means of egress whenever the building is occupied. (MBC 1008.1, 1008.2)

Status: PLAN REVIEW

Notes:

EMERGENCY POWER FOR ILLUMINATION.

Provide an emergency power system to illuminate all means of egress for at least 90 minutes in the event of a power failure. (MBC 1008.3.4)

Status: PLAN REVIEW

Notes:

DIRECTIONAL SIGNAGE.

Directional signage indicating the location of all other means of egress and which are accessible means of egress shall be provided at the following: 1. At exits serving a required accessible space but not providing an approved accessible means of egress. 2. At elevator landings. 3. Within areas of refuge. (IFC 1009.10)

Status: PLAN REVIEW

Notes:

DOOR SWING.

Egress doors shall be of the pivoted or side-hinged swinging type and shall swing in the direction of egress travel where serving a room a room or area containing an occupant load of 50 or more persons or a Group H occupancy. (IFC 1010.1.2, 1010.1.2.1)

Status: PLAN REVIEW

Notes:

DOOR SWING FORCE.

The force for pushing or pulling open interior swinging egress doors, other than fire doors, shall not exceed 5 pounds. These forces do not apply to the force required to retract latch bolts or disengage other devices that hold the doors in a closed position. For other swinging doors, as well as sliding and folding doors, the door latch shall release when subjected to a 15-pound force. The door shall be set in motion when subjected to a 30-pound force. The door shall swing to a full-open position when subjected to a 15-pound force. (IFC 1010.1.3)

Status: PLAN REVIEW

Notes:

POWER OPERATED DOORS.

Where means of egress doors are operated or assisted by power, the design shall be such that in the event of power failure, the door is capable of being opened manually to permit means of egress travel or closed where necessary to safeguard means of egress. The forces required to open these doors manually shall not exceed those specified in Section 1010.1.3, except that the force to set the door in motion shall not exceed 50 pounds. The door shall be capable of swinging from any position to the full width of the opening in which such door is installed when a force is applied to the door on the side from which egress is made. (IFC 1010.1.4.2)

Status: PLAN REVIEW

Notes:

DOOR OPERATIONS HARDWARE.

Door handles, pulls, latches, locks and other operating devices on doors required to be accessible shall not require tight grasping, tight pinching or twisting of the wrist to operate. (MBC 1010.1.9.1)

Status: PLAN REVIEW

Notes:

EXIT SIGNS REQUIRED.

Exits and exit access doors shall be marked by an approved exit sign readily visible from any direction of egress travel. The path of egress travel to exits and within exits shall be marked by readily visible exit signs to clearly indicate the direction of egress travel in cases where the exit or the path of egress travel is not immediately visible to the occupants. Intervening means of egress doors within exits shall be marked by exit signs. Exit sign placement shall be such that no point in an exit access corridor or exit passageway is more than 100 feet or the listed viewing distance for the sign, whichever is less, from the nearest visible exit sign. (IFC 1013.1)

Status: PLAN REVIEW

Notes:

EGRESS THROUGH INTERVENING SPACES.

Egress shall not pass through kitchens, storage rooms, closets or spaces used for similar purposes. (IFC 1016.2)

Status: PLAN REVIEW

Notes:

EGRESS THROUGH STOCKROOMS IN GROUP M OCCUPANCIES.

Means of egress are not prohibited through stockrooms in Group M occupancies when all of the following are met: 1. The stock is of the same hazard classification as that found in the main retail area; 2. Not more than 50% of the exit access is through the stockroom; 3. The stockroom is not subject to locking from the egress side; and 4. There is a demarcated, minimum 44-inch-wide aisle defined by similar construction that will maintain the required width and lead directly from the retail area to the exit without obstructions. (IFC 1016.2)

Status: PLAN REVIEW

Notes:

NONEXIT IDENTIFICATION.

Where a door is adjacent to, constructed similar to and can be confused with a means of egress door, that door shall be identified with an approved sign that identifies the room name or use of the room. (IFC 1031.5)

Status: PLAN REVIEW

Notes:

FLAMMABLE AND COMBUSTIBLE LIQUIDS

LABELING AND SIGNAGE FOR FLAMMABLE LIQUIDS.

Provide warning signs for the purpose of identifying the hazards of storing or using flammable liquids. Signage for identification and warning such as for the inherent hazard of flammable liquids or smoking shall be provided in accordance with this chapter and Sections 5003.5 and 5003.6. (IFC 5703.5)

Status: PLAN REVIEW

Notes:

STYLE OF FLAMMABLE LIQUIDS WARNING SIGNS.

Warning signs shall be of a durable material. Signs warning of the hazard of flammable liquids shall have white lettering on a red background and shall read: DANGER – FLAMMABLE LIQUIDS. Letters shall not be less than 3 inches in height and ½ inch in stroke. (IFC 5703.5.1)

Status: PLAN REVIEW

Notes:

FLAMMABLE LIQUID SIGN LOCATION.

Signs shall be posted in locations as required by the fire code official. Piping containing flammable liquids shall be identified in accordance with ASME A13.1. (IFC 5703.5.2)

Status: PLAN REVIEW

Notes:

FLAMMABLE/COMBUSTIBLE LIQUID WARNING LABELS.

Individual containers, packages and cartons shall be identified, marked, labeled and placarded in accordance with federal regulations and applicable state laws. (IFC 5703.5.3)

Status: PLAN REVIEW

Notes:

PROTECTION FROM VEHICLES.

Provide guard posts or other approved means to protect piping, valves or fittings subject to vehicular damage in accordance with Section 312. (IFC 5703.6.4)

Status: PLAN REVIEW

Notes:

SMOKING AND OPEN FLAME FOR FLAMMABLE/COMBUSTIBLE LIQUIDS STORAGE.

Signs shall be posted in storage areas prohibiting open flames and smoking. Signs shall comply with Section 5703.5. (IFC 5704.2.3.1)

Status: PLAN REVIEW

Notes:

VEHICLE IMPACT PROTECTION.

Where protected above-ground tanks, piping, electrical conduit or dispensers are subject to vehicular impact, they shall be protected therefrom, either by having the impact protection incorporated into the system design in compliance with the impact test protocol of UL 2085, or by meeting the provisions of Section 312, or where necessary, a combination of both. Where guard posts or other approved barriers are provided, they shall be independent of each above-ground tank. (IFC 5704.2.9.7.4)

Status: **PLAN REVIEW**

Notes:

CLOSING**SUBJECT TO FIELD INSPECTION.**

There may be additional requirements as a result of conditions found during inspections.

Status: **PLAN REVIEW**

Notes:

OCCUPANCY PROHIBITED BEFORE APPROVAL.

The building or structure shall not be occupied prior to inspection and approval by the fire code official and issuance of a certificate of occupancy. (IFC 105.3.3) Stock and supplies shall not be moved in without approval of the fire department. Call the City of Kentwood Fire Prevention Bureau at 616/554-0797 at least 24 hours in advance to schedule inspections.

Status: **PLAN REVIEW**

Notes:

PLANS APPROVED AS SUBMITTED**PLANS APPROVED AS SUBMITTED.**

We have reviewed your plans for the above facility and approve them as submitted. Any changes must be reviewed and approved by the Kentwood Fire Prevention Bureau. All construction and processes must meet applicable codes and standards.

Status: **PLAN REVIEW**

Notes:

Additional Time Spent on Inspection:

Category	Start Date / Time	End Date / Time
Notes: No Additional time recorded		

Total Additional Time: 0 minutes

Inspection Time: 0 minutes

Total Time: 0 minutes

Summary:

Overall Result: Plan Review

Plan Review Completed.

Inspector Notes:

Inspector:

Name: QUICK, PATRICK A
Rank: FM
Work Phone(s): (616) 915-4265
Email(s): quickp@kentwood.us

Signature

Date