



# City of San Juan Bautista

*The “City of History”*

[www.san-juan-bautista.ca.us](http://www.san-juan-bautista.ca.us)

## AGENDA

### **REGULAR PLANNING COMMISSION MEETING**

CITY HALL COUNCIL CHAMBERS  
311 Second Street  
San Juan Bautista, California

**TUESDAY ~ FEBRUARY 5, 2019**

*In compliance with the American with Disabilities Act, if you need special assistance to attend or participate in the meeting, please call the City Clerk's Office at (831) 623-4661, extension 13 at least 48 hours prior to the meeting.*

*Any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda will be made available for public inspection at the meeting and in the City Clerk's office located at City Hall, 311 Second Street, San Juan Bautista, California during normal business hours.*

1. **Call to Order** **6:00 PM**  
**Pledge of Allegiance**  
**Roll Call**
2. **Public Comment**
3. **Informal Project Review**  
Any potential and/or future project applicant may present their project to the Commission during Informal Project Review for the purpose of gaining information as preliminary feedback only. No formal application is required and no action will be taken by the Commission on any item at this time.
4. **Consent Items**  
All matters listed under the Consent Agenda may be enacted by one motion authorizing actions indicated for those items so designated. There will be no separate discussion of these items unless requested by a member of the Commission, a staff member, or a citizen.
  - A. **Approve Affidavit of Posting Agenda**
  - B. **Approve Affidavit of Posting Public Hearing Notice**
  - C. **Approve Minutes for the November 13, 2018 Meeting**
  - D. **Approve Minutes for the December 4, 2018 Meeting**
5. **Action Items**
  - A. **Consider the proposed design and construction of new restrooms at Verutti Park at the corner of Second Street and San Jose Street and approve a resolution. This item is exempt from the California Environmental Quality Act (CEQA) per Sections 15300.2 and 15301.**
  - B. **Consider proposed new signs at Valero Gas Station located at 63 Muckelemei Street. (APN 002-320-002) Application is to install new signs and update logo on the existing signs. Applicant: Sal Pablo**
6. **Public Hearing Items**
  - A. **Consider a Site and Design Review for new construction for two modular homes and one accessory building located on Lasuen Drive (APN 002-540-010). Approval of the Site and Design Review is exempt from CEQA pursuant to Article 19, Sections 15300.2 and 15303. Applicants: Dave McGovert and Kevin McGovert**

- B. Consider a request for a Conditional Use Permit for the onsite sale of alcoholic beverages at Hacienda De Leal, 410 The Alameda (APN 002-520-013). Approval of the Conditional Use Permit is exempt from the CEQA pursuant to Article 19, Section 15300.2 and 15301. Applicants: Mark Medina and Frank Leal**

**7. Discussion Items**

- A. Discussion of the Roundabout for the Rancho Vista Subdivision**
- B. Placement of Wayfinding and Decorative Signage in San Juan Bautista**
- C. Orientation, Training and Education for Planning Commissioners**
- D. Report from Planner on current and upcoming projects**

**8. Comments**

- A. Planning Commissioners**
- B. City Planner**
- C. City Manager**

**9. Adjournment**

**AFFIDAVIT OF POSTING**

I, TRISH PAETZ, DO NOW DECLARE, UNDER THE PENALTIES OF PERJURY THAT I AM THE DEPUTY CITY CLERK IN THE CITY OF SAN JUAN BAUTISTA AND THAT I POSTED THREE (3) TRUE COPIES OF THE ATTACHED PLANNING COMMISSION AGENDA. I FURTHER DECLARE THAT I POSTED SAID AGENDA ON THE 30<sup>th</sup> DAY OF JANUARY 2019, AND I POSTED THEM IN THE FOLLOWING LOCATIONS IN SAID CITY OF SAN JUAN BAUTISTA, COUNTY OF SAN BENITO, CALIFORNIA.

1. ON THE BULLETIN BOARD AT CITY HALL, 311 SECOND STREET.
2. ON THE BULLETIN BOARD AT THE CITY LIBRARY, 801 SECOND STREET.
3. ON THE BULLETIN BOARD AT THE ENTRANCE TO THE UNITED STATES POST OFFICE, 301 THE ALAMEDA

SIGNED AT SAN JUAN BAUTISTA, COUNTY OF SAN BENITO, CALIFORNIA,  
ON THE 30<sup>th</sup> DAY OF JANUARY 2019.



TRISH PAETZ, DEPUTY CITY CLERK

### AFFIDAVIT OF POSTING

I, TRISH PAETZ, DO NOW DECLARE, UNDER THE PENALTIES OF PERJURY THAT I AM THE DEPUTY CITY CLERK IN THE CITY OF SAN JUAN BAUTISTA AND THAT I POSTED THREE (3) TRUE COPIES OF THE ATTACHED PLANNING COMMISSION PUBLIC HEARING NOTICE. I FURTHER DECLARE THAT I POSTED SAID NOTICE ON THE 25<sup>th</sup> DAY OF JANUARY 2019, AND I POSTED THEM IN THE FOLLOWING LOCATIONS IN SAID CITY OF SAN JUAN BAUTISTA, COUNTY OF SAN BENITO, CALIFORNIA.

1. ON THE BULLETIN BOARD AT CITY HALL, 311 SECOND STREET.
2. ON THE BULLETIN BOARD AT THE CITY LIBRARY, 801 SECOND STREET.
3. ON THE BULLETIN BOARD AT THE ENTRANCE TO THE UNITED STATES POST OFFICE, 301 THE ALAMEDA

SIGNED AT SAN JUAN BAUTISTA, COUNTY OF SAN BENITO, CALIFORNIA,  
ON THE 25<sup>TH</sup> DAY OF JANUARY 2019.



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TRISH PAETZ, DEPUTY CITY CLERK



## **NOTICE OF PUBLIC HEARING CITY OF SAN JUAN BAUTISTA**

Pursuant to Government Code Section 65090, the Planning Commission of the City of San Juan Bautista gives notice of a public hearing on **February 5, 2019** at 6:00 p.m. in the Council Chambers at San Juan Bautista City Hall, 311 Second Street. During the public hearing, the following items will be discussed:

- Consider a Site and Design Review for new construction for two modular homes and one accessory building located on Lasuen Drive (APN 002-540-010) - Approval of the Site and Design Review is exempt from the California Environmental Quality Act pursuant to Article 19, Sections 15300.2 and 15303 of the California Environmental Quality Act. Applicants: Dave and Kevin McGovern
- Consider a request for a Conditional Use Permit for the onsite sale of Alcoholic Beverages located at Hacienda De Leal located at 410 The Alameda (APN 002-520-013). There is no further development proposed onsite. Approval of the Conditional Use Permit Request is exempt from the California Environmental Quality Act pursuant to Article 19, Section 15300.2 and 15301 of the California Environmental Quality Act. Applicants: Mark Medina and Frank Leal

Staff reports and the full text of all items to be discussed will be available for public review at City Hall on **January 29, 2019**. All members of the public are encouraged to attend the meeting and may address the Planning Commission on the issue during the public hearing. Written comments may be hand delivered or mailed to City Hall (311 Second Street, P.O. Box 1420, San Juan Bautista, CA 95045), or e-mailed to [cityplanning@san-juan-bautista.ca.us](mailto:cityplanning@san-juan-bautista.ca.us), not later than **5:00 p.m., February 4, 2019**.

If a challenge is made on the action of the proposed project, pursuant to Government Code Section 65009 court testimony may be limited to only those issues raised at the public hearing described in this notice or in written correspondence delivered to the City at or prior to the public hearing.

Posted: January 25, 2019

**CITY OF SAN JUAN BAUTISTA  
PLANNING COMMISSION REGULAR MEETING  
NOVEMBER 13, 2018  
DRAFT MINUTES**

**1. CALL TO ORDER** – Chairperson Boyd called the meeting to order at 6:48 p.m.

**B. ROLL CALL**

**Present:** Commissioners Franco, Boyd, Medeiros and Brewer

**Absent:** Commissioner Freels

**Staff Present:** City Manager LaForge, Associate Planner Kennedy, City Attorney Mall and Administrative Services Manager Paetz

**2. PUBLIC COMMENT**

No comments were received.

**3. INFORMAL PROJECT REVIEW**

Nothing presented.

**4. CONSENT ITEMS**

- A. Approve Affidavit of Posting Agenda**
- B. Approve Minutes of August 7, 2018 Meeting**
- D. Approve Minutes of October 2, 2018 Meeting**

A motion was made by Commissioner Franco and seconded by Commissioner Medeiros to approve all items on the consent agenda. The motion passed unanimously, 4-0-0-1, with Commissioner Freels absent.

**5. ACTION ITEMS**

- A. Review and Consider Action on Harvey's Lockup located at 27 Monterey Street. Consider Resolution to approve or deny action.**

Associate Planner Kennedy presented a timeline for Harvey's Lockup, and distributed a letter from Precision as well. City Manager LaForge commented that staff was blindsided when Harvey's Lockup was brought up to Council. Steve Ekman, contractor for Harvey's lockup, reported on the status of compliance projects, and distributed two more handouts. Commissioner Franco asked if a performance bond was appropriate. Mr. Ekman felt they are so far into the compliance projects that a bond would not be necessary. Sprinkler system is additional, \$120k. City Attorney Mall advised that cities are immune from fire destruction lawsuits. Commissioner Medeiros commented he was satisfied with the process as explained. No action taken.

Whereupon, City Manager LaForge left the meeting.

## **6. DISCUSSION ITEMS**

### **A. Proposed residential development on a large parcel located on Lausen Drive (APN 002-50-010). Property owners are Dave and Kevin McGovert.**

Associate Planner Kennedy presented a proposed project. The applicant was present. A question and answer period followed with the applicant. The applicant's engineer, Matt Kelley, explained the project, and the site which involves a hillside. During public comment, Jolene Cosio objected to a cargo trailer left on the property, and asked if a sidewalk would be installed along the split rail fence. Wanda Guibert was concerned with whether the applicants are complying with the city's hillside ordinance. Kevin McGovert explains that the shed is for materials for the applicants' construction company.

### **B. Report from Planner on current and upcoming projects.**

Associate Planner Kennedy explained specific projects on the report and responded to questions posed by commissioners.

## **7. COMMENTS & REPORTS**

### **A. Planning Commissioners**

There was a request for review of board policies on the next agenda.

### **B. City Manager**

No comments were received.

### **C. Associate Planner**

No comments were received.

## **8. ADJOURNMENT**

The meeting adjourned at 8:22 p.m.

**CITY OF SAN JUAN BAUTISTA  
PLANNING COMMISSION REGULAR MEETING  
DECEMBER 4, 2018  
DRAFT MINUTES**

**1. CALL TO ORDER** – Chairperson Boyd called the meeting to order at 7:05 p.m.

**B. ROLL CALL**

**Present:** Commissioners Franco, Boyd, and Freels

**Absent:** Commissioners Medeiros and Brewer

**Staff Present:** Associate Planner Kennedy and  
Administrative Services Manager Paetz

**2. PUBLIC COMMENT**

No comments were received.

**3. INFORMAL PROJECT REVIEW**

Nothing presented.

**4. ACTION ITEMS**

**A. Approve Affidavit of Posting Agenda**

A motion was made by Commissioner Freels and seconded by Commissioner Franco to approve the affidavit of posting agenda. The motion passed unanimously, 3-0-0-2, with Commissioners Medeiros and Brewer absent.

**B. Consider a recommendation from the Historic Resources Board for a restaurant at 404 Third Street (APN 002-140-005) and expansion of an existing residential unit located at 406 Third Street. The applicant is Anne Nguyen. Approve Resolution 2018-12 approving a minor alteration for new living space within an existing residential unit and a tenant improvement for a new restaurant located at 404 and 406 Third Street**

Associate Planner Kennedy presented the project. The applicant expressed concern with whether exterior painting would hold up the process. Associate Planner Kennedy suggested the trim color be handled when signage is proposed. During public comment, Wanda Guibert commented that the applicant should not have to paint a third color until she is ready to paint the building.

A motion was made by Commissioner Franco and seconded by Commissioner Freels to approve Resolution 2018-12 approving a minor alteration for new living space within an existing residential unit and a tenant improvement for a new restaurant located at 404

and 406 Third Street. The motion passed unanimously, 3-0-0-2, with Commissioners Medeiros and Brewer absent.

**C. Consider an Amendment to the City Design Guidelines by Adding a New Chapter: Streetscape Design Guidelines**

Associate Planner Kennedy presented the item. During public comment, Wanda Guibert explained the reason for the change to the guidelines.

A motion was made by Commissioner Freels and seconded by Commissioner Franco to approve Resolution 2018-13 asking the City Council to approve an amendment to the Streetscape Design Guidelines. The motion passed unanimously, 3-0-0-2, with Commissioners Medeiros and Brewer absent.

**5. DISCUSSION ITEMS**

**A. Proposed ABC License for Hacienda de Leal located at 410 The Alameda**

Associate Planner Kennedy provided information about a proposed ABC license. No action was taken. There was no public comment.

**B. Report from Planner on current and upcoming projects.**

Associate Planner Kennedy explained specific projects on the report and responded to questions posed by commissioners.

**C. Confirm rescheduling of January 2019 Regular Planning Commission Meeting**

The Regular Planning Commission meeting for January 2019 falls on January 1, which is a City holiday. Commissioners agreed to reschedule the meeting to January 8, 2019.

**D. Future appointments to the Historic Resources Board and Planning Commission. Applications are being accepted at this time.**

Associate Planner Kennedy provided this as informational only.

**6. COMMENTS & REPORTS**

**A. Planning Commissioners**

Commissioner Franco submitted his formal resignation. Chairperson Boyd questioned why the City Attorney and City Manager were not present at tonight's meeting.

**B. City Manager**

Not present.

**C. Associate Planner**

Associate Planner Kennedy expressed thanks and appreciation to retiring Commissioner Franco.

**7. ADJOURNMENT**

The meeting adjourned at 8:08 p.m.

## **CITY OF SAN JUAN BAUTISTA**

### **PLANNING COMMISSION**

#### **STAFF REPORT**

**Date:** February 5, 2019  
**To:** Planning Commission  
**From:** Todd Kennedy, Associate Planner  
**Project:** Construction of Restrooms at Verutti Park

#### **BACKGROUND:**

This item is being proposed by City Staff in order to construct new restrooms at Verutti Park. Verutti Park is located on the corner of San Jose Street and Second Street across from the Public Works Yard. The restrooms are proposed to be constructed with a building design and materials that meet the design guidelines.

This item was presented to the Planning Commission on January 8, 2019 and again on January 14, 2019. At those hearings feedback was provided from the Commissioners and members of the public. That feedback included to revise the plans to install windows, provide landscaping planters, and provide adequate lighting. Those features have been included in the latest drawings. Flush mounted lighting on the building has been included as well as an individual light post near the building. The design can either be similar to the existing street lights in town or a rustic design. Samples have been included.

Existing infrastructure including water and sewer access is already onsite and is ready to be utilized. There is no need for installing any new infrastructure under the street. This proposal is exempt from the California Environmental Quality Act (CEQA) per Sections 15300.2 and 15301.

#### **ACTION:**

Staff recommends the Planning Commission approve the proposed restrooms as proposed.

#### **ATTACHMENTS:**

Draft Resolution  
Revised Project Plans  
Site Photos  
Light Post Design Samples

## RESOLUTION 2019 - xx

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SAN JUAN BAUTISTA APPROVING A MINOR SITE AND DESIGN REVIEW FOR THE CONSTRUCTION OF NEW RESTROOMS LOCATED AT VERUTTI PARK LOCATED ON SECOND STREET BETWEEN JEFFERSON STREET AND SAN JOSE STREET IN SAN JUAN BAUTISTA. APN 002-110-001**

**WHEREAS**, the Planning Commission conducted public hearings on January 8, 2019 and January 14, 2019 to review the staff report and material, and to provide feedback to City Staff, and

**WHEREAS**, the Planning Commission has reviewed the information provided from City Staff, and

**WHEREAS**, the Planning Commission reviewed the staff report for the project together with findings that the proposed Minor Site and Design Review is categorically exempt from CEQA (California Environmental Quality Act) review per Sections 15300.2 and 15301, and

**WHEREAS**, the Planning Commission has considered the information and makes the following finding to approve the Minor Site and Design Review request.

1. That the proposed Minor Site and Design Review is compliant with the Design Guidelines, has no impact on the site and surrounding uses, and will provide a benefit to park visitors.

**NOW, THEREFORE, BE IT RESOLVED, that** the Planning Commission of the City of San Juan Bautista hereby approves a Minor Site and Design Review for new restrooms located at Verutti Park located on Second Street between San Jose Street and Jefferson Street in San Juan Bautista.

**PASSED AND ADOPTED** by the Planning Commission of the City of San Juan Bautista at a regular meeting held on February 5, 2019 by the following vote:

**AYES:**

**NOES:**

**ABSENT:**

**ABSTAIN:**

ATTEST:

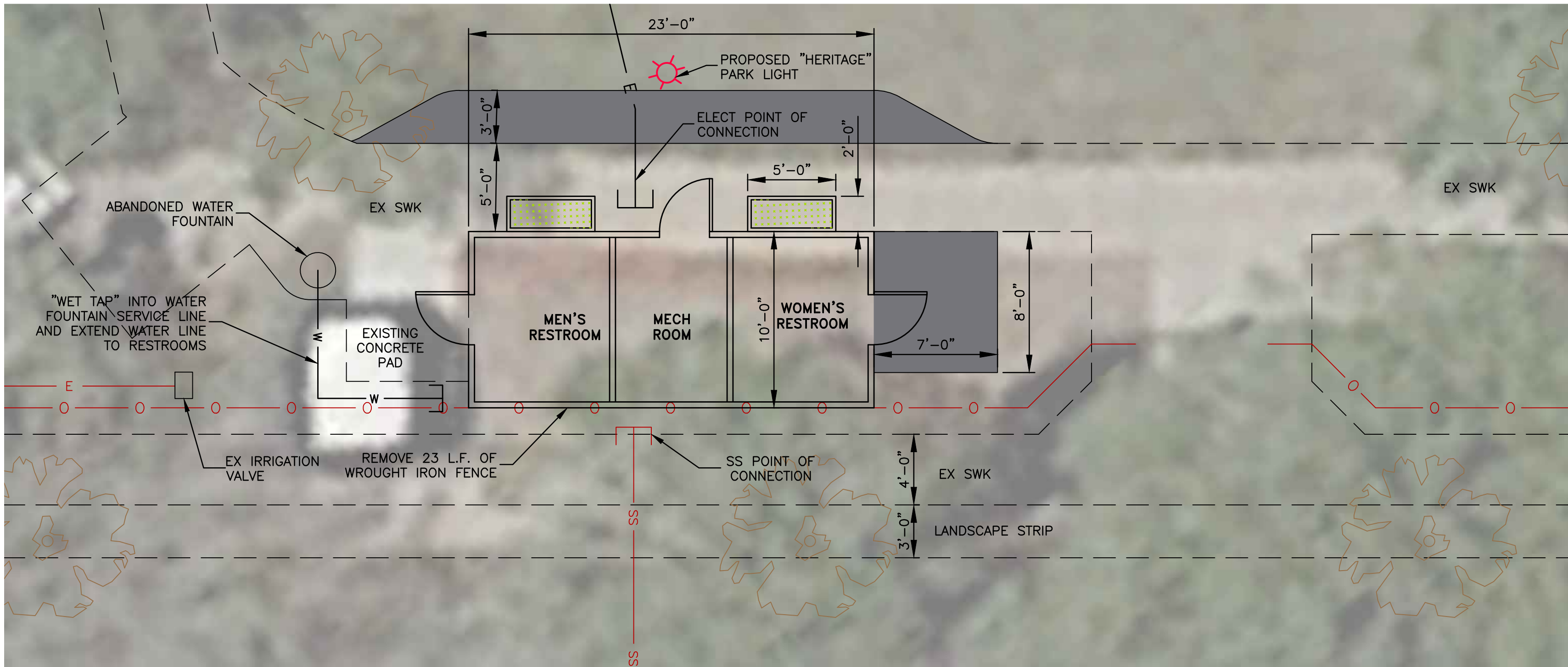
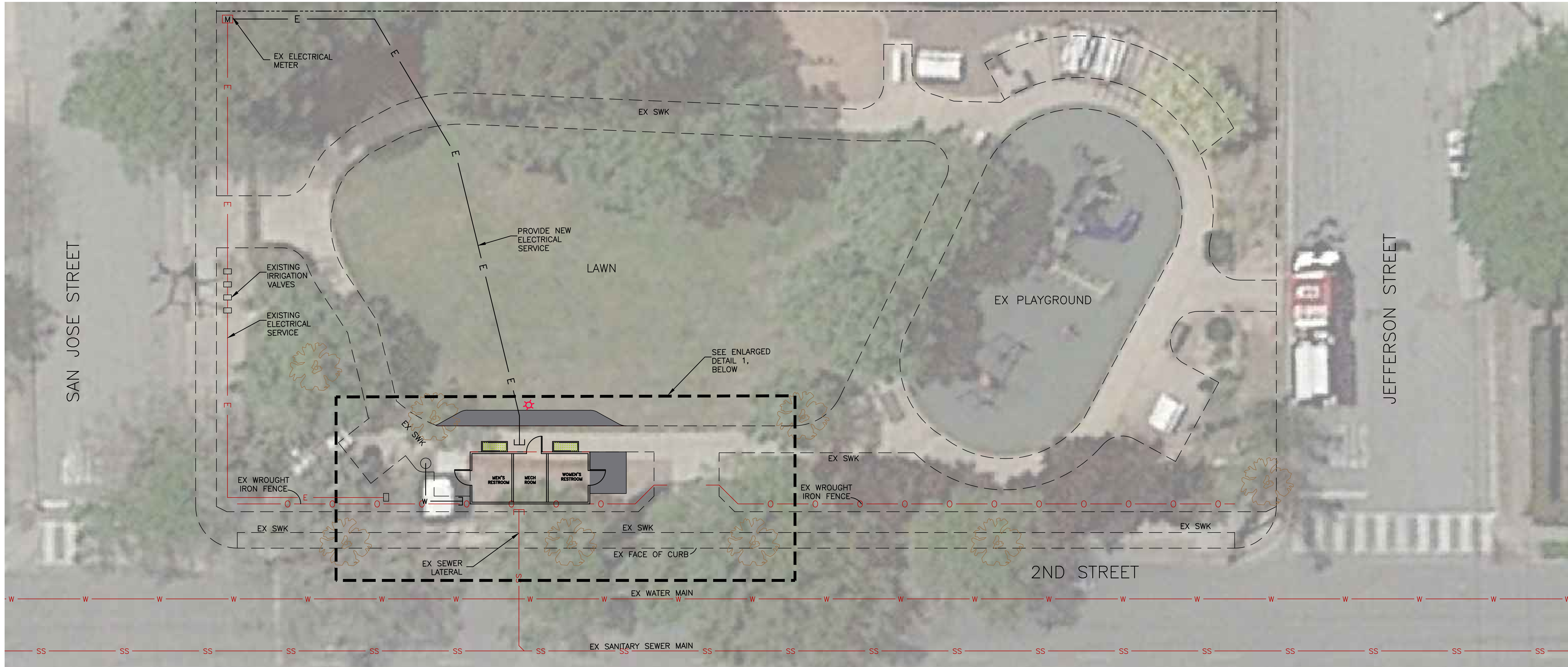
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Scott Freels, Chairman

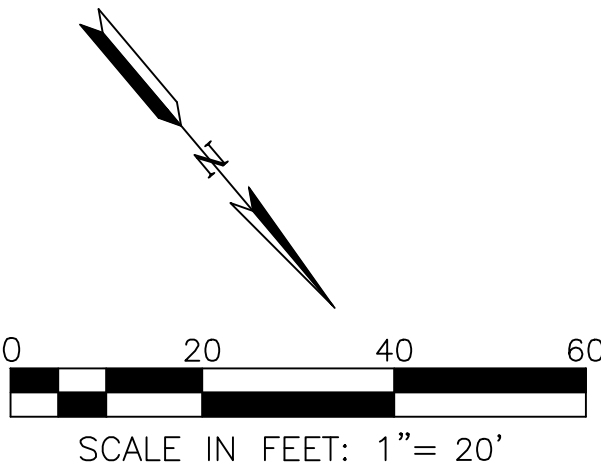
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Trish Paetz, Deputy City Clerk





- LEGEND**
- CONCRETE SIDEWALK (150± S.F.)
  - PROPOSED PLANTER BOX
  - PROPOSED PARK LIGHT
  - EX SIDEWALK
  - EX WROUGHT IRON FENCE
  - EX SEWER MAIN
  - EX SEWER MAIN
  - EXISTING TREE



1 ENLARGED DETAIL  
1"=5'

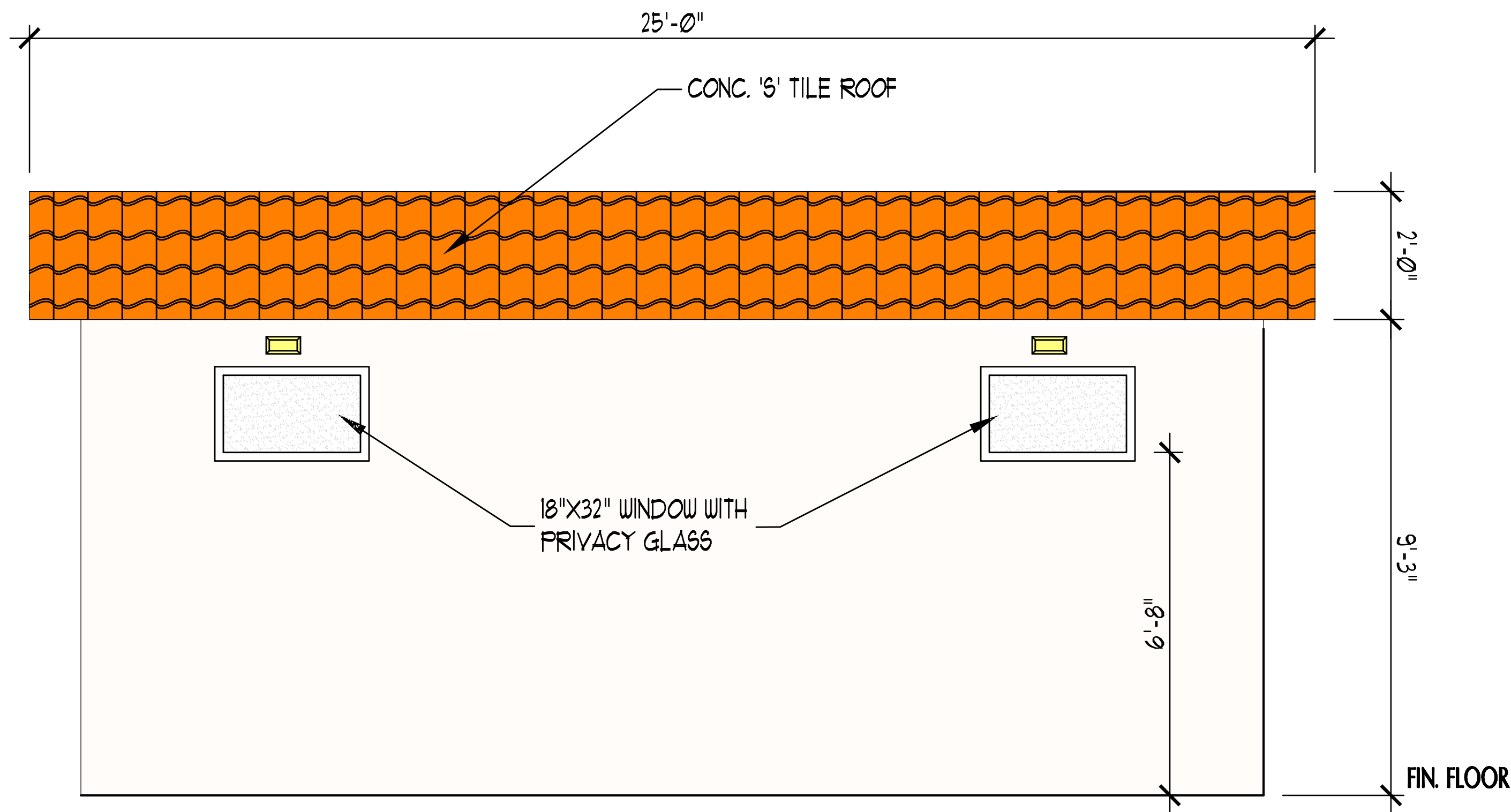
2 "HERITAGE PARK LIGHT  
SAMPLE"  
N.T.S.

SITE PLAN

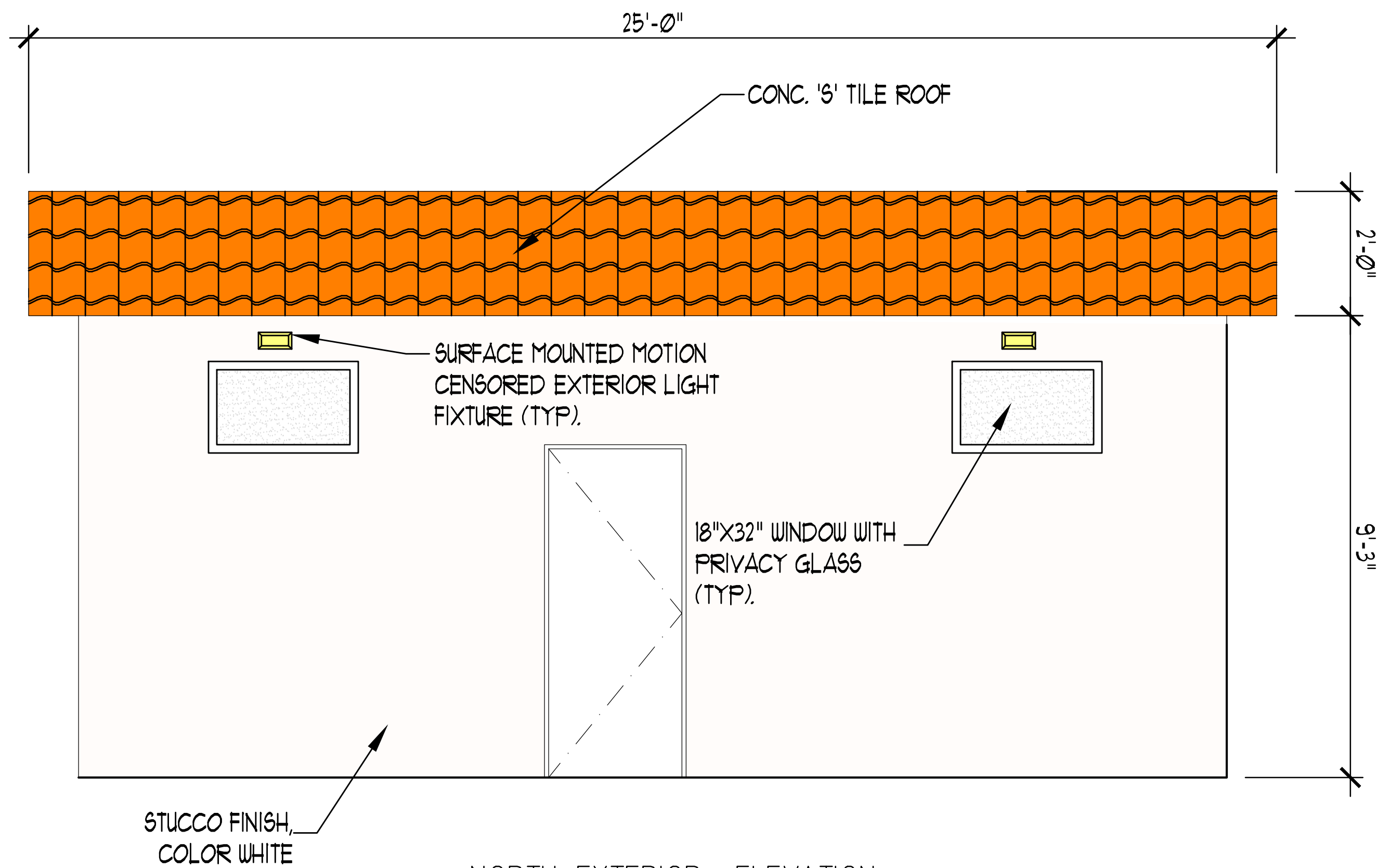
CITY OF  
SAN JUAN BAUTISTA  
VERUTTI PARK  
RESTROOM

DESIGNED BY	
DRAWN BY	
CHECKED BY	
DATE ISSUED	JAN, 2019
JOB NO.	
DWG NO.	C-1
SHEET	1 OF 4

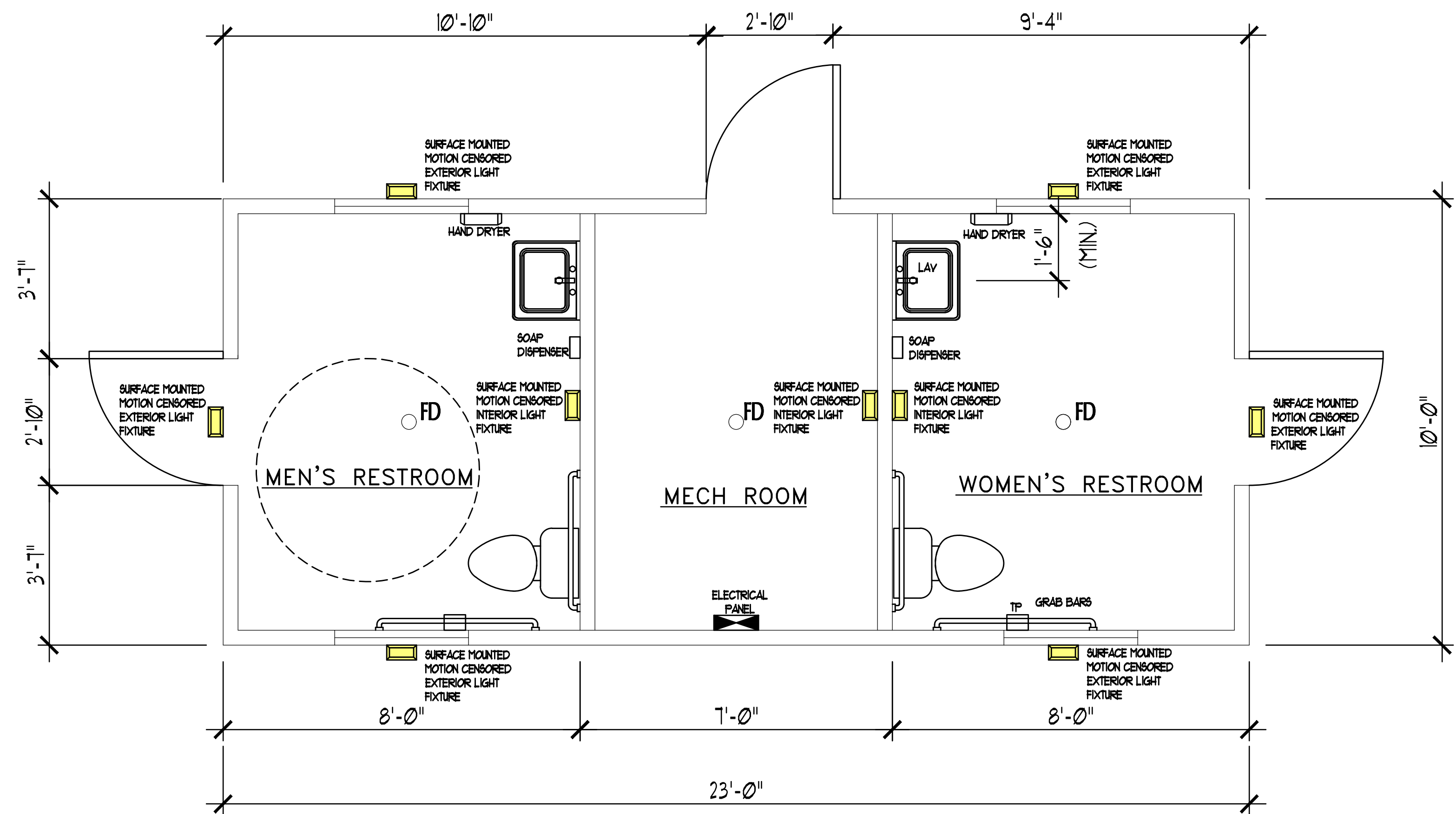




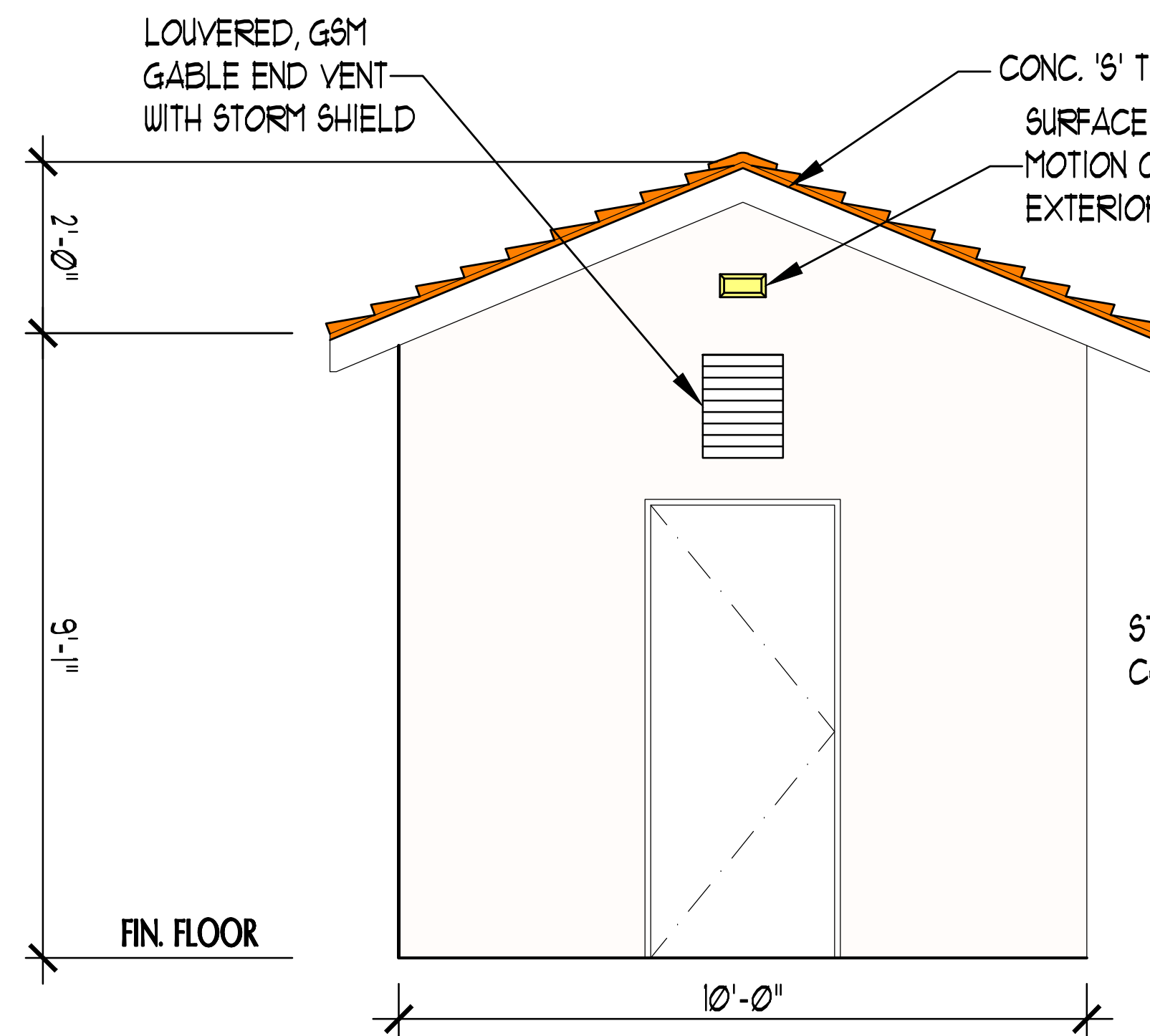
SOUTH EXTERIOR ELEVATION  
1/2"=1'-0"



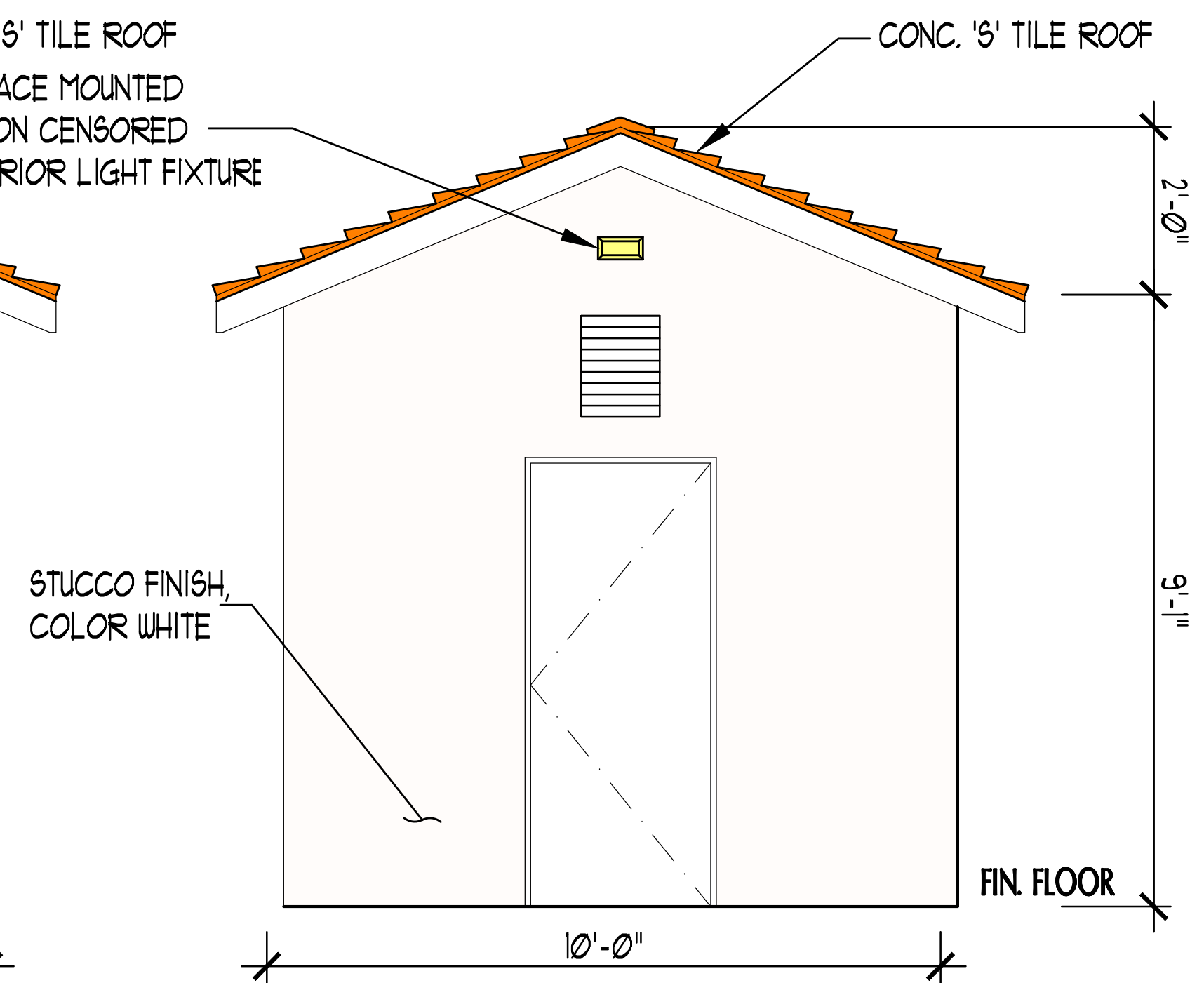
NORTH EXTERIOR ELEVATION  
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FLOOR PLAN  
1/2"=1'-0"



WEST EXTERIOR ELEVATION  
1/2"=1'-0"

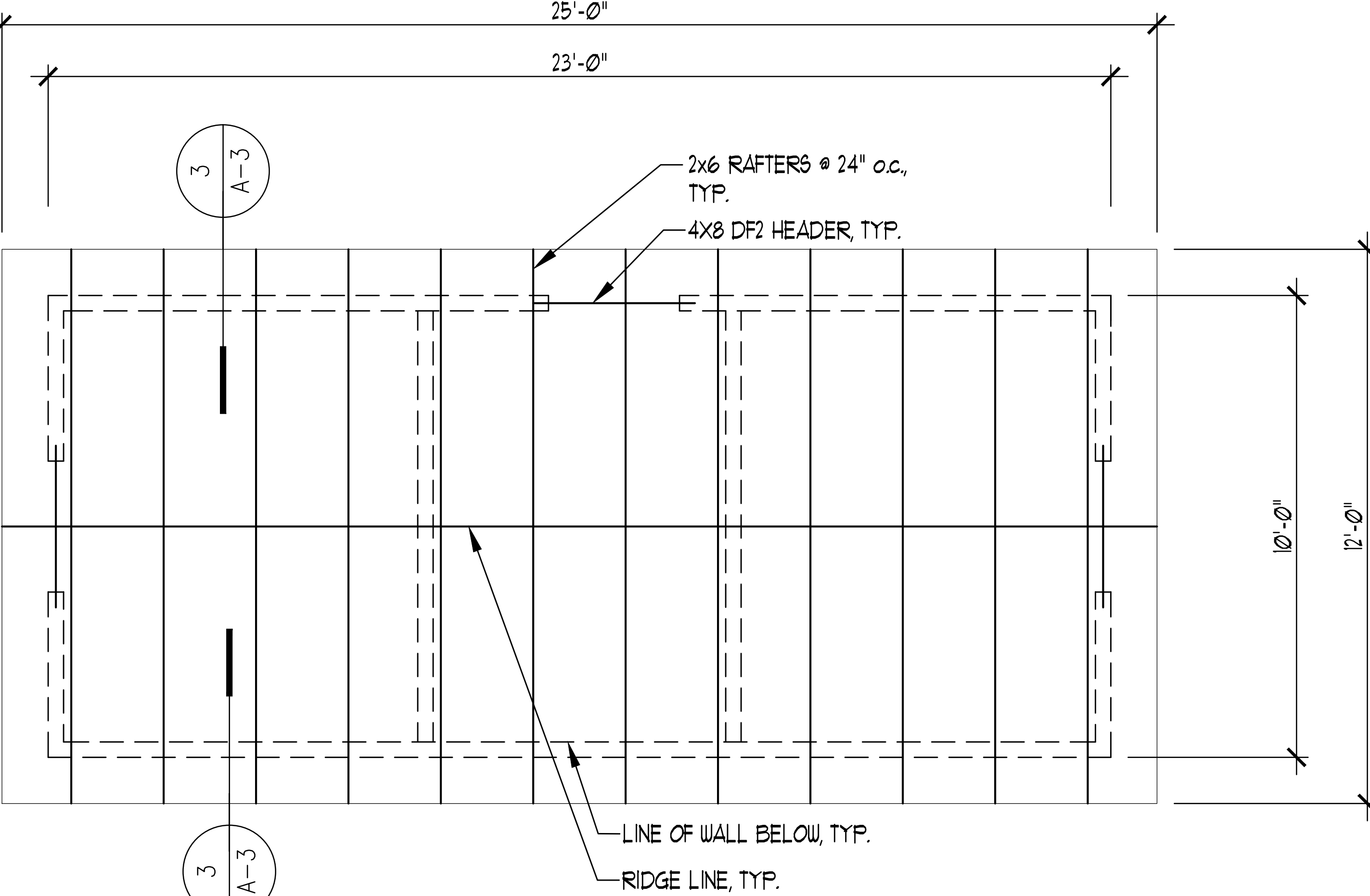


EAST EXTERIOR ELEVATION  
1/2"=1'-0"

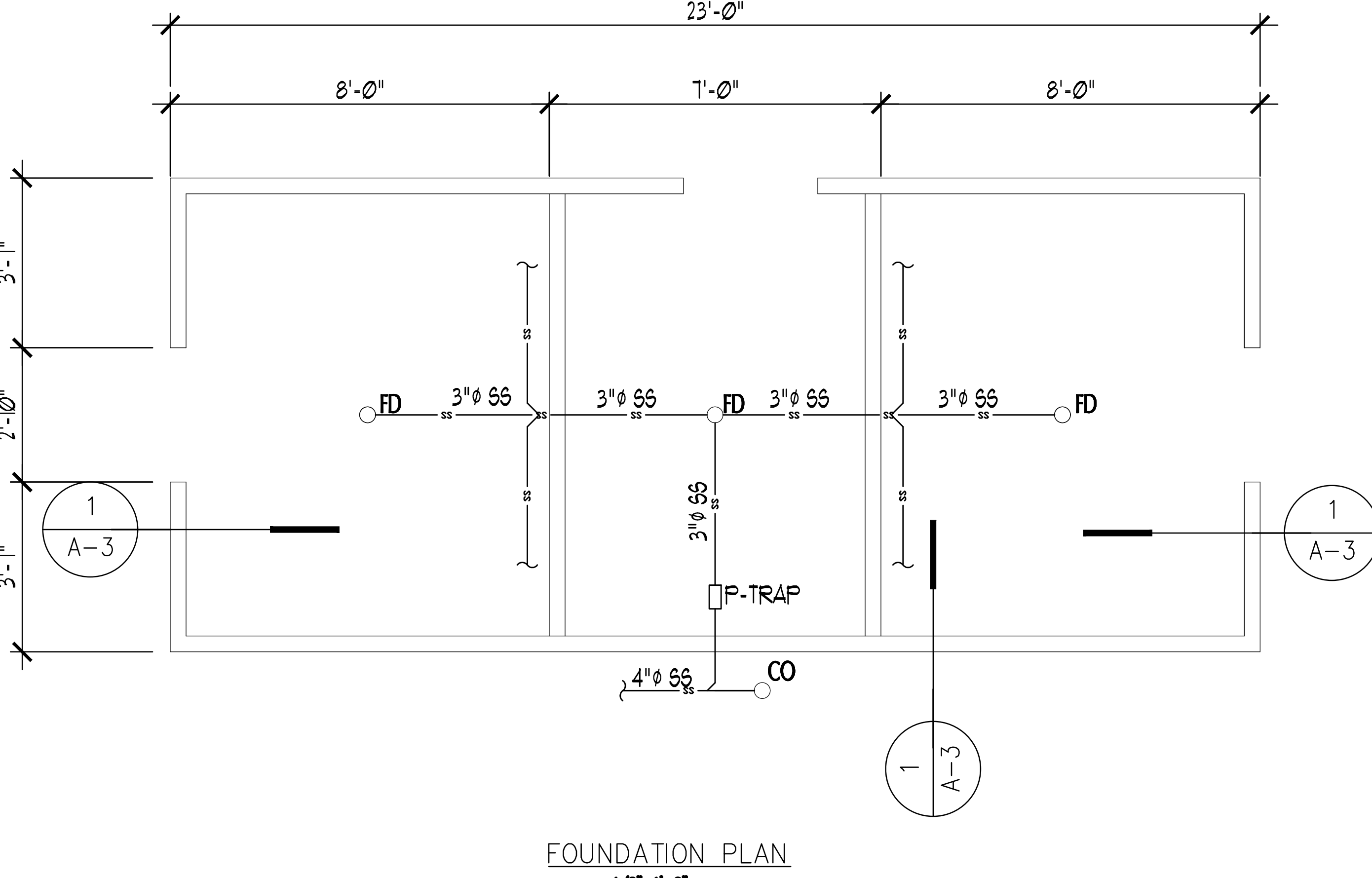
FLOOR PLAN AND  
ELEVATIONS

CITY OF  
SAN JUAN BAUTISTA  
VERUTTI PARK  
RESTROOM

DESIGNED BY	
DRAWN BY	
CHECKED BY	
DATE ISSUED	JAN, 2019
JOB NO.	
DWG. NO.	A-1
SHEET	2 OF 4



ROOF FRAMING PLAN



## FOUNDATION PLAN

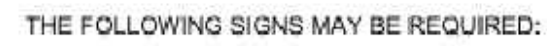
NO.	BY	DATE	REVISION

# ROOF FRAMING AND FOUNDATION PLAN

**CITY OF  
SAN JUAN BAUTISTA  
VERUTTI PARK  
RESTROOM**

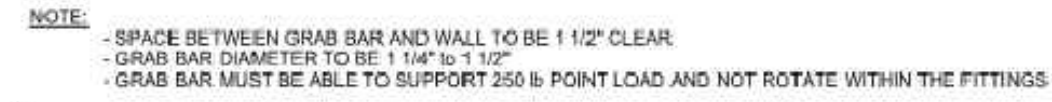
DESIGNED BY \_\_\_\_\_  
DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_  
DATE ISSUED  
JAN, 2019  
JOB NO. \_\_\_\_\_  
DWG NO. \_\_\_\_\_  
A-2  
SHEET 3 OF 4





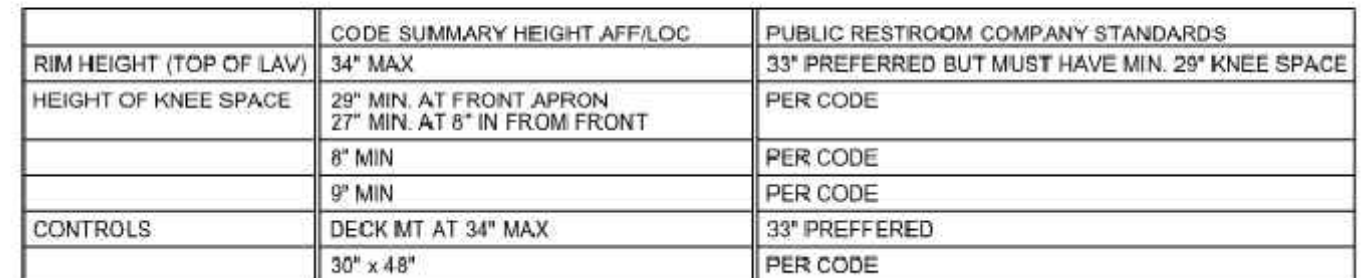
- DSA/AC CHECKLIST 17.5.9 & 17.5.10  
DSA 1127A.7 CBC 1115B.6

RESTROOM SIGNS AT DOORS (TYPICAL)  
**NO SCALE**



	CODE SUMMARY HEIGHT AFFLOC	PUBLIC RESTROOM COMPANY STANDARDS
GRAB BARS	33" MIN / 36" MAX. TO TOP	34" TO TOP
TOILET/SEAT	17" MIN / 19" MAX	17" TO TOP OF SEAT
TOILET FLUSH VALVE	40" MAX., 10" TO WIDE SIDE	CENTERLINES TO BE 28" AFF AND 10" FROM CENTER OF WC
TP DISPENSER	DISPENSER OUTLET TO BE 19" MIN. AFF & 12" MAX. FROM FRONT OF WC. TO CTR OF FURTHEST ROLL	9" FROM FRONT OF TOILET TO CENTER OF FURTHEST TOILET PAPER ROLL AND 30" TO TOP.

TOILET DESIGN CRITERIA (TYPICAL)  
**NO SCALE**



LAVATORY DESIGN CRITERIA  
**NO SCALE**



## DETAILS

**CITY OF  
SAN JUAN BAUTISTA  
VERUTTI PARK  
RESTROOM**

DESIGNED BY

RAWN BY

HECKED BY

DATE ISSUED  
JAN, 2019

JOB NO.

WG NO.

A-3

HEET 4 OF 4



















# **CITY OF SAN JUAN BAUTISTA**

## **PLANNING COMMISSION**

### **STAFF REPORT**

**Date:** February 5, 2019

**From:** Todd Kennedy, Associate Planner

**Project:** New Signage at Valero Gas Station located at 63 Muckelemei Street

#### **BACKGROUND:**

This item is being presented as a Sign Review for new updated signage to replace the existing signage located at the Valero Gas Station. This change is taking place because Valero is updating their logos and colors. These are changes in sizes taking place and there are no additional signs proposed. These changes are only changes in copy.

Furthermore, as part of this change, the monument sign is proposed to have digital price numbers installed. This is also part of the logo update from the information received by City Staff.

These changes appear to be reasonable and since Valero has been operating at the site for a many years, the sign package is part of the business. The existing sign package was also reviewed by the Planning Commission.

#### **ACTION:**

Staff recommends the Planning Commission approve the proposed sign changes. No issues are identified by staff.

#### **ATTACHMENTS:**

Draft Resolution  
Plans  
Sign images



## RESOLUTION 2019 - xx

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SAN JUAN BAUTISTA APPROVING A SIGN PERMIT FOR THE REPLACEMENT OF THE EXISTING SIGN PACKAGE LOCATED AT THE VALERO GAS STATION LOCATED AT 63 MUCKELEMI STREET IN SAN JUAN BAUTISTA. APN 002-320-002**

**WHEREAS**, the Planning Commission has considered the recommendation from City Staff and makes the following finding to approve the Sign Permit request.

1. That the proposed Sign Permit request is compatible with the existing gas station and does not have any impacts on the existing property and surrounding properties.

**NOW, THEREFORE, BE IT RESOLVED**, that the Planning Commission of the City of San Juan Bautista hereby approves a Sign Permit for the Valero Gas Station located at 63 Muckelemi Street in San Juan Bautista.

**PASSED AND ADOPTED** by the Planning Commission of the City of San Juan Bautista at a regular meeting held on February 5, 2019 by the following vote:

**AYES:**

**NOES:**

**ABSENT:**

**ABSTAIN:**

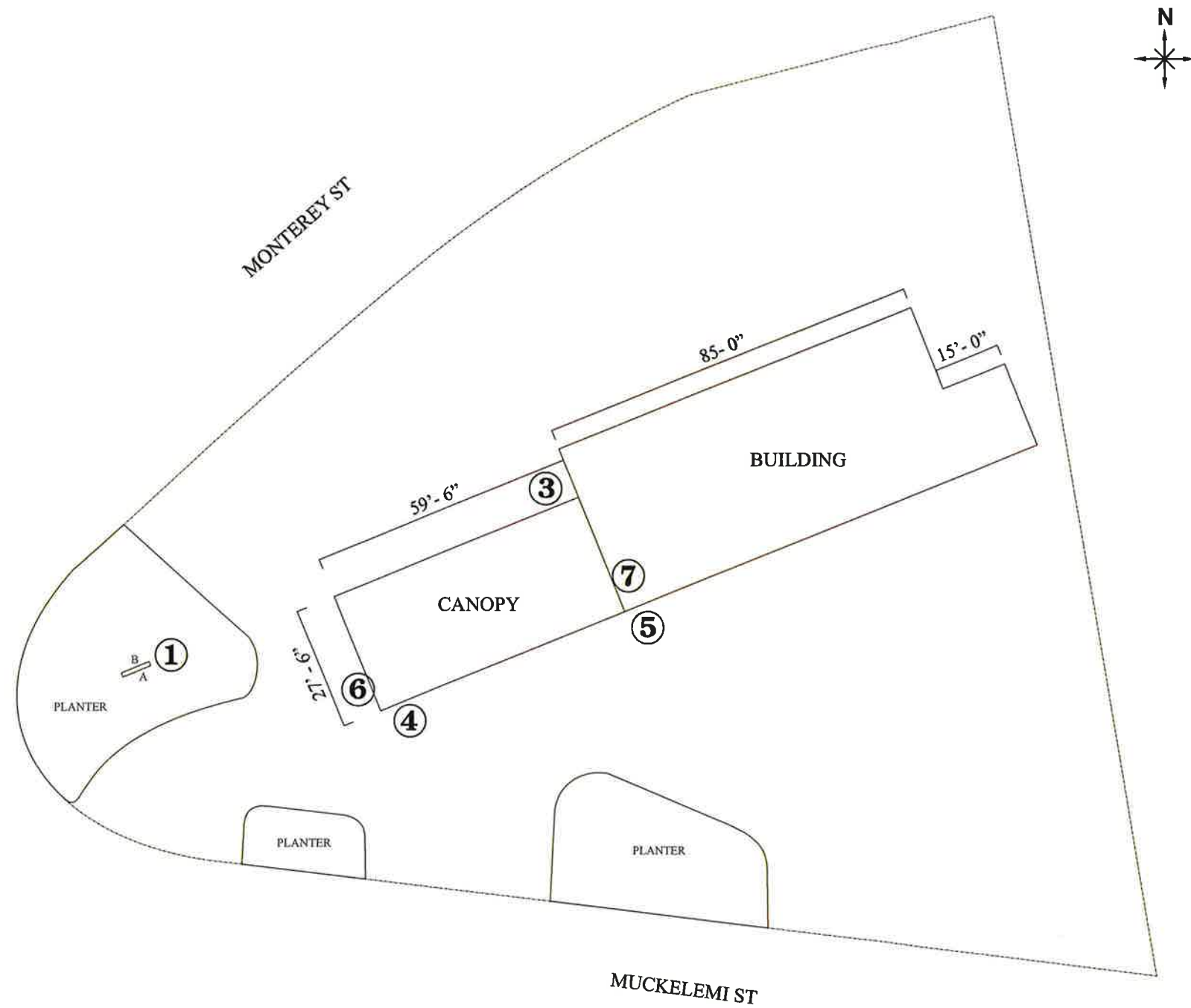
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Scott Freels, Chairperson

ATTEST:

---

Trish Paetz, Deputy City Clerk



EXISTING SITE

② — HI-RISE POLE SIGN IN  
CLOSE PROXIMITY TO  
HIGHWAY 156.

**NOTE:**  
THESE DRAWINGS MAY NOT  
BE THE FINAL COLOR SCHEME  
AND APPROVED LAYOUT

SITE PLAN:  
SCALE: 1/32" = 1'-0"



**Sign Development Inc.**

License #576277

Upland, CA 91786 (909) 920-5535

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**CUSTOMER APPROVAL**

SIGN AND PRINT FULL NAME

DATE

**NO.** U5890

**PAGE:** 1 OF 7

**DATE:** 12.18.18SM

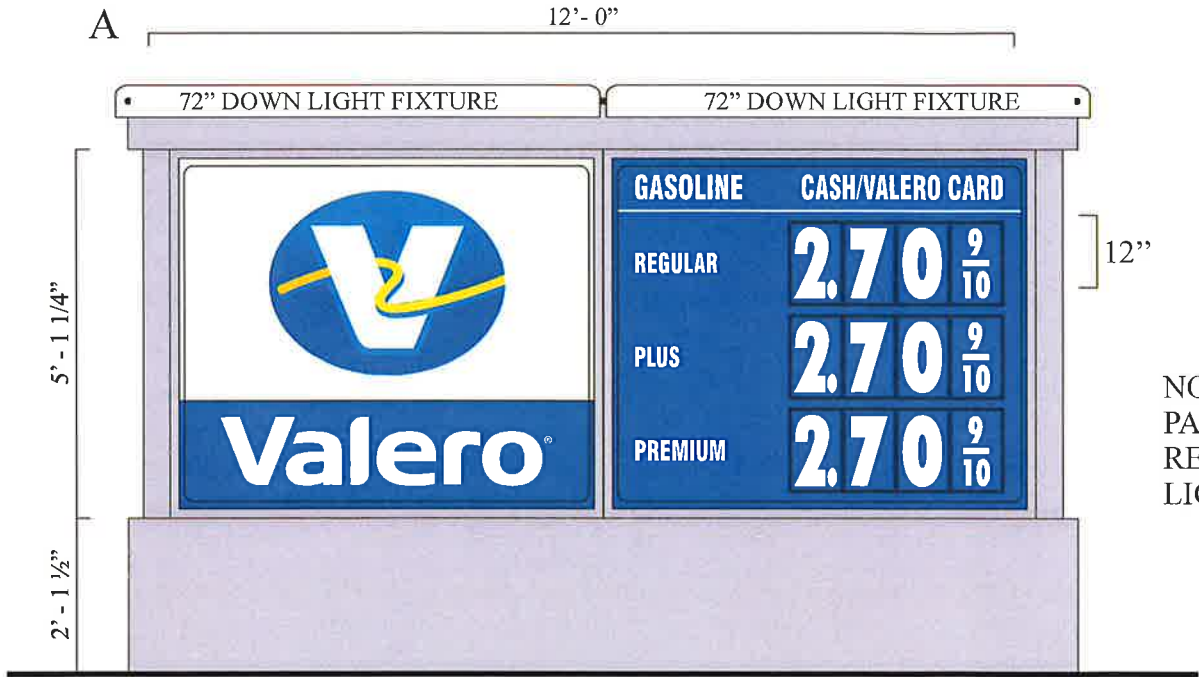
**REV:**

**Valero**

63 MUCKELEMI ST.  
SAN JUAN BAUTISTA, CA 95045



EXISTING D/F PRICE/ID SIGN  
48"



① REFACE EXISTING D/F PRICE/ID SIGN  
71.25 sq. ft.

SCALE: 3/8" = 1'-0"

NOTE:  
PAINT EXISTING CABINETS,  
RETAINERS, AND BASE VALERO  
LIGHT GRAY



PAD SIGN  
SCALE: 3/8" = 1'-0"

THERE MUST BE A PAD SIGN CLEARLY POSTING  
THE CREDIT/DEBIT PRICE AND LOCATED AT OR NEAR  
THE BASE OF THE MAIN ID WHERE IT IS CLEARLY  
VISIBLE TO THE CONSUMER

**NOTE:**  
**THESE DRAWINGS MAY NOT**  
**BE THE FINAL COLOR SCHEME**  
**AND APPROVED LAYOUT**



**Sign Development Inc.**

License #576277

Upland, CA 91786 (909) 920-5535

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**CUSTOMER APPROVAL**

SIGN AND PRINT FULL NAME

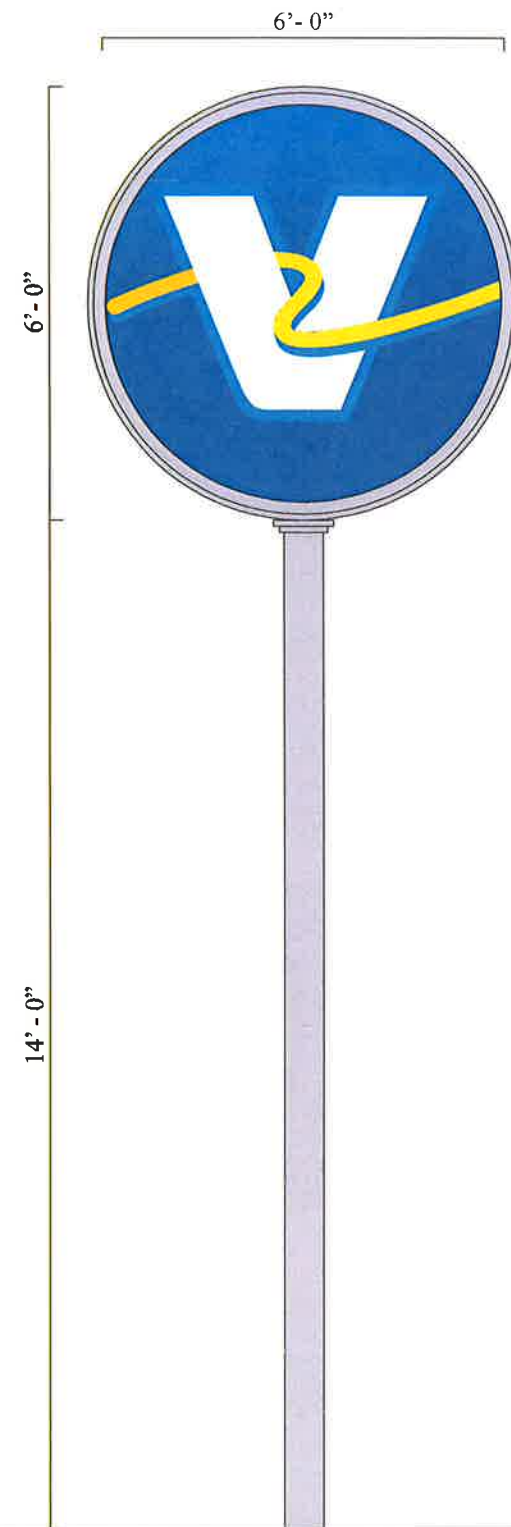
DATE

**NO.** U5890 **PAGE:** 2 OF 7  
**DATE:** 12.18.18SM  
**REV:**



63 MUCKELEMI ST.  
SAN JUAN BAUTISTA, CA 95045





NOTE:  
PAINT EXISTING CABINET,  
RETAINERS, AND POLE STRUCTURE  
VALERO LIGHT GRAY



EXISTING D/F HI-RISE SIGN

**NOTE:**  
THESE DRAWINGS MAY NOT  
BE THE FINAL COLOR SCHEME  
AND APPROVED LAYOUT

② REFACE EXISTING NON-ILLUMINATED D/F HI-RISE SIGN  
71.25 sq. ft. SCALE: 3/8" = 1'-0"



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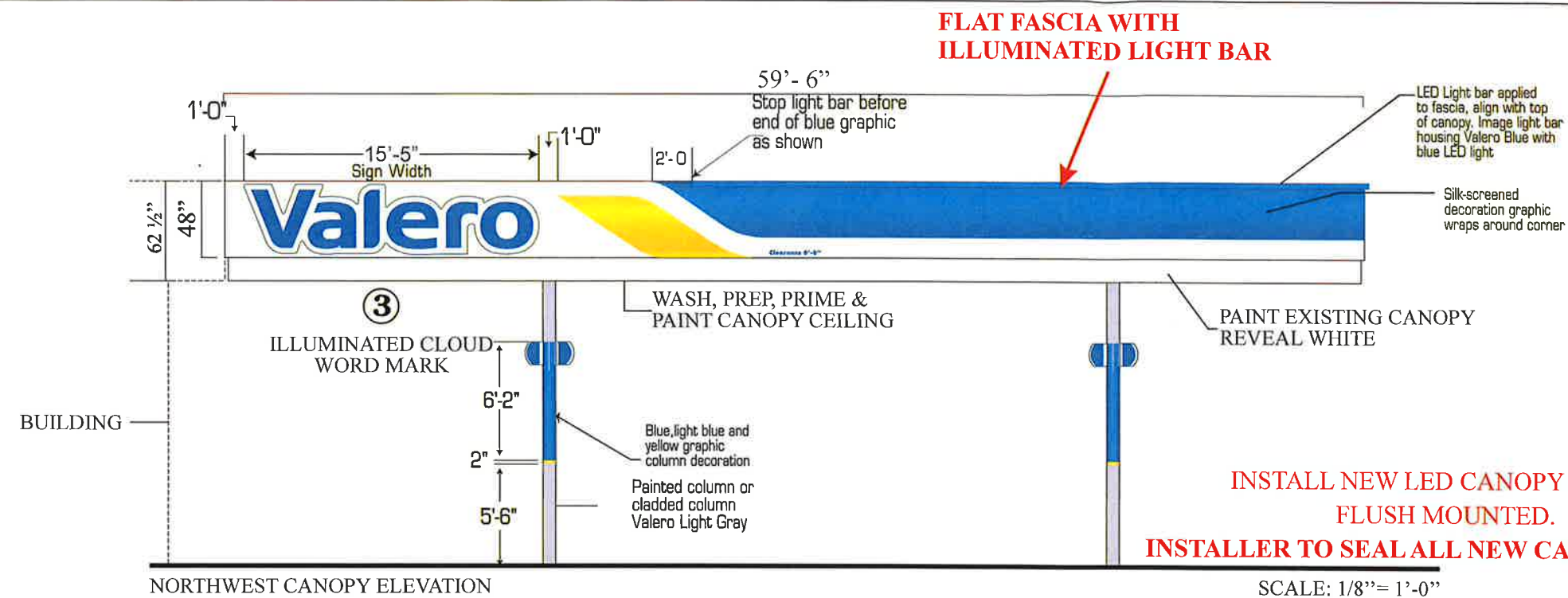
**NO.** U5890

**PAGE:** 3 OF 7

**DATE:** 12.18.18SM

**REV:**

**Valero** 63 MUCKELEMI ST.  
SAN JUAN BAUTISTA, CA 95045



NOTES:  
 REMOVE ALL ADS AND AD SIGNS FROM BUMPER POLES AND CANOPY COLUMNS.  
 PAINT ALL GUARD HOOPS AND BUMPER POLES VALERO BLUE.  
 PAINT CANOPY COLUMNS VALERO LIGHT GRAY.  
 INSTALL NEW VALERO TRASH CADDIES.

**REMOVE EXISTING ACM FASCIA BEFORE IMAGING**

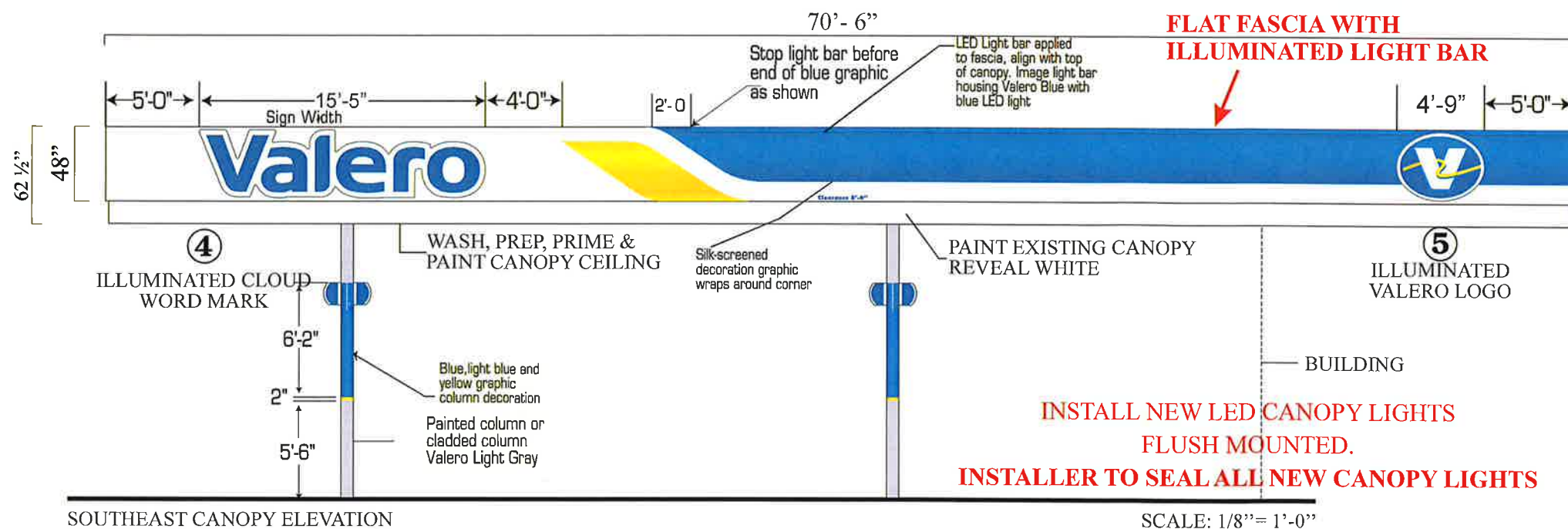


**INSTALL NEW LED CANOPY LIGHTS  
 FLUSH MOUNTED.  
 INSTALLER TO SEAL ALL NEW CANOPY LIGHTS**



SCALE: 1/4" = 1'-0"

**NOTE:  
 THESE DRAWINGS MAY NOT  
 BE THE FINAL COLOR SCHEME  
 AND APPROVED LAYOUT**



**INSTALL NEW LED CANOPY LIGHTS  
 FLUSH MOUNTED.  
 INSTALLER TO SEAL ALL NEW CANOPY LIGHTS**

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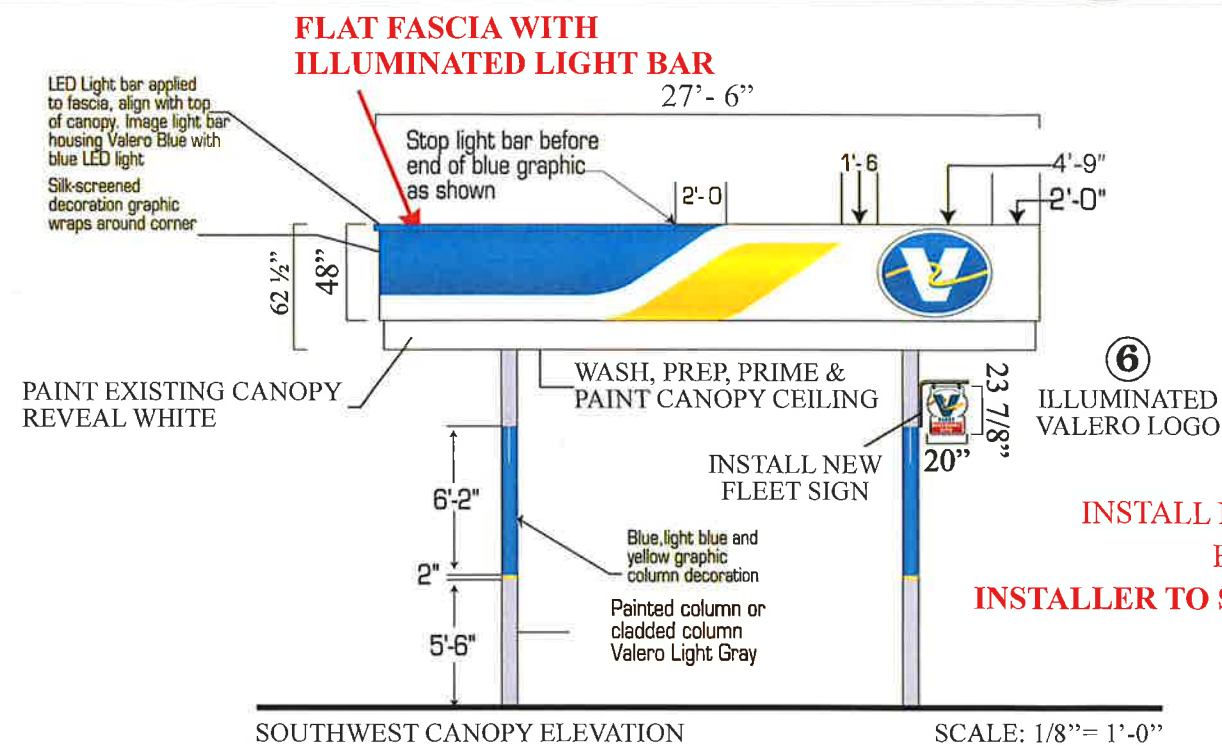
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**NO. U5890**  
**DATE: 12.18.18SM**  
**REV:**

**PAGE: 4 OF 7**

**Valero** 63 MUCKELEMI ST.  
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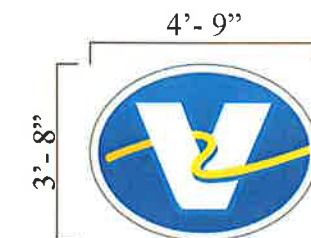
**INSTALL NEW LED CANOPY LIGHTS  
FLUSH MOUNTED.  
INSTALLER TO SEAL ALL NEW CANOPY LIGHTS**



EXISTING SOUTHWEST CANOPY ELEVATION

NOTES:  
REMOVE ALL ADS AND AD SIGNS FROM BUMPER POLES AND CANOPY COLUMNS.  
PAINT ALL GUARD HOOPS AND BUMPER POLES VALERO BLUE.  
PAINT CANOPY COLUMNS VALERO LIGHT GRAY.  
INSTALL NEW VALERO TRASH CADDIES.

**REMOVE EXISTING ACM  
FASCIA BEFORE IMAGING**



SCALE: 1/4" = 1'-0"

⑥ ILLUMINATED 44"  
VALERO LOGO

**NOTE:  
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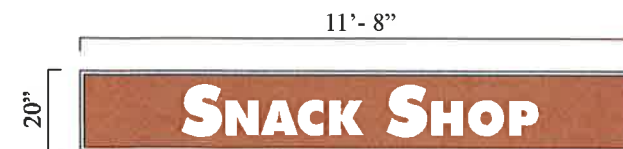
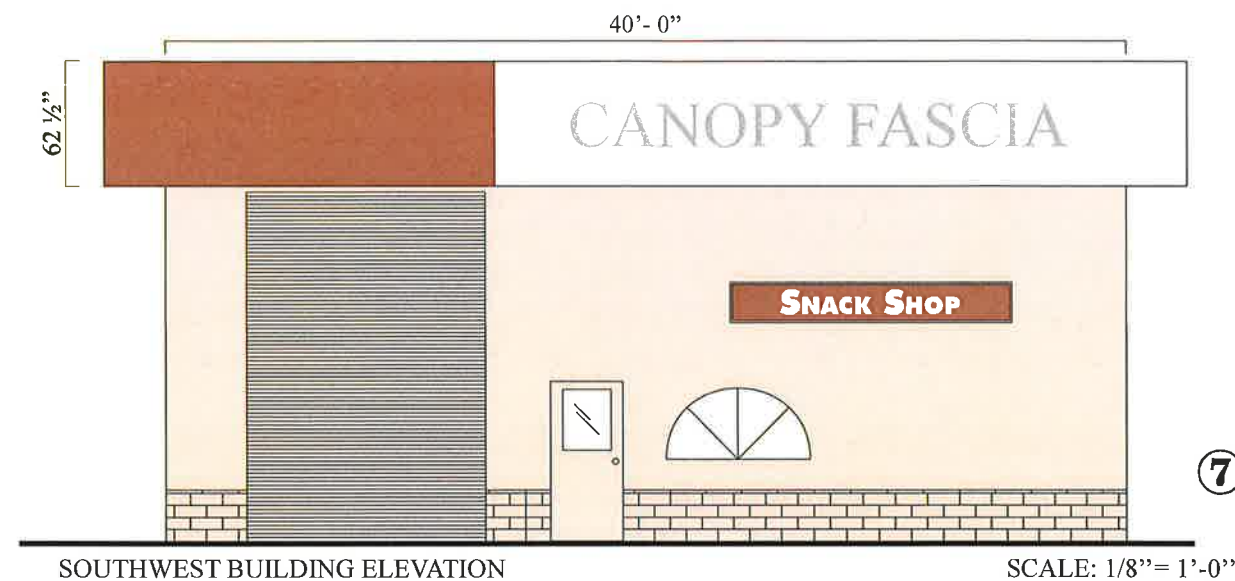
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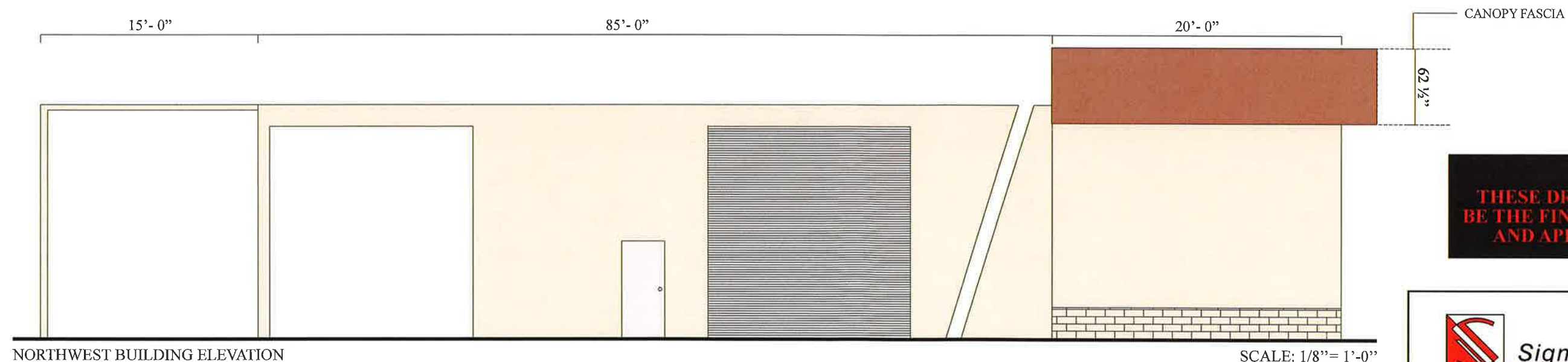


- ⑦ REFACE EXISTING S/F BUILDING SIGN,  
FLAT LEXAN FACES, 2nd SURFACE VINYL DECORATION,  
3M 3630-137 EUROPEAN BLUE



EXISTING SOUTHWEST BUILDING ELEVATION

NOTES:  
REMOVE ALL AD SIGNS AND BANNERS FROM BUILDING AND WINDOWS.  
PAINT BUILDING EXTERIOR VALERO SAND.  
PAINT EXISTING FASCIA SW 7718 OAK CREEK. **BUILDING PORTION ONLY.**  
REFACE EXISTING BUILDING SIGN.  
PAINT ALL BUMPER POLES VALERO BLUE.



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**PAGE:** 6 OF 7

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PAINT ALL BUMPER POLES VALERO BLUE.  
NO TOUCH EXISTING PROPANE TANK.



PAINT ALL BUMPER POLES VALERO BLUE.  
NO TOUCH EXISTING AIR & WATER MACHINES.



PAINT ALL BUMPER POLES VALERO BLUE.  
APPLY WHITE BLOCKOUT VINYL TO COVER  
OLD VALERO LOGO.



NO TOUCH EXISTING CAR WASH CENTER.



PAINT PREVIOUSLY PAINTED VENT PIPES VALERO SAND.  
NO TOUCH EXISTING TANK.

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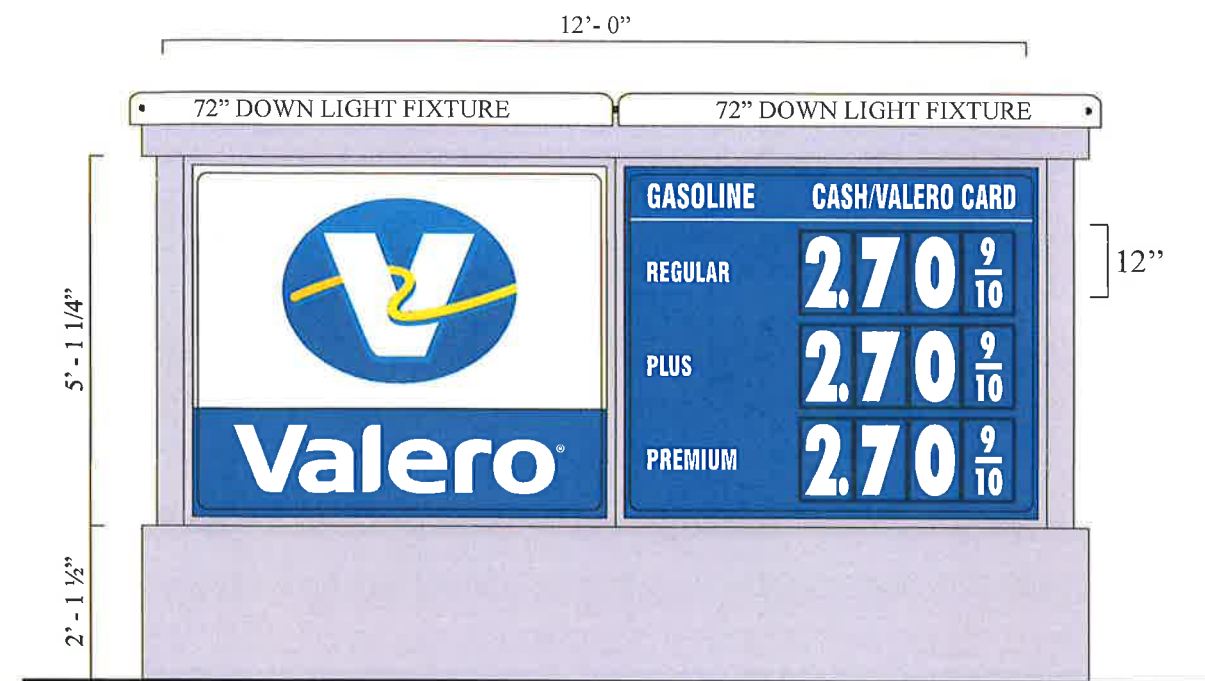
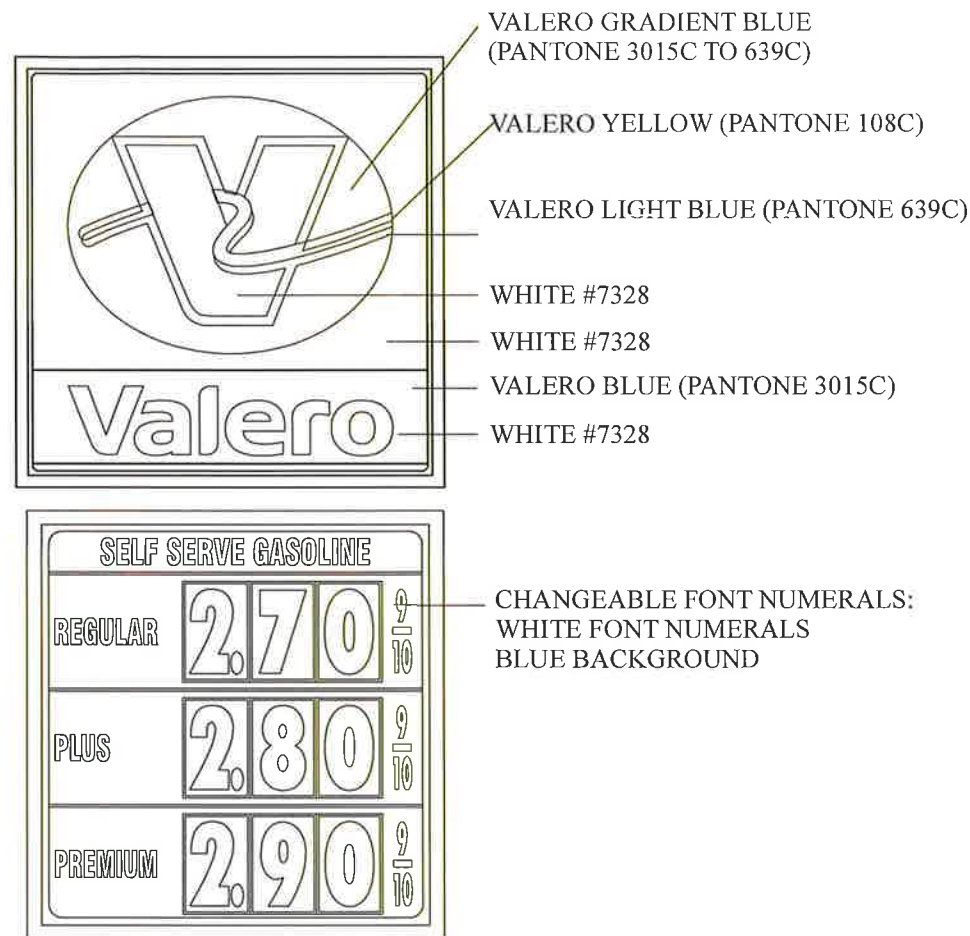
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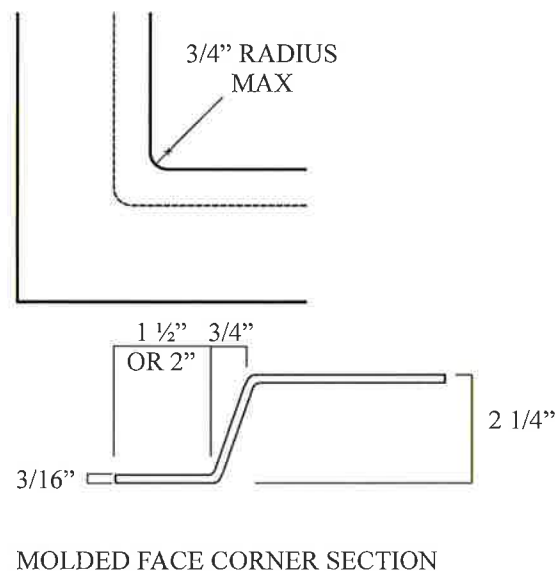
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SAN JUAN BAUTISTA, CA 95045





① REFACE EXISTING D/F PRICE/ID SIGN  
71.25 sq. ft.

SCALE: 3/8" = 1'-0"



REFACE/REPAINT EXISTING CABINET.

CABINET CONSTRUCTION: ALUMINUM  
EXTERNAL ILLUMINATION

FACE MATERIALS: MOLDED POLYCARBONATE  
FACES WITH TRANSLUCENT 3M VINYL  
SECOND SURFACE DECORATION

FINISH SPECIFICATIONS: SHERWIN WILLIAMS  
PAINTED, RETAINERS, DIVIDER BARS, POLE AND  
BASE TO MATCH VALERO MOCKING BIRD DARK GRAY

GENERAL NOTE: ALL SIGNAGE TO BE MANUFACTURED  
AND INSTALLED ACCORDING TO LOCAL AND NATIONAL  
BUILDING AND ELECTRICAL CODES

**NOTE:**  
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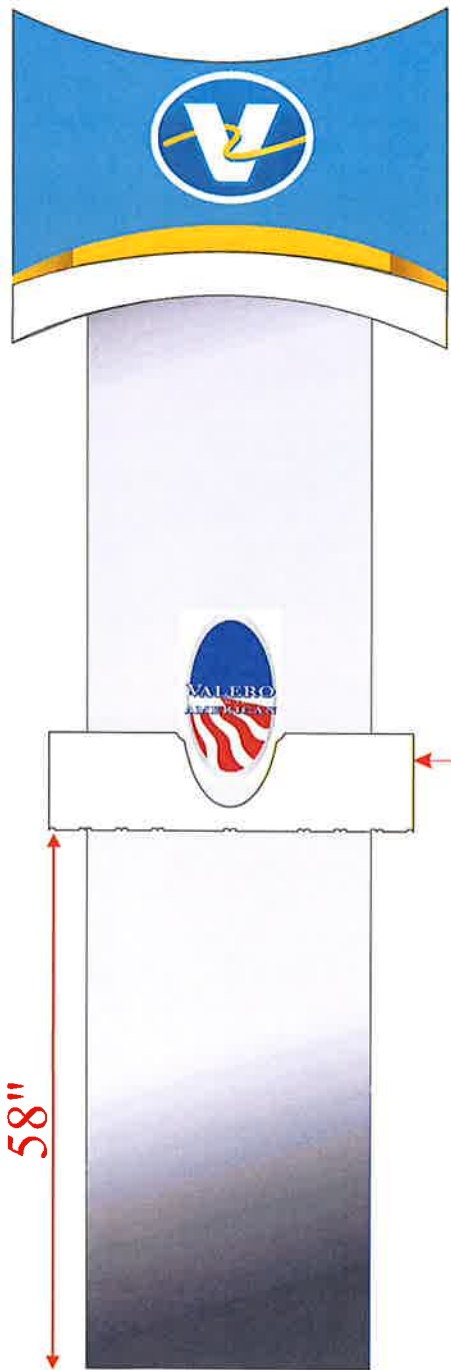
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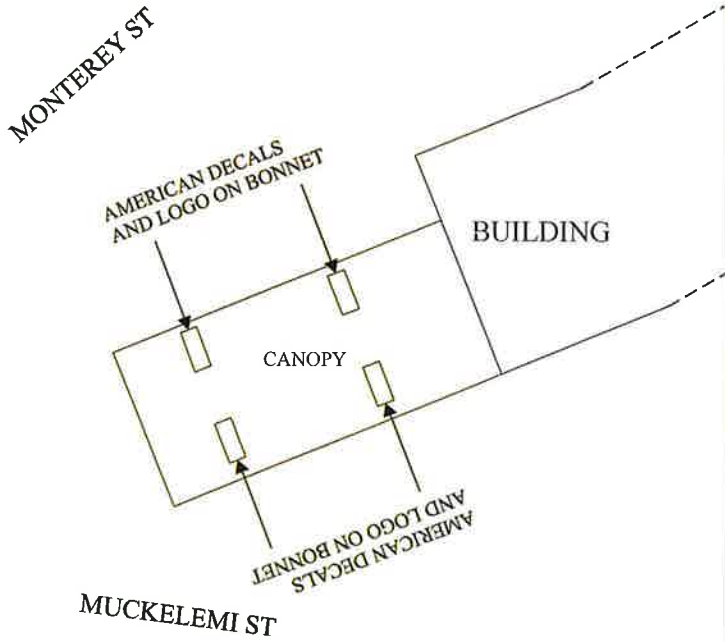
\*\*\*\*\*VERY IMPORTANT\*\*\*\*\*  
MUST NOT DRILL INTO DISPENSERS  
WHEN INSTALLING VALANCES.  
MUST USE EXISTING BOLTS TO  
ATTACH BRACKETS AND VALANCES



USE TEMPLATE  
PROVIDED FOR  
PLACEMENT



EXISTING DISPENSER



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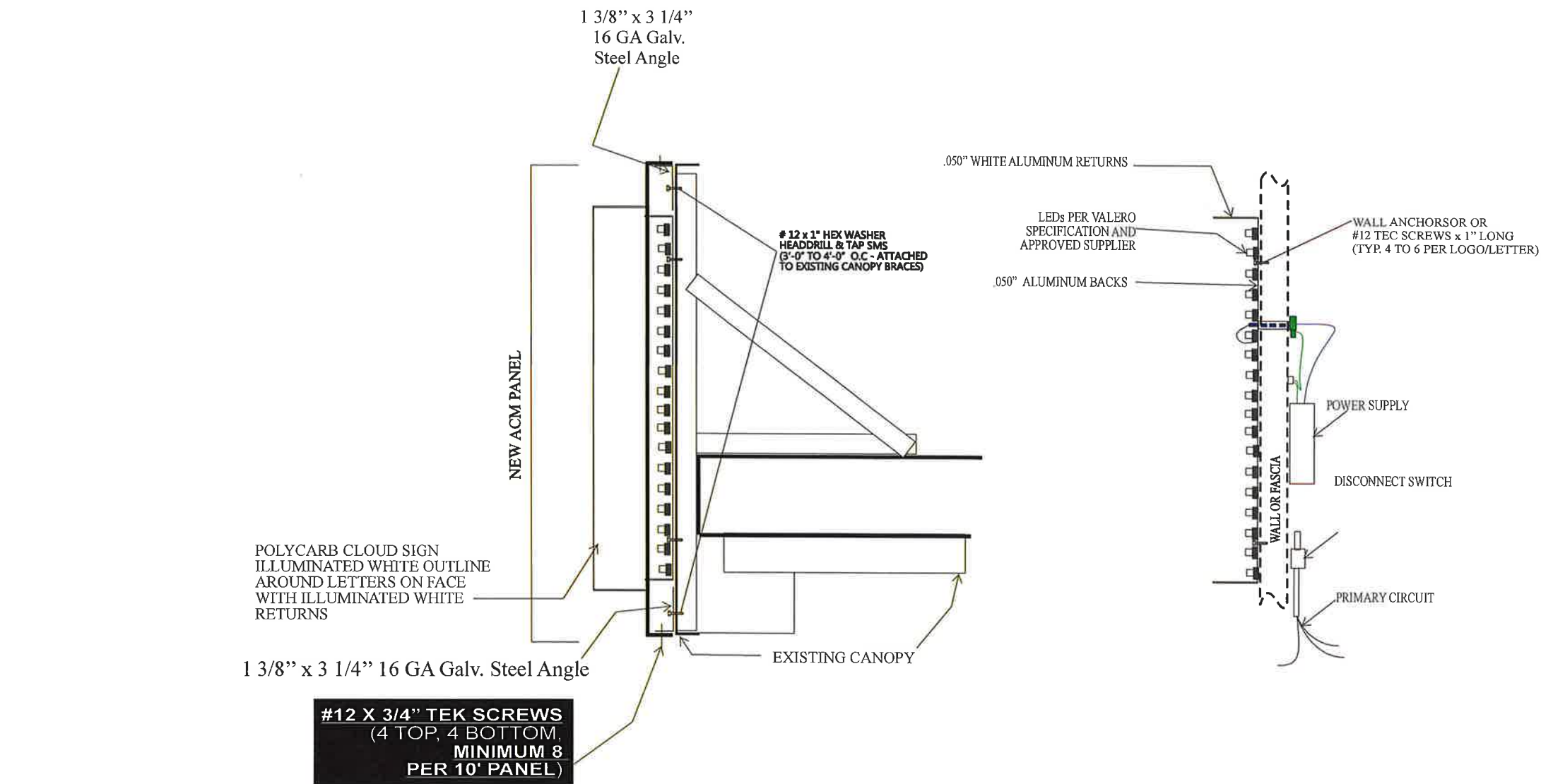
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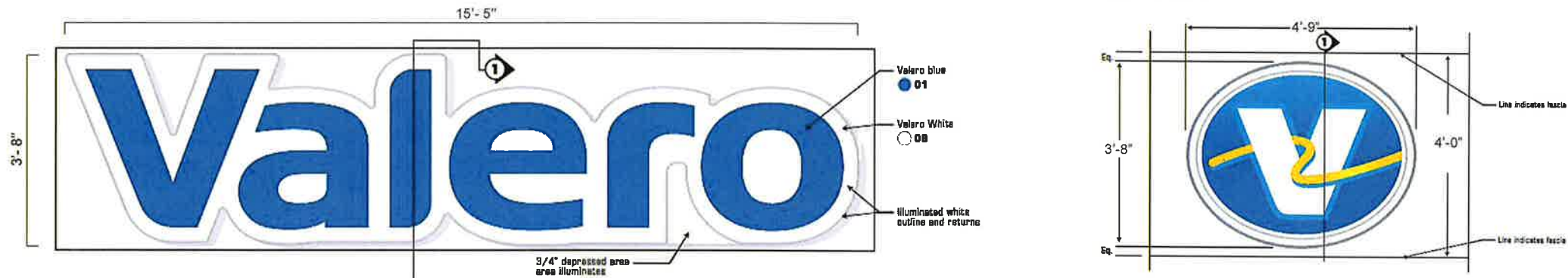
**63 MUCKELEMI ST.  
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WORDMARK & LOGO DETAILS LED ILLUMINATION



ACM FASCIA WITH ILLUMINATED COPY



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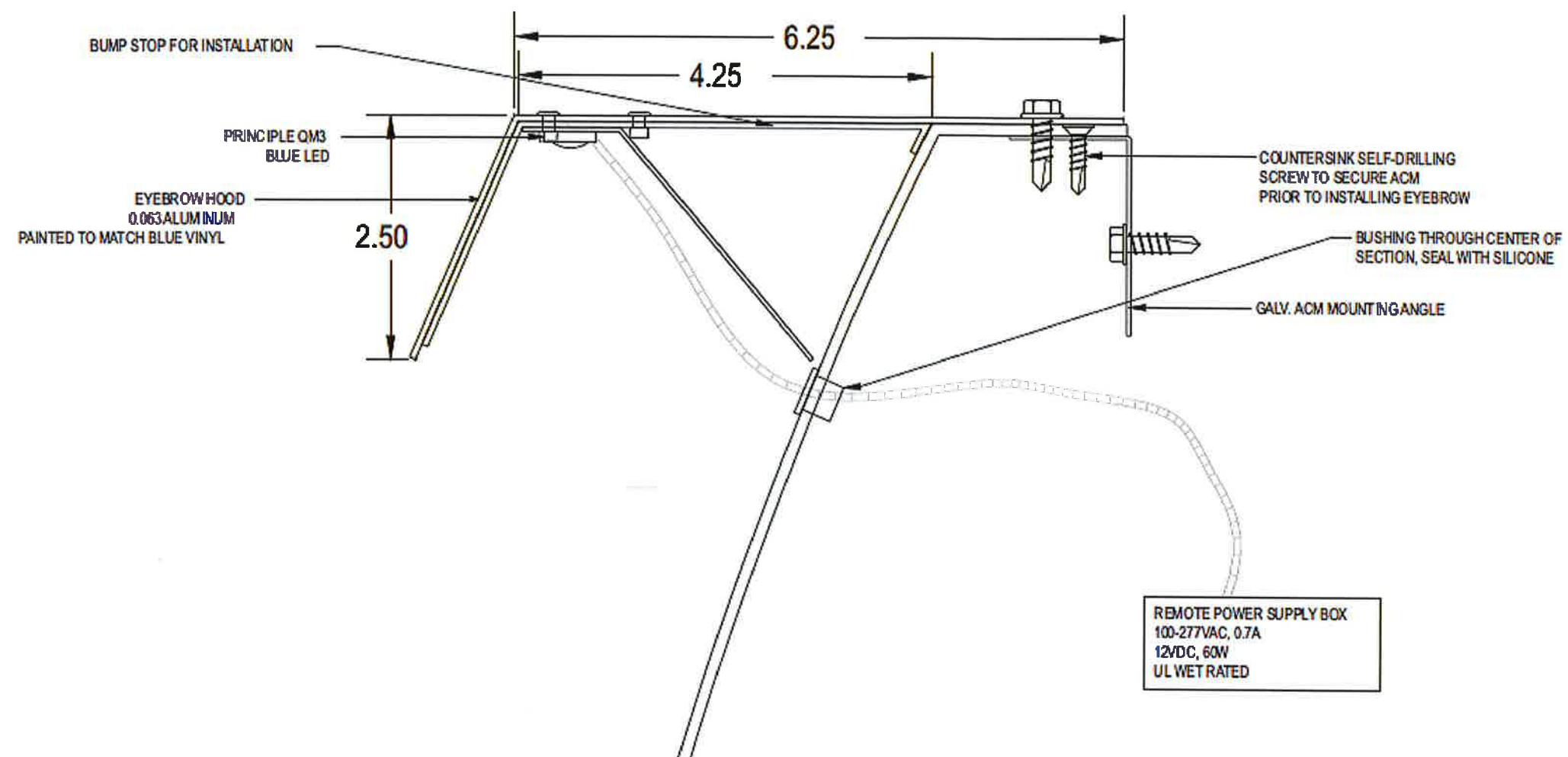
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EYEBROW SECTION VIEW DETAILS LED ILLUMINATION



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Valero

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**Valero**

GASOLINE CASH/VALERO CARD

REGULAR

2.99 <sup>9</sup>/<sub>10</sub>

PLUS

3.19 <sup>9</sup>/<sub>10</sub>

PREMIUM

3.29 <sup>9</sup>/<sub>10</sub>

DIESEL  
NO. 2

3.69 <sup>9</sup>/<sub>10</sub>







Valero

Food Shop

CASH/VALERO CARD GASOLINE CREDIT/DEBIT

3.23 <sup>9</sup>/<sub>10</sub>

3.23 <sup>9</sup>/<sub>10</sub>

REGULAR

3.31 <sup>9</sup>/<sub>10</sub>

3.31 <sup>9</sup>/<sub>10</sub>

PLUS

3.39 <sup>9</sup>/<sub>10</sub>

3.39 <sup>9</sup>/<sub>10</sub>

PREMIUM



# CITY OF SAN JUAN BAUTISTA

## PLANNING COMMISSION

### STAFF REPORT

**DATE:** February 5, 2019

**PROJECT:** Site Design and Review for new construction of two Manufactured Homes and one Accessory Building on a 4 acre parcel.

**Location:** 498 Lasuen Drive and 480 Lasuen Drive. APN #002-540-010

### DISCUSSION:

This proposal includes the construction of two manufactured homes, an accessory building, and a cargo container on a large vacant lot. The property and applicants are Dave and Kevin McGovert.

Currently, there are no existing habitable structures onsite. There is an existing white picket fence onsite that was constructed by the property owners. The site is located in the foothills with views of town. The area is semi-rural and is located near the city limits. The property owners wish to retain the rural characteristics of the site by developing a small amount of the area. The zoning classification is R-1 (Low Density Residential) and the General Plan Designation is Low Density Residential as well. Manufactured homes and accessory buildings are allowed by Site and Design Review. This project includes one main manufactured home, a secondary unit that is manufactured, and an accessory building. Also, there is an existing cargo container located onsite for the property owner to use as storage which will remain on the site. The two residential units meet the City Design Guidelines. They include log cabin siding and design which fits the profile of a rural type design. The City's Design Guidelines encourage the use of earth tone colors and a variety of building materials.

Since purchasing the property, the owners have completed a substantial amount of work onsite including maintenance, weed and trash removal, tree removal, and updates to the infrastructure. After construction, the property owners intend to plant mature trees and install landscaping. The trees to be planted will meet the Hillside Ordinance View Shed provisions; that way views looking uphill will be improved with landscaping and trees.

Staff believes this project will be an improvement to the area and the city as a whole. The development of a vacant parcel will take place while maintaining

rural characteristics, the property will be well maintained, and views will be enhanced. Staff supports this project as it is proposed.

**CEQA:** Every project is required to be analyzed to determine potential environmental effects on the surrounding area and environment. Staff reviewed the project and cross checked it with the guidelines from the California Environmental Quality Act (CEQA). It was concluded from the review that the project can be categorically exempt from CEQA review. The project meets the guidelines for exemption under CEQA, Article 19, Sections 15300.2 and 15303.

**Findings:**

1. The proposed project is being developed in a hillside setting with an average slope of 24%. The manner and scale of the development will retain and enhance the City's rural atmosphere and protect the natural environment. The scale of the development is small and the buildings will be located in places where any impacts will be minimal.
2. The project is proposed with landscaping, design, and locations where view sheds of the hillside area from Highway 156 and other rural streets and roads will be protected by having a rustic design and have enhanced landscaping and trees provided onsite. The buildings are also located on locations where they will not block views of neighboring property owners.
3. The proposed buildings on site were reviewed and are placed onsite where impacts will be minimal, scale of the project is small and natural features will be preserved.
4. The proposed project will be constructed on locations onsite according to the findings stated in the geologic report and the engineer's determination where impacts from excavation will be kept minimal. Best practices for soil erosion and development will be taking place.
5. This project is being proposed where the scale is small in relation to the size of the entire parcel, constructed on locations where views are not obstructed, minimal environmental impacts, and minimal soil erosion. Therefore, the project will not be detrimental to the public welfare of the City and its residents.

**Action:**

1. Planning Commission Chairman open the Public Hearing
2. Receive Oral comments, written communications, electronic communications and public input and testimony.
3. Close the Public Hearing
4. Planning Commission discuss the project
5. Planning Commission deliberate and make the determination to deny, modify, approve or continue the application to a date certain.



6. Consider adoption of Resolution 2019-XX approving the Site and Design Review of the proposed project.

**ATTACHED:**

Draft Resolution  
Project Environmental Setting  
Geological Inspection  
Design Elevations  
Plans

## RESOLUTION 2019 - xx

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SAN JUAN BAUTISTA APPROVING A SITE AND DESIGN REVIEW FOR NEW CONSTRUCTION OF TWO MANUFACTURED HOMES AND ONE ACCESSORY BUILDING ON A FOUR ACRE PARCEL ZONED LOW DENSITY RESIDENTIAL LOCATED ON LASUEN DRIVE, SAN JUAN BAUTISTA, CALIFORNIA. APN 002-540-010**

**WHEREAS**, the Planning Commission conducted a public hearing on February 5, 2019 to review the proposed Site and Design Review, and

**WHEREAS**, the Planning Commission received oral comments, written correspondence, electronic communication and public testimony at the public hearing, and

**WHEREAS**, the Planning Commission acknowledges that notices were sent to property owners within 300 feet of the project boundaries as listed on the latest equalized assessment roll, and

**WHEREAS**, the Planning Commission reviewed the staff report for the project and applicable zoning codes and standards, and

**WHEREAS**, the Planning Commission reviewed the staff report for the project together with findings that the proposed Major Historic Alteration is categorically exempt from CEQA (California Environmental Quality Act) review, and

**WHEREAS**, the Planning Commission has made all findings and information and makes the following finding to approve the Site Design and Review request.

1. The proposed project is being developed in a hillside setting with an average slope of 24%. The manner and scale of the development will retain and enhance the City's rural atmosphere and protect the natural environment. The scale of the development is small and the buildings will be located in places where any impacts will be minimal.
2. The project is proposed with landscaping, design, and locations where view sheds of the hillside area from Highway 156 and other rural streets and roads will be protected by having a rustic design and have enhanced landscaping and trees provided onsite. The

buildings are also located on locations where they will not block views of neighboring property owners.

3. The proposed buildings on site were reviewed and are placed onsite where impacts will be minimal, scale of the project is small and natural features will be preserved.
4. The proposed project will be constructed on locations onsite according to the findings stated in the geologic report and the engineer's determination where impacts from excavation will kept minimal. Best practices for soil erosion and development will be taking place.
5. This project is being proposed where the scale is small in relation to the size of the entire parcel, constructed on locations where views are not obstructed, minimal environmental impacts, and minimal soil erosion. Therefore, the project will not be detrimental to the public welfare of the City and its residents.

**NOW, THEREFORE, BE IT RESOLVED,** that the Planning Commission of the City of San Juan Bautista hereby approves the Site and Design Review located on Lasuen Drive, San Juan Bautista, California, subject to the following conditions.

1. The existing white picket fence currently encroaches in the Public Right-of-Way. The fence shall be relocated so it is in line with the property line so no encroachment takes place.
2. The property owner shall provide calculations to show the integrity and capacity of the existing sewer line access on Lasuen Drive in order to verify the sewer line will be adequate to accommodate the two new residences onsite.
3. The proposed scopes of work on the entire site shall be developed as shown on the proposed plans.
4. The proposed scopes of work are subject to applicable Building, Engineering, Planning, and Fire Codes.
5. Any further development on this site shall require further Planning Review and Environmental Review.

**PASSED AND ADOPTED** by the Planning Commission of the City of San Juan Bautista at a regular meeting held on February 5, 2019 by the following vote:

**AYES:**

**NOES:**

**ABSENT:**

**ABSTAIN:**

---

Scott Freels, Chairperson

ATTEST:

---

Trish Paetz, Deputy City Clerk



**KELLEY**  
**ENGINEERING & SURVEYING**

**400 PARK CENTER DRIVE, SUITE #4. HOLLISTER, CA 95023-2546**  
**OFFICE: (831) 636-1104 FAX (831) 636-1837**

**LANDS OF MCGOVERT**

**ENVIRONMENTAL SETTING**

Dave McGovert and son Kevin wish to make San Juan Bautista their new home, by proposing two new manufactured homes on Lausen Street APN 002-540-010.

The site is located near the southern City limit line on Lausen Drive, south of Highway 156 and west of Washington Street. The property was once a part of the Nyland Ranch. The 4 acre lot was created by a lot line adjustment. It is accessed by Lausen Drive, a City street. Electricity and gas are available from Pacific Gas & Electric. Telephone service is available from A.T. & T. Water, sewer, and garbage pick-up are available from the City of San Juan Bautista.

There are no existing structures on the site. One tree was permitted to be removed and is removed from the site. The site is covered by grasses suitable to control erosion. I saw no visible evidence of animal habitat on the property.

The property lies outside any mapped geologic hazard zones.

The ground slopes from the southwest to the northeast. Slopes vary from under 10% to 30%. A geologic inspection was performed by CapRock Geology, Inc. and a report dated September 21, 2018 is included with this submittal. The report identifies a potential slope failure area in the southeast portion of the property. This area is identified on the site plan. The geologist recommended a 60 foot setback from this area for all proposed structures. As a mitigation measure for this condition we are identifying the 60 foot setback from the potential slope failure area and are proposing no work in this area. The two proposed homes are located on the western portion of property. The contours in the area of potential slope failure indicate that in the event of a slope failure there would be no effect on the two homes.

A soil and foundation investigation was performed by American Soil Testing and Engineering, Inc. and a report dated September 28, 2018 is included with this submittal. The report provides grading and foundation recommendations. The report indicates that the site is suitable for the proposed construction provided that the recommendations in the report are followed.

Being on a hillside on an elevation higher than most of the City the property offers beautiful views to the north and east. **The pad elevation of the upper home site is set so as not to affect the view of the uphill neighbor.** All proposed buildings on this site are below the skyline and do not affect any views from the hillside or of the skyline.

I am unaware of any cultural or historic significance of this property or adjacent properties.

Neighboring developments to east are also situated on hillside lots on Washington Street, Lang Street and Stephens Drive. This area is developed as single family residential and is zoned low density residential.

Immediately to the south is a single family home situated on the lower ridge of a two tiered hill.

Mr. McGovert has received permission from Mr. Gagnon to access his sewer lateral for sewer service. Gagnon's sewer lateral runs the length of the property and is shown on the site plan.

Site grading is being kept to a minimum by proposing smaller homes and short driveways. All work is designed to conform to the natural grade by orienting the building and driveways along the natural contours. Preliminary grading plans have been prepared and the earthwork is estimated to be approximately 1650 cubic yards of cut and fill. This is a balanced site requiring no import or export of soil.

Erosion will be controlled by hydroseeding all disturbed slopes and soil runoff will be contained with silt fence. The total disturbed area measures less than 1 acre, so a State storm water permit is not required. We are using a low impact development strategy by implementing rain barrels at each structure. Rain captured in the rain barrels will be re-used to water landscaping and gardens. This is an approved post-construction runoff reduction technique specified in the California Regional Water Quality Control Board Central Coast Region Resolution No. R3-2013-0032.

PO Box 7546, Spreckels, CA 93962-7546

TEL: 831 / 484-5053

EMAIL: Caprockgeology@gmail.com

caprockgeology.com

Mr. David McGovern  
500 Chardonnay Way  
Hollister, California 95023

Ref. No.: 5042-02  
September 21, 2018

**GEOLOGICAL INSPECTION  
500 LASUEN STREET  
PARCEL 1, REC. FILE 2016-0003061  
SAN JUAN BAUTISTA, CALIFORNIA**

The purpose of this letter is to describe the results of the reconnaissance-level geological inspection of the hillside site on August 3, 2018. The inspection of the property is prior to potential purchase of the property. The inspection consisted of viewing the property, proposed building location, and slope. The property is above Highway 156 in the city of San Juan Bautista on a north facing slope of the Gabilan Mountain Range. Plans or site specific topographic maps were not available and soil sampling, foundation inspection, or slope stability analysis was NOT performed.

The approximately four acre site (4.06 acres) is located on a north facing slope which extends gradually and steeply down to Highway 156. The elevation of the potential building location (southwest corner of property) is about 390 feet above sea level. A single family home is located on the ridge crest above the building site on the neighboring property. The undeveloped site is covered with annual grasses and scattered trees.

The site is underlain by the unexposed Eocene age sedimentary San Juan Bautista Formation that dips 30 degrees southwest into the north facing slope according to the California Geological Survey, Monterey 1:100,000 scale map, 2002. The slopes are susceptible to large and small scale slope failure. Landslides exist near the property extending down from ridge to near Highway 156. Nearby slope failure evident during the site visit consisted of evidence of slumping and smaller scale landslides.

The active San Andreas Fault Zone is located about one-half mile north east of the site, the exact location of the fault was not determined. The site will experience moderate to severe shaking in the event of a large earthquake.

The existence of slope failure of different types at the site indicates that site development (while not being excluded) requires careful site selection. Some setback distance from the steeper slopes is recommended. The southwest corner of the site is proposed for a modular home and is geologically suitable and preferable compared to other potential building sites on the property.

The southwest corner is near the crest of the ridge and is flanked below by a nose of the ridge on City of San Juan Bautista property.

The southeast portion of the property as delineated on the accompanying site topographic map (Kelly Engineering, 2018) has been noted as a potential slope failure area based on aerial photographs and hummocky terrain. Slope failure appears to consist of shallow earthflows but deeper slope failure is not precluded due to the preliminary nature of this reconnaissance. A sixty foot setback is recommended and is delineated.

This letter has been prepared on behalf of Mr. David McGovert as a description of services performed in San Benito County, California. In performing our professional services, we have applied present engineering and scientific judgment and used a level of effort consistent with the standard of practice measured on the date of this letter and in the locale of the project site for similar type activities. CapRock makes no warranty, expressed or implied, in fact or by law, whether of merchantability, fitness for any particular purpose, or otherwise, concerning any of the services provided by CapRock to the client.

Please feel free to contact me at (831) 484-5053 if you have any questions regarding this letter.

Sincerely,

**CapRock Geology, Inc.**

A handwritten signature in cursive script, reading "Robert Barminski".

Robert Barminski, R.G., C.E.G.

Principal Geologist





**NOTE:**  
FINISH MATERIAL AND COLOR ARE  
PRELIMINARY AND SUBJECT TO  
CHANGE BASED ON OWNER  
PREFERENCE AND PRODUCT  
AVAILABILITY.



	BY
In accordance with section 6735 (a) of the Professional Engineer's Act these plans are <b>PRELIMINARY</b> and therefore do not bear the signature and seal of a registered civil engineer.	

**KELLEY**  
**ENGINEERING & SURVEYING**  
400 PARK CENTER DRIVE, SUITE #4, HOLLISTER, CA 95023  
OFFICE (831) 636-1104 FAX (831) 636-1837



**MCGOVERT**  
**PRIMARY RESIDENCE PHOTO ELEVATIONS**  
**LAUSEN STREET, SAN JUAN BAUTISTA**

Job No.:	18031
Designed:	PHH
Scale:	NTS
Date:	Oct 2018
Sheet:	1
of	1



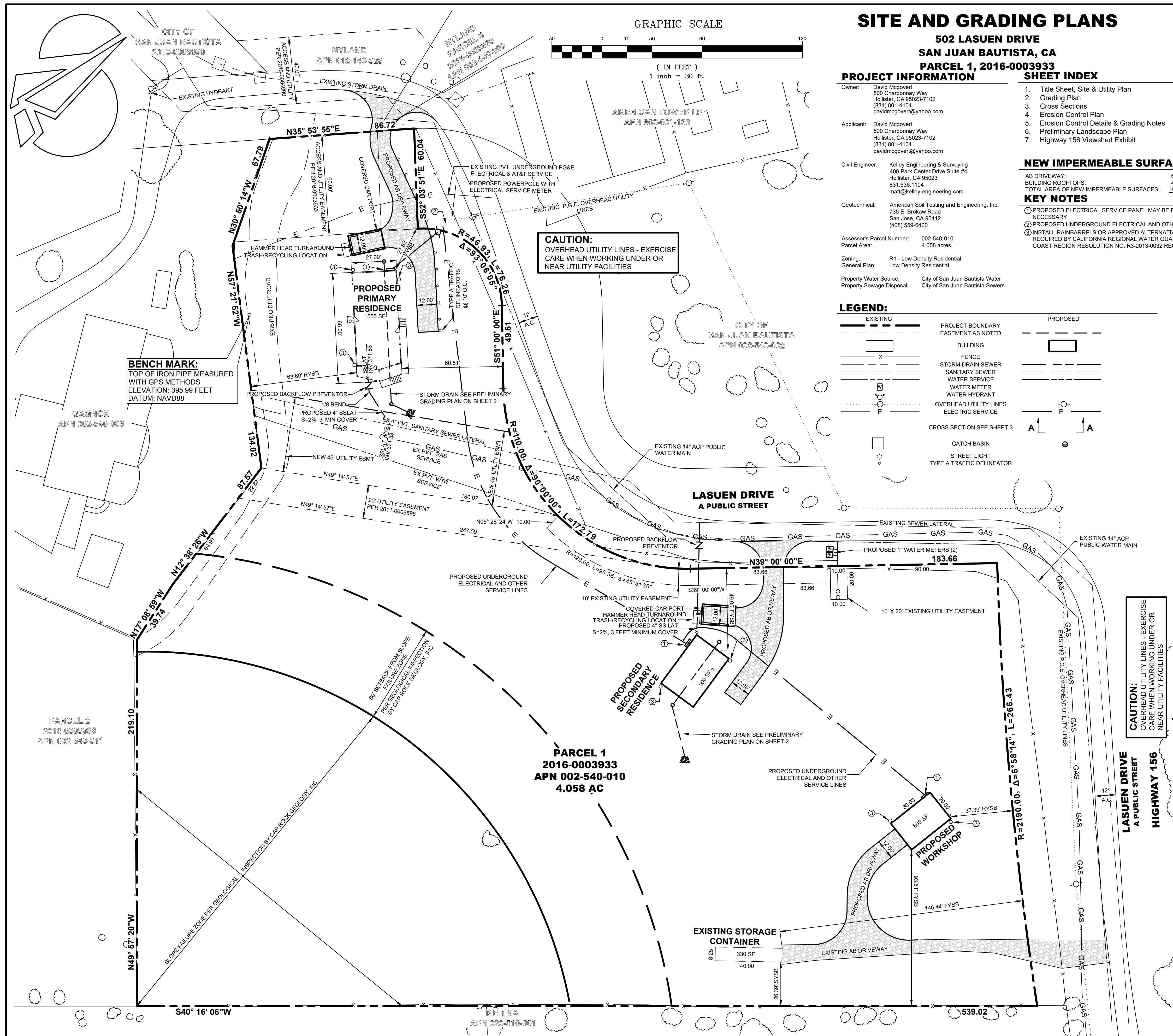


**NOTE:**  
FINISH MATERIAL AND COLOR ARE  
PRELIMINARY AND SUBJECT TO  
CHANGE BASED ON OWNER  
PREFERENCE AND PRODUCT  
AVAILABILITY.



<div><div>△</div><div>△</div><div>△</div></div> <div>BY</div>	<div><div>In accordance with section 6735 (a) of the Professional Engineer's Act these plans are</div><div>PRELIMINARY</div><div>and therefore do not bear the signature and seal of a registered civil engineer.</div></div>	<div>KELLEY</div> <div>ENGINEERING &amp; SURVEYING</div> <div>400 PARK CENTER DRIVE, SUITE #4, HOLLISTER, CA 95023</div> <div>OFFICE (831) 636-1104 FAX (831) 636-1837</div>		<div>MCGOVERT</div> <div>SECONDARY RESIDENCE PHOTO ELEV</div> <div>LAUSEN STREET, SAN JUAN BAUTISTA</div>	<div>Job No.: 18031</div> <div>Designed: PHH</div> <div>Scale: NTS</div> <div>Date: Oct 2018</div> <div>Sheet: 1</div> <div>of 1</div>
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**ABBREVIATIONS:**

APN	ASSESSOR'S PARCEL NUMBER	FF	FINISH FLOOR	MIN	MINIMUM	SYSB	SIDE YARD SETBACK
BYSB	BACK YARD SETBACK	FYBS	FRONT YARD SETBACK	NTS	NOT TO SCALE	WTR	WATER
CB	CATCH BASIN	FG	FINISH GRADE	R	RADIUS	SD	STORM DRAIN
CBC	CALIFORNIA BUILDING CODE	GFF	GARAGE FINISH FLOOR	SD	STORM DRAIN	SF	SQUARE FEET
CY	CUBIC YARDS	INV	INVERT	SS	SANITARY SEWER	SSLAT	SANITARY SEWER LATERAL
Δ	DELTA ANGLE	LF	LENGTH	SSLAT	SANITARY SEWER LATERAL	STA	STATION
E	EXISTING GROUND	MAX	MAXIMUM				
ELEV	ELEVATION						

**UNDERGROUND UTILITY NOTE:**  
THE TYPES, LOCATIONS, SIZES AND/OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE AND WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF SUCH UNDERGROUND UTILITIES. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES. HOWEVER, THE ENGINEER CAN ASSUME NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF ITS DELINEATION OF SUCH UNDERGROUND UTILITIES WHICH MAY BE ENCOUNTERED, SHOWN OR NOT SHOWN ON THIS PLAN.

**BOUNDARY NOTE:**  
THE BOUNDARY SHOWN ON THESE PLANS IS PROTRACTED FROM RECORD INFORMATION ONLY AND DOES NOT CONSTITUTE A SURVEY OF THE LAND. SAID RECORD INFORMATION PROVIDED BY PRELIMINARY REPORT DATED FEBRUARY 28, 2018, PREPARED BY FIRST AMERICAN TITLE COMPANY.

**FARMLAND**  
Prime Farmland: According to the San Benito County Important Farmland 2014 Map by California Department of Conservation, the project site contains no prime farmland.  
Grade 1 Soil: According to Benito County's WebGIS, the project site contains no Grade 1 soil.

**FLOOD ZONES AND SEISMIC SPECIAL STUDIES ZONES**  
FLOOD ZONE: According to FIRM 06089C0158D, Map Revised 4/16/2009, this property lies within ZONE X; Areas determined to be outside the 0.2% annual chance floodplain.  
SEISMIC ZONE: According to the Alquist Priolo Special Studies Zone Map, San Juan Bautista Quadrangle, dated 7/11/1974 this project does not lie within an Alquist-Priolo fault zone.

**GENERAL NOTES**  
1. ALL CONSTRUCTION MUST BE TO THE CITY OF SAN JUAN BAUTISTA STANDARDS AND ACCEPTED BY THE PUBLIC WORKS INSPECTOR.  
2. CONTRACTOR SHALL MEET WITH THE CITY OF SAN JUAN BAUTISTA AT LEAST 48 HOURS PRIOR TO START OF CONSTRUCTION, 24 HOURS NOTICE REQUIRED ON ALL INSPECTIONS. IN GENERAL, THE CITY ENGINEER SHALL INSPECT WORK WITHIN THE PUBLIC RIGHT-OF-WAY AND CITY BUILDING OFFICIAL SHALL INSPECT WORK ON PRIVATE PROPERTY.  
3. CONTRACTOR IS RESPONSIBLE TO MAKE ALL ARRANGEMENTS FOR SITE INSPECTIONS AND INSURE THAT ALL CURRENT STANDARDS FOR THE CITY OF SAN JUAN BAUTISTA ARE FOLLOWED PRIOR TO THE BEGINNING ANY PHASE OF CONSTRUCTION WORK.  
4. CONSTRUCTION SHALL BE LIMITED TO BETWEEN THE HOURS OF 7:00 A.M. AND 6:00 P.M., MONDAY THRU FRIDAY AND INSPECTION REQUESTS SHALL BE LIMITED TO NORMAL CITY BUSINESS HOURS: 8:00 A.M. TO 5:00 P.M., MONDAY THRU FRIDAY. ARRANGEMENTS FOR ANY OVERTIME INSPECTION SERVICES AND PAYMENT OF FEES FOR SAME SHOULD BE MADE 48 HOURS IN ADVANCE AND ARE SUBJECT TO INSPECTION AVAILABILITY AND APPROVAL BY THE CITY ENGINEER, AND OR CITY BUILDING OFFICIAL.  
5. THE OWNER IS RESPONSIBLE FOR ARRANGEMENTS TO PAY FOR ALL MATERIAL TESTING REQUIRED BY THE CITY ENGINEER, AND OR CITY BUILDING OFFICIAL. IT IS THE CONTRACTOR'S RESPONSIBILITY TO SEE TO IT THAT ALL REQUIRED TESTING IS PERFORMED.  
6. DUST CONTROL DURING THE GRADING PROCESS IS THE RESPONSIBILITY OF THE CONTRACTOR. IT IS ALSO THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN CLEANLINESS OF THE EXISTING IMPROVED STREETS IN THE CONSTRUCTION AREA.  
7. WATER FOR DUST CONTROL AND USE FOR COMPACTION MAY BE PURCHASED FROM THE APPROPRIATE AGENCY PRIOR TO START OF ANY WORK, AND IT IS THE RESPONSIBILITY OF THE CONTRACTOR FOR ANY FEES OR DEPOSITS.  
8. IT IS THE CONTRACTOR'S RESPONSIBILITY TO HANDLE SMALL SOILS IMBALANCES BY DISTRIBUTING THE REQUIRED CUT OR FILL OVER AN APPROPRIATE AREA OF THE SITE. ALTERNATIVELY THE REMAINING SOIL IMBALANCE CAN BE IMPORTED AND EXPORTED OFFSITE AS NEEDED.  
9. CONTRACTOR IS TO MAKE PROVISIONS FOR TRENCH SPOILS.  
10. PADS SHALL BE GRADED TO WITHIN +/- 0.1 FEET OF THE GIVEN ELEVATION.  
11. SAWCUT ALL TRENCHES IN EXISTING PAVEMENT TO A CLEAN, NEAT VERTICAL EDGE.  
12. CONTRACTOR IS RESPONSIBLE FOR COMPACTION OF ALL UTILITY TRENCHES INCLUDING P.G. & E., AND FOR THE SPOILS GENERATED BY THESE SAME UTILITY TRENCHES.  
13. ALL ENDS, BENDS, AND TEES ON WATER LINES MUST HAVE ADEQUATE THRUST BLOCKS CALCULATED FROM CITY OF SAN JUAN BAUTISTA STANDARDS.  
14. CITY WILL OPERATE ALL EXISTING WATER VALVES. CONTRACTOR SHALL MAKE ARRANGEMENTS IN ADVANCE WITH THE PUBLIC WORKS INSPECTOR.  
15. SEWER SERVICES FOR RESIDENTIAL LOTS TO BE MINIMUM 4" PIPE.  
16. CONTRACTOR SHALL FURNISH CERTIFICATIONS OF COMPLIANCE TO THE CITY OF SAN JUAN BAUTISTA FOR CRUSHED MISCELLANEOUS BASE MATERIAL PRIOR TO BUILDING DRIVEWAYS.  
17. IN AREAS WHICH ARE TO RECEIVE A.C., P.C.C. OR AGGREGATE BASE, THE CONTRACTOR SHALL MAINTAIN SUBGRADE AT THE AS GRADED WATER CONTENT. IF THE SUBGRADE IS ALLOWED TO DRY, THE WATER CONTENT OF SOIL SHOULD BE RAISED TO THE RECOMMENDED VALUE SPECIFIED FOR THE PROJECT.  
18. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING FACILITIES AND TO IMMEDIATELY NOTIFY THE CITY ENGINEER IN WRITING OF ANY FIELD CONFLICTS OR OMISSIONS.  
19. PRIOR TO THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT FOR APPROVAL THE PROPOSED ROUTES FOR ALL CONSTRUCTION TRAFFIC ON THE PROJECT. UPON APPROVAL, THE CONTRACTOR SHALL STRICTLY ADHERE TO THE APPROVED ROUTES ONLY.  
20. THE CONTRACTOR SHALL MAINTAIN ACCESS TO RESIDENCES, BUSINESSES, AND THE CITY OF SAN JUAN BAUTISTA ALONG THE STREETS TO BE REPAIRED THROUGHOUT THE LIFE OF THE CONTRACT.  
21. THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICES ALERT (U.S.A.) 800-227-2600 TWO (2) WORKING DAYS PRIOR TO THE START OF ANY EXCAVATION OR DEMOLITION.  
22. THE CONTRACTOR IS TO COORDINATE WITH UTILITY OWNERS OTHER THAN THE CITY OF SAN JUAN BAUTISTA TO HAVE THEIR MANHOLES OR VALVE BOXES ADJUSTED AS NEEDED TO ACCOMMODATE THE PROPOSED CONSTRUCTION. CONTRACTOR SHALL ADJUST ALL AFFECTED UTILITIES OF THE CITY OF SAN JUAN BAUTISTA TO FINISH GRADE.  
23. ANY DAMAGE TO ANY EXISTING FACILITIES INCLUDING BUT NOT LIMITED TO: TREES, LANDSCAPING, IRRIGATION, FENCES, WALLS, SIDEWALK, AND OTHER PAVED SURFACES, SHALL BE REPAIRED TO THE SATISFACTION OF THE OWNER AND THE CITY ENGINEER, AND OR CITY BUILDING OFFICIAL, AT THE CONTRACTOR'S EXPENSE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PHOTOGRAPH OR VIDEOTAPE EXISTING CONDITIONS PRIOR TO THE START OF CONSTRUCTION TO SUBSTANTIATE ANY PRE-CONSTRUCTION DEFECTS.  
24. THE CONTRACTOR SHALL RESTORE ANY AND ALL PAVEMENT AND OTHER FACILITIES OUTSIDE THE LIMITS OF WORK AFFECTED BY THE CONSTRUCTION OPERATIONS AT NO ADDITIONAL COST.  
25. THE CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL AND SIGNING PLANS TO THE CITY AT THE PRECONSTRUCTION CONFERENCE. TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE THE CONTRACTOR'S RESPONSIBILITY. THE CONTRACTOR SHALL PROVIDE ALL LIGHTS, SIGNS, BARRICADES, FLAGMEN, AND ANY OTHER REQUIRED DEVICES TO PROVIDE SAFE PASSAGE OF PUBLIC VEHICULAR AND PEDESTRIAN TRAFFIC.  
26. BIDDERS SHOULD NOTE THE PRESENCE OF OVERHEAD UTILITIES IN THE WORK AREA AS PART OF THEIR PRE-BID INSPECTION. BIDDERS SHALL NOTE THE TYPE AND LOCATION OF OVERHEAD UTILITIES IN THE WORK AREA AND BIDDERS PRICE SHALL INCLUDE THE COST FOR WORKING IN AREAS WHERE UTILITIES EXIST AT THE TIME OF BIDDING.  
27. THE OWNER SHALL OBTAIN AN ENCROACHMENT PERMIT, AFTER PLAN APPROVAL, BUT BEFORE ANY CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY IS ALLOWED.  
28. OWNER AND OR CONTRACTOR SHALL MAINTAIN DUST, AND SHALL KEEP ALL DEBRIS AND MATERIALS OUT OF THE PUBLIC RIGHT-OF-WAY. CONTRACTOR SHALL CLEAN THE STREET AS DIRECTED BY THE CITY ENGINEER, AND OR THE CITY BUILDING OFFICIAL.

**FIRE CHIEF APPROVAL**  
FIRE HYDRANT LOCATIONS AND FIRE ACCESS APPROVED AS SHOWN ON THESE PLANS. APPROVAL OF FIRE ACCESS AND HYDRANT LOCATIONS DOES NOT CONSTITUTE APPROVAL OF FIRE SPRINKLER SYSTEMS.

**CITY ENGINEER APPROVAL**  
APPROVAL OF THESE PLANS DOES NOT RELEASE THE OWNER OF RESPONSIBILITY FOR CORRECTIONS, ERRORS OR OMISSIONS CONTAINED HEREIN. IF DURING THE COURSE OF CONSTRUCTION OF THESE IMPROVEMENTS, PUBLIC INTEREST REQUIRES A MODIFICATION OR DEPARTURE FROM THE CITY SPECIFICATIONS OR THESE IMPROVEMENT PLANS, THE CITY ENGINEER SHALL HAVE THE AUTHORITY TO REQUIRE SUCH MODIFICATIONS OF DEPARTURE AND TO SPECIFY THE MANNER IN WHICH THE SAME IS TO BE MADE. ALTHOUGH THE PLANS WITHIN APPEAR TO MEET MINIMUM REQUIREMENTS OF THE CITY ORDINANCES, THIS APPROVAL DOES NOT CERTIFY SOIL OR SLOPE STABILITY, ADEQUACY OF SURFACE OR SUBSURFACE DRAINAGE CONTROLS, OR THAT SLIDES, SLOPE FAILURE, OR WATER RELATED DAMAGE WILL NOT OCCUR.

**CHARLIE BEDOLLA**  
FIRE CHIEF, CITY OF SAN JUAN BAUTISTA

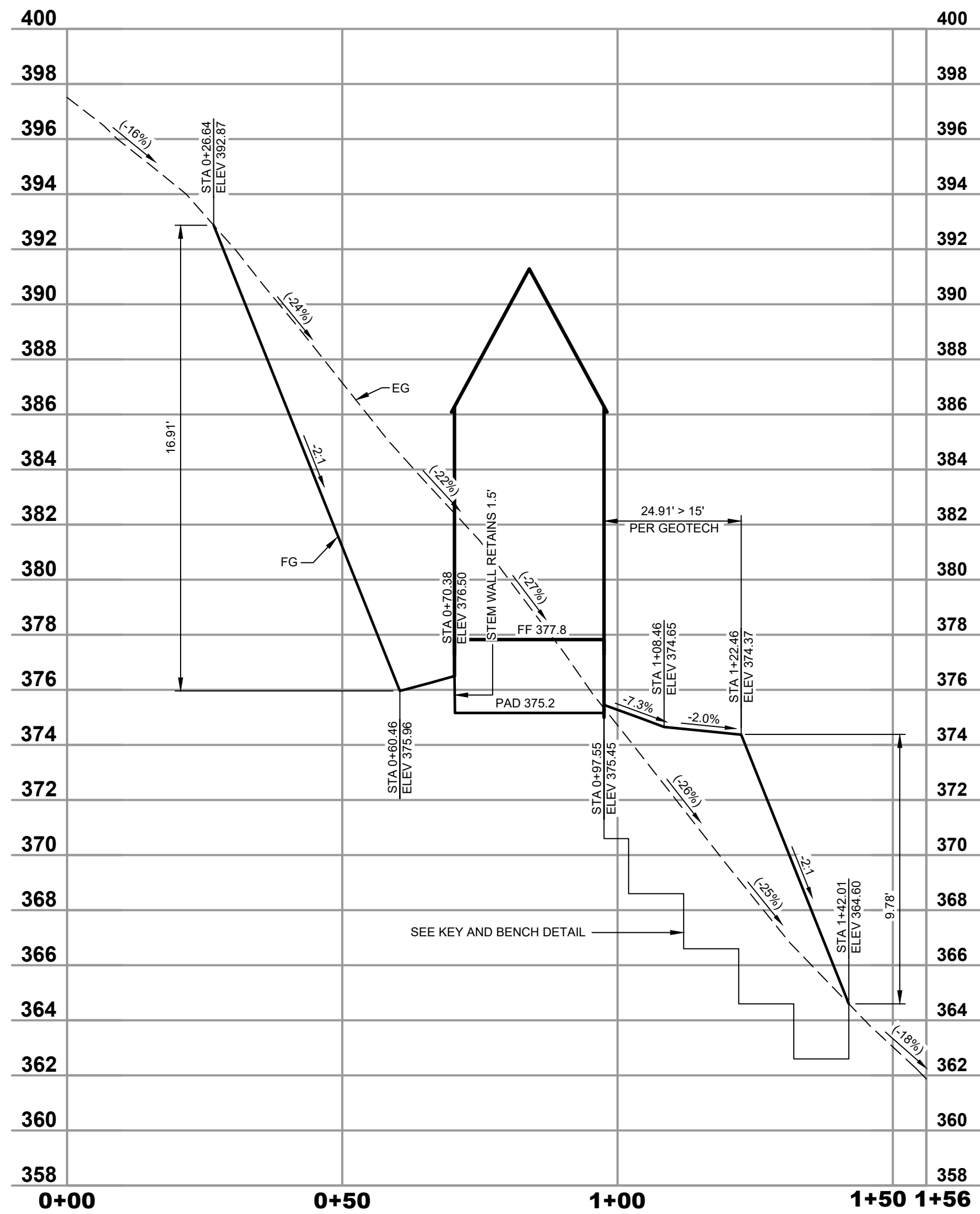
**PATRICK DOBBINS, RCE 48223 EXP. 6/30/2020**  
CITY ENGINEER  
CITY OF SAN JUAN BAUTISTA

△	BY	DATE	REVISIONS	APPR	<b>KELLEY ENGINEERING &amp; SURVEYING</b> 400 PARK CENTER DRIVE, SUITE #4 HOLLISTER, CA 95023 OFFICE (831) 636-1104 FAX (831) 636-1837		DATE: DECEMBER 2018	<b>MCGOVERT</b> <b>TITLE SHEET, SITE &amp; UTILITY PLAN</b> <b>502 LASUEN DRIVE, SAN JUAN BAUTISTA</b>	<b>SHEET 1 OF 7</b>
							SCALE: 1" = 30'		
							DESIGNED: MJK, TJK		
							DRAWN: TJK		
							JOB NO.: 18031		



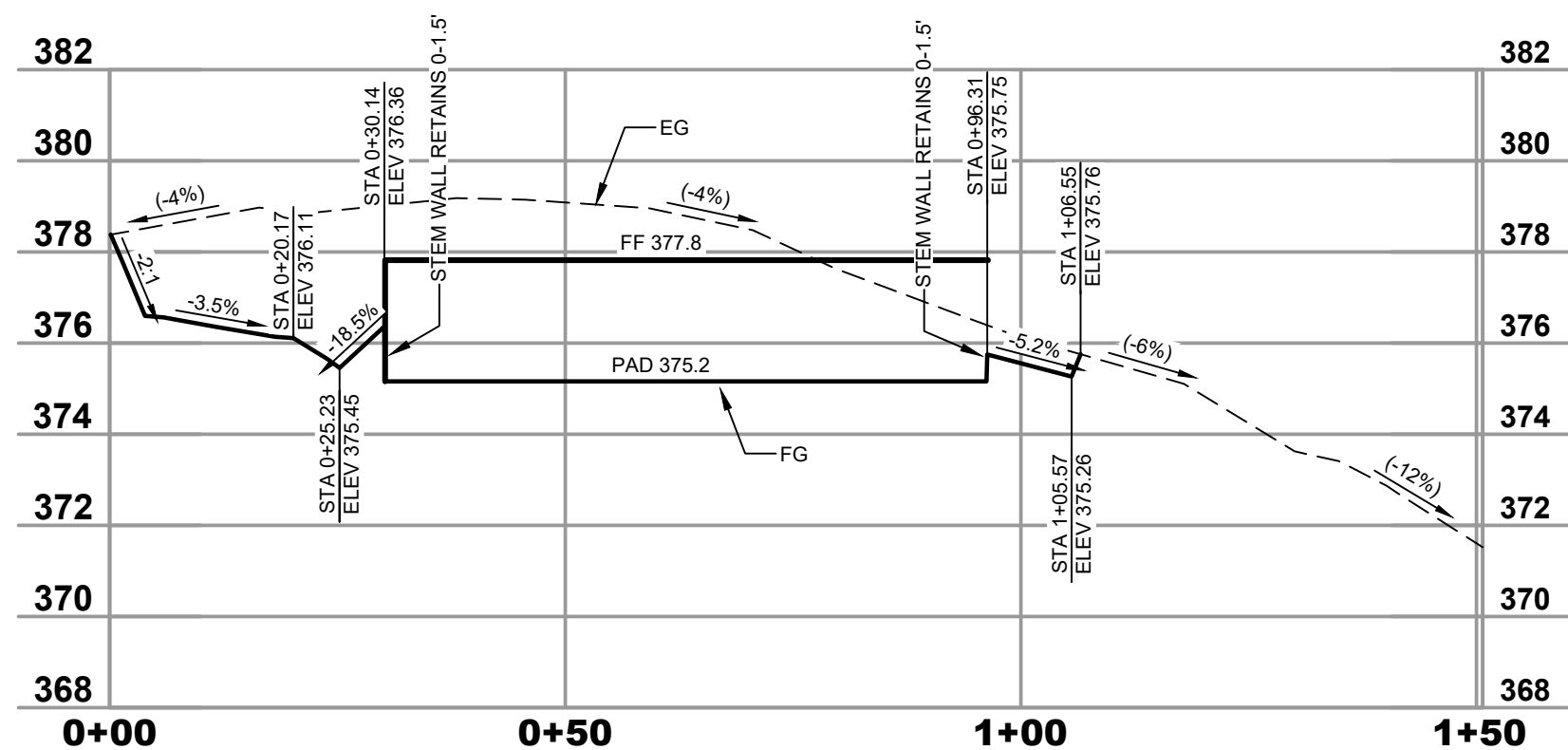






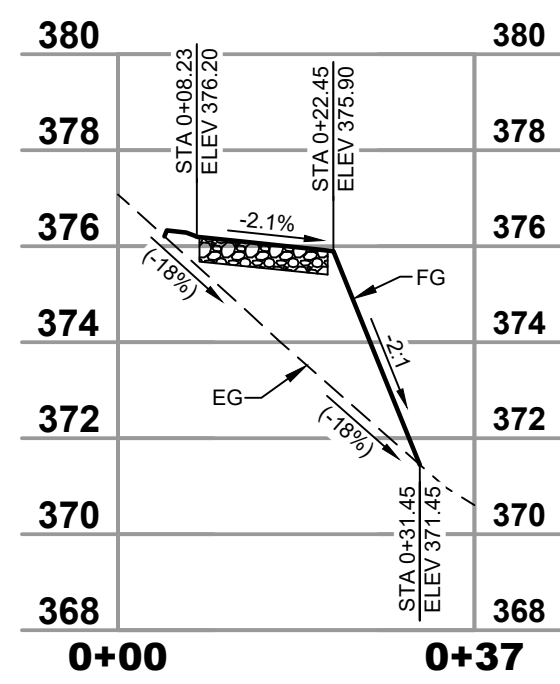
SECTION A-A

SCALE: H: 1" = 20' V: 1" = 4' (5x)



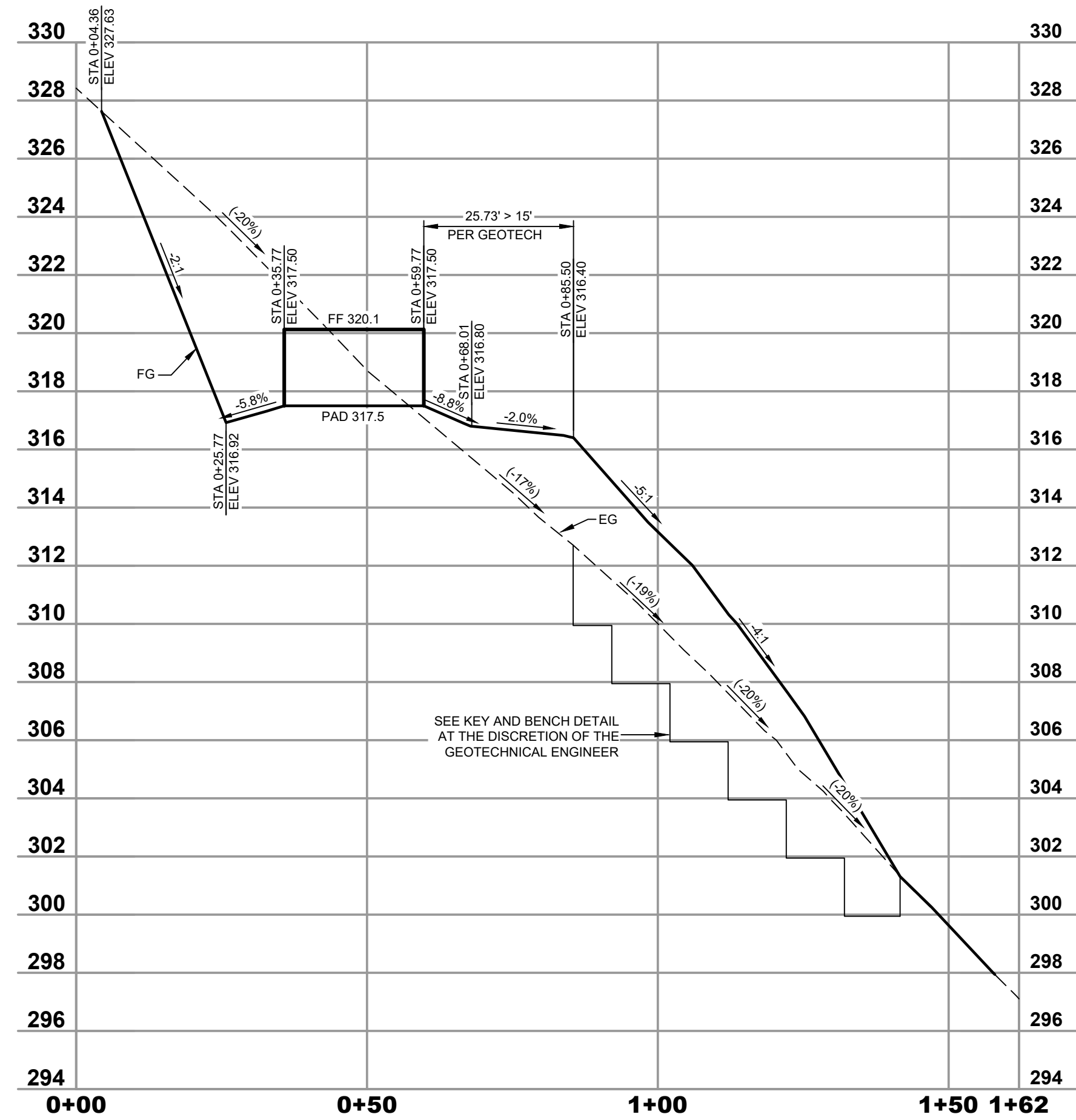
SECTION B-B

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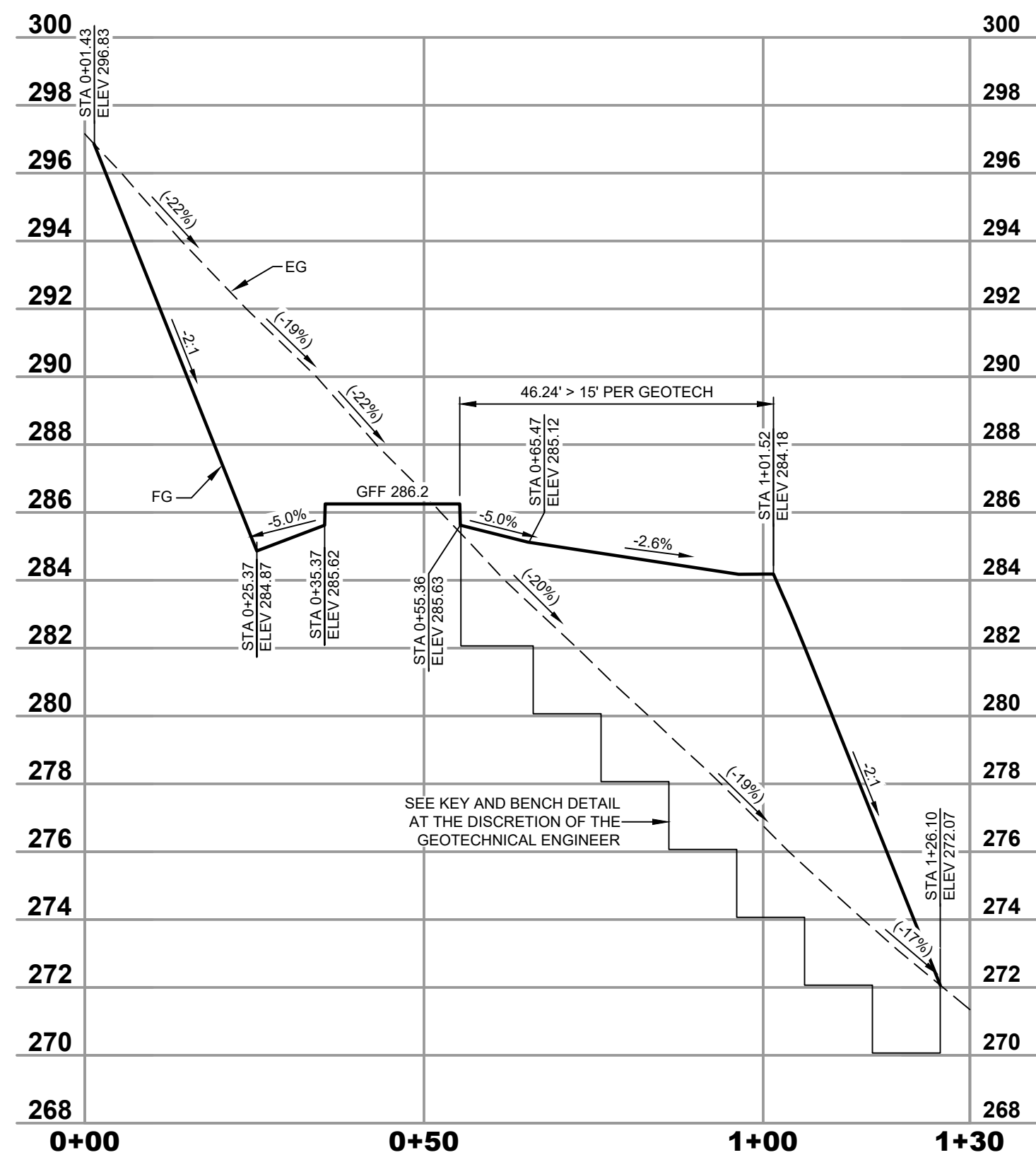
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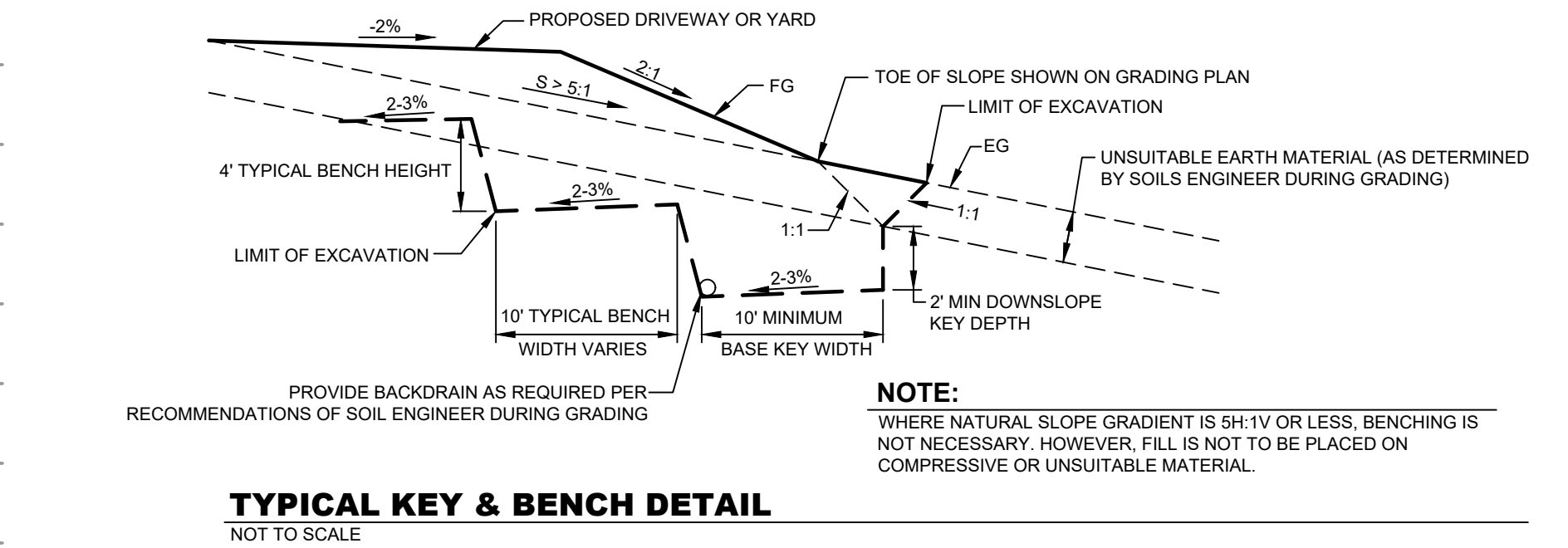
SECTION D-D

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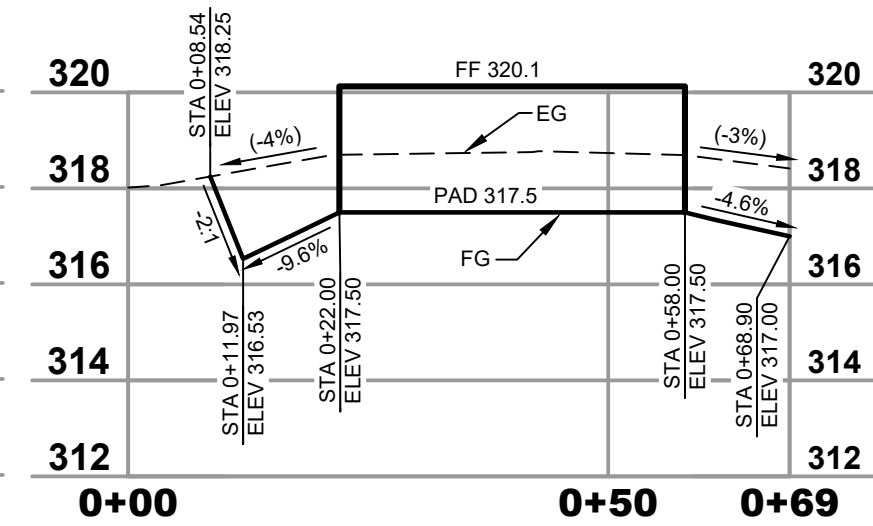
SECTION E-E

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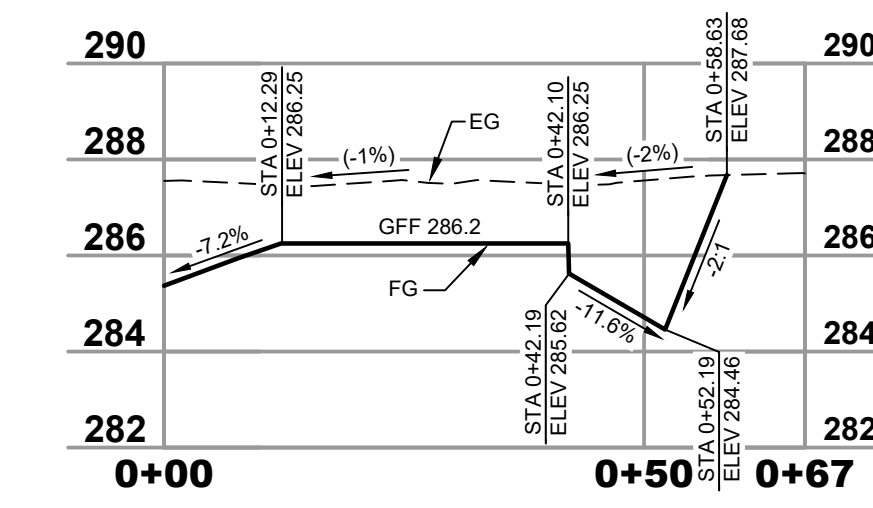
TYPICAL KEY & BENCH DETAIL

NOT TO SCALE



SECTION F-F

SCALE: H: 1" = 20' V: 1" = 4' (5x)



SECTION G-G

SCALE: H: 1" = 20' V: 1" = 4' (5x)

BY	DATE	REVISIONS	APPR

**KELLEY**  
**ENGINEERING & SURVEYING**  
400 PARK CENTER DRIVE, SUITE #4  
HOLLISTER, CA 95023  
OFFICE (831) 636-1104 FAX (831) 636-1837

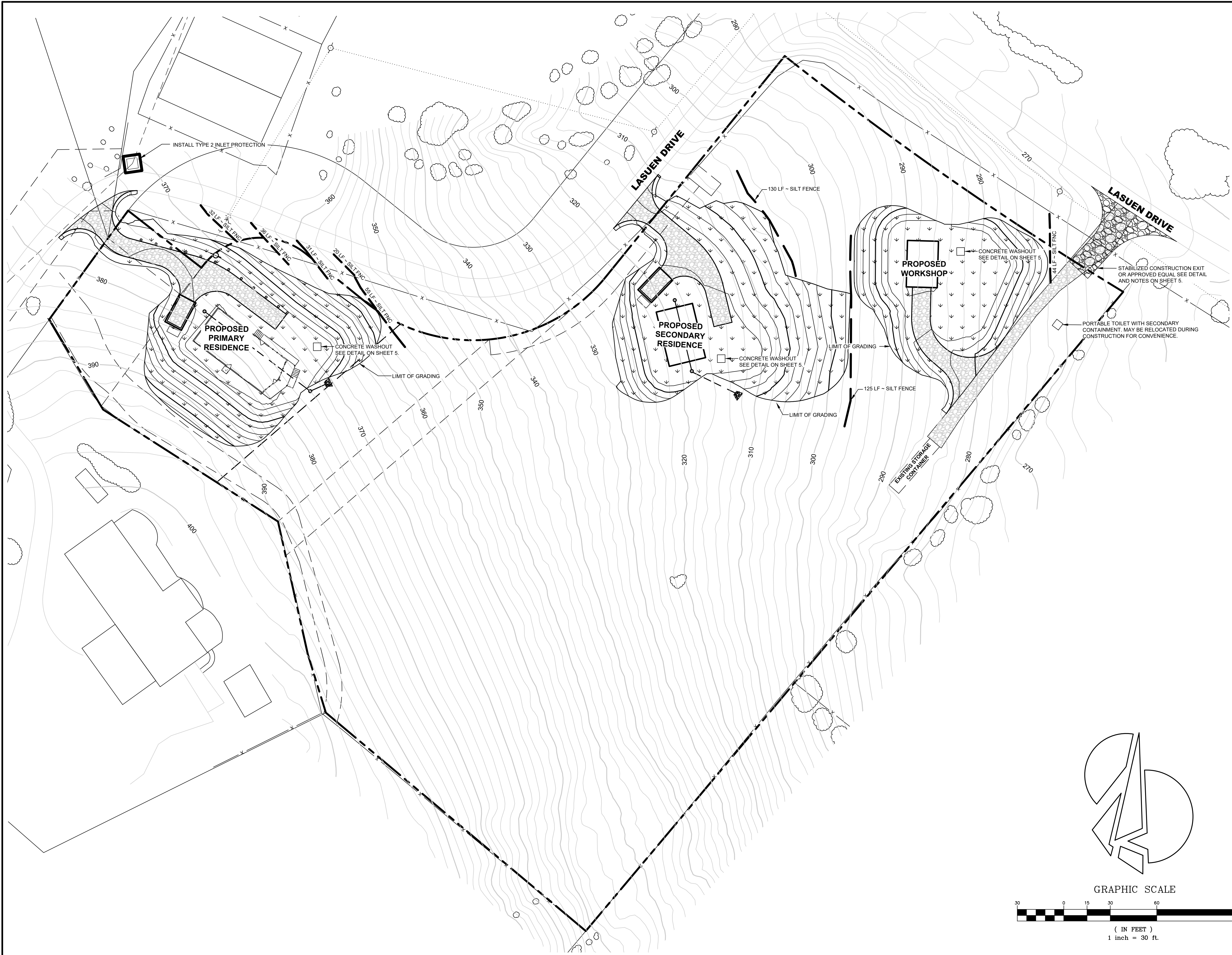


DATE: DECEMBER 2018  
SCALE: AS NOTED  
DESIGNED: MJK, TJK  
DRAWN: TJK  
JOB No.: 18031

**MCGOVERT**  
**CROSS SECTIONS**  
502 LASUEN DRIVE, SAN JUAN BAUTISTA

**SHEET**  
**3**  
OF 7





EROSION CONTROL LEGEND

- SILT FENCE**  
INSTALL SILT FENCE PROTECTION PER SE-1 AS SHOWN ON SHEET 5 TO PREVENT SEDIMENT FROM EXITING THE SITE TO REMAIN IN PLACE UNTIL VEGETATION IS ESTABLISHED.
- Revegetation**  
on any disturbed areas that are not pavement or aggregate base
- TYPE 2 STORM DRAIN INLET PROTECTION**  
INSTALL TYPE 2 STORM DRAIN INLET PROTECTION PER SE-10 ON SHEET 5 AT ALL EXISTING STORM DRAIN INLETS RECEIVING SURFACE RUNOFF FROM THE PROJECT SITE. TO REMAIN IN PLACE UNTIL VEGETATION IS ESTABLISHED.

REVEGETATION:

All disturbed areas are to be re-seeded according to the revegetation plan.

**REVEGETATION PLAN:**  
The work shall consist of hydro-seeding erosion control material consisting of a mixture of stabilizing emulsion, fiber, seed, commercial fertilizer, and water to embankment slopes and excavation slopes including obliterated roadway areas. Seed shall consist of the following:

Botanical Name (common name)	Percentage (minimum)	Pounds (minimum) Germination	Pounds per acre
Vulpia myuros (Zorro annual fescue)	80	90	10
Bromus rubens (Pancho red bromo)	95	85	10
Eriogonum fasciculatum (Duro Calif Buckwheat)	90	85	10

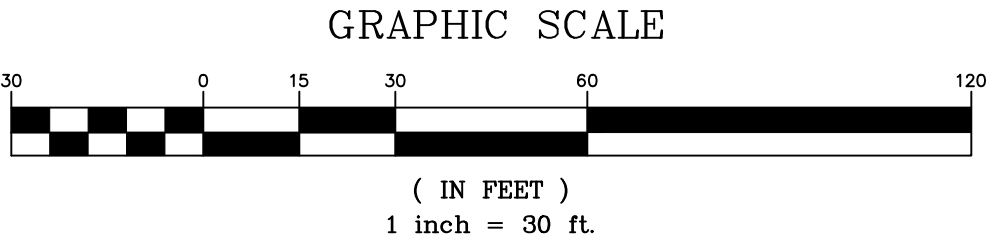
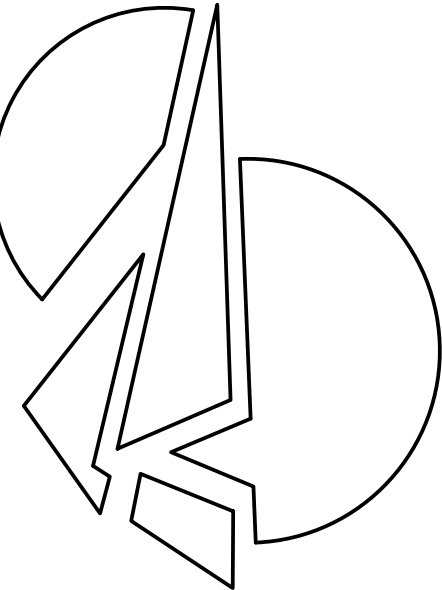
Seed shall be mixed on the project site in the presence of the engineer. Seed shall be applied at the rate of 30 pounds per acre (slope measurement) and fertilizer shall be applied at the rate of 300 pounds per acre (slope measurement). If hydro-seeding equipment is used to apply seed, the mixture shall be applied within 60 minutes after the seed has been added the mixture.

BMP NOTES

- PORTABLE SANITARY FACILITIES SHALL HAVE SECONDARY CONTAINMENT, AND BE LOCATED ON RELATIVELY LEVEL GROUND AWAY FROM TRAFFIC AREAS AND STORM DRAIN INLETS.
- THE CONTRACTOR SHALL NOTIFY THE CITY 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION TO REQUEST INSPECTION OF STORMWATER BMP'S. ALL STORMWATER BMP'S SHALL BE IN PLACE PRIOR TO THE START OF CONSTRUCTION, AND MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT.
- THE INTERIM BMP PLAN IS CONSIDERED A "LIVING DOCUMENT" WHICH MAY BE SUBJECT TO CHANGE FROM TIME TO TIME IN ORDER TO FACILITATE CONSTRUCTION. ALL REQUESTED CHANGES MUST BE APPROVED BY THE CITY OF SAN JUAN BAUTISTA PRIOR TO INSTALLATION.
- THE CONTRACTOR SHALL INSPECT ALL STORMWATER BMP'S REGULARLY TO ASSURE THAT THEY ARE FUNCTIONING PROPERLY. IF A BMP FAILS, THE CONTRACTOR SHALL MAKE REPAIRS IMMEDIATELY AND CLEAN ALL PORTIONS OF STORM DRAIN SYSTEMS THAT MAY HAVE BEEN CONTAMINATED BY THE FAILURE OF THE BMP TO THE SATISFACTION OF THE CITY OF SAN JUAN BAUTISTA.

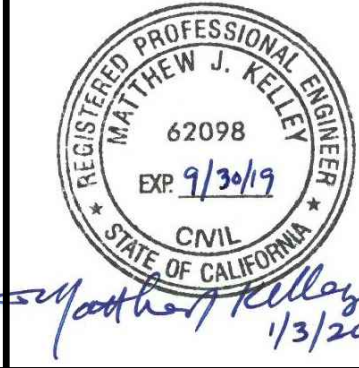
AREA OF DISTURBANCE:

PRIMARY RESIDENCE:	0.342 ACRES
SECONDARY RESIDENCE:	0.347 ACRES
WORKSHOP:	0.233 ACRES
TOTAL:	0.922 ACRES



△					
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△	BY	DATE	REVISIONS	APPR	

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OFFICE (831) 636-1104 FAX (831) 636-1837



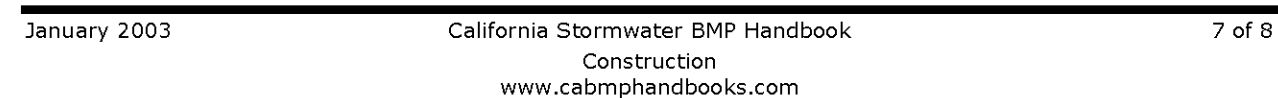
DATE:	DECEMBER 2018
SCALE:	1" = 30'
DESIGNED:	MJK, TJK
DRAWN:	TJK
JOB No.:	18031

**MCGOVERT  
EROSION CONTROL PLAN  
502 LASUEN DRIVE, SAN JUAN BAUTISTA**

**SHEET  
4  
OF 7**

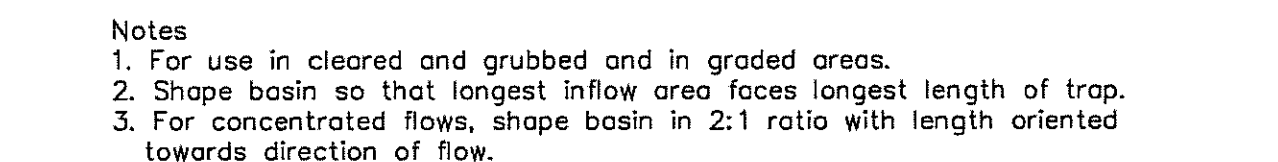


**SE-1**



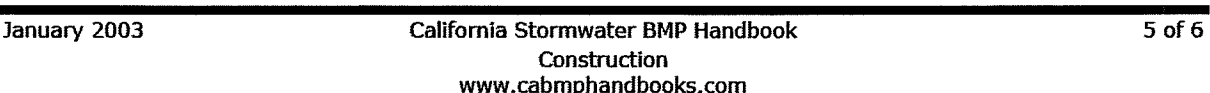
1. SPLIT FENCE FABRIC SHOULD BE WOVEN POLYPROPYLENE WITH A MINIMUM WIDTH OF 36 IN. AND A MINIMUM TENSILE STRENGTH OF 100 LB. FORCE. THE FABRIC SHOULD CONFORM TO THE REQUIREMENTS OF ASTM DESIGNATION D 1555-1. THE FABRIC SHOULD BE PROVIDED WITH AN OVERLAP OF 12 IN. AND A MINIMUM LAYER. THE REINFORCEMENT LAYER SHOULD BE A POLYPROPYLENE, OR EQUIVALENT, NET PROVIDED BY THE MANUFACTURER. THE PERMITTIVITY OF THE FABRIC SHOULD BE BETWEEN 0.15EC-1 AND 0.15EC-2 IN CONFORMANCE WITH THE REQUIREMENTS IN ASTM DESIGNATION D 1555-1.
2. WOOD STAKES SHOULD BE COMMERCIAL QUALITY LUMBER OF SIZE AND SHAPE SHOWN ON THE PLANS. EACH STAKE SHOULD BE FREE FROM DECAY, SPLITS, OR CRACKS LONGER THAN THE STAKE. STAKES SHOULD BE OF SUFFICIENT LENGTH THAT THEY WOULD WEAKEN THE STAKES AND CAUSE THE STAKES TO BE STRUCTURALLY UNSUITABLE.
3. STAPLES SHOULD BE USED TO FASTEN THE FENCE FABRIC TO THE STAKES SHOULD BE NOT LESS THAN 1/4 IN. LONG AND EXPOSED TO THE WEATHER. STAPLES SHOULD BE USED TO FASTEN THE WIRE USED TO FASTEN THE TOPS OF THE STAKES TOGETHER WHEN JOINING JOINTS TWO SECTIONS OF FENCE SHOULD BE 3 GAUGE OR HEAVIER WIRE. GALVANIZING OF THE FASTENING WIRE WILL BE REQUIRED.
4. THERE ARE NEW PRODUCTS THAT MAY USE PREFABRICATED PLASTIC HOLDERS FOR THE SPLIT FENCE AND USE BAR REINFORCEMENT INSTEAD OF WOOD STAKES. IF BAR REINFORCEMENT IS USED IN LIEU OF WOOD STAKES, USE NUMBER FOUR OR GREATER BAR. PROVIDE END

1. SILT FENCES ARE TO BE CONSTRUCTED ON A LEVEL CONTOUR. SUFFICIENT AREA SHOULD EXIST BEHIND THE FENCE FOR PONDING TO OCCUR WITHOUT FLOODING OR OVERTOPPING THE FENCE
2. A TRENCH SHOULD BE EXCAVATED APPROXIMATELY 6 IN. WIDE AND 6 IN. DEEP ALONG THE LINE. THE PROPOSED SILT FENCE BOTTOM OF THE SILT FENCE SHOULD BE KEPT IN A MINIMUM OF 1 IN.
3. POSTS SHOULD BE SPACED A MAXIMUM OF 6 FT. APART AND DRIVEN SECURELY INTO THE GROUND A MINIMUM OF 18 IN. OR 12 IN. BELOW THE BOTTOM OF THE TRENCH.
4. THE TRENCH SHOULD BE FILLED WITH SILT. THE SILT SHOULD BE PLACED IN A MESH SUPPORT FENCE. THE FENCE SHOULD BE FASTENED SECURELY TO THE UPSLOPE SIDE OF POSTS USING HEAVY-DUTY WIRE STAPLES AT LEAST 1 IN. LONG. THE MESH SHOULD EXTEND INTO THE TRENCH, WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED. THE MESH SUPPORT FABRIC SHOULD BE FASTENED TO THE UPSLOPE SIDE OF THE TRENCH, WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED. THE MESH SUPPORT FABRIC SHOULD BE SECURED TO THE LENGTH OF THE BARRIER. WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHOULD BE SPUNCE TOGETHER ONLY AT A SUPPORT POST, WITH A MINIMUM 6 IN. OVERLAP AND BOTH ENDS SECURELY FASTENED TO THE POST.
5. THE TRENCH SHOULD BE BACKFILLED WITH COMPACT NATIVE MATERIAL.
6. CONSTRUCT SILT FENCES WITH A SETBACK OF AT LEAST 3 FT. FROM THE TOE OF THE SLOPE. WHERE A SILT FENCE IS DETERMINED TO BE NOT PRACTICABLE DUE TO SPECIFIC SITE CONDITIONS, THE SILT FENCE MAY BE CONSTRUCTED AT THE TOE OF THE SLOPE, BUT SHOULD BE MAINTAINED AS FAR FROM THE TOE OF THE SLOPE AS PRACTICABLE AND AS NEAR AS POSSIBLE TO THE TOE OF THE SLOPE WILL BE LESS EFFECTIVE AND DIFFICULT TO MAINTAIN.
7. CONSTRUCT THE LENGTH OF EACH REACH SO THAT THE CHANGE IN BASE ELEVATION ALONG THE REACH DOES NOT EXCEED 1/3 THE HEIGHT OF THE BARRIER, IN NO CASE SHOULD THE REACH EXCEED 500 FT.



### STABILIZED CONSTRUCTION ENTRANCE/EXIT NOTES

1. SELECT A LEVEL GROUND SURFACE WHERE POSSIBLE.
2. CONSTRUCT 3 TO 6 IN. DIAMETER STONES.
3. SET MINIMUM DEPTH OF STONES 6 TO 12 IN. OR AS RECOMMENDED BY SOILS ENGINEER.
4. CONSTRUCT LENGTH OF 50 FT MINIMUM, AND 30 FT MINIMUM WIDTH.
5. PROVIDE AMPLE TURNING RADIUS AS PART OF THE ENTRANCE.
6. LIMIT THE POINTS OF ENTRANCE/EXIT TO ONE CONSTRUCTION SITE.
7. LIMIT SPEED OF VEHICLES TO CONTROL DUST.
8. PROPERLY GRADE EACH CONSTRUCTION ENTRANCE/EXIT TO PREVENT RUNOFF FROM LEAVING THE CONSTRUCTION SITE.
9. ROUTE RUNOFF FROM STABILIZED ENTRANCES/EXITS THROUGH A SEDIMENT TRAPPING DEVICE BEFORE LEAVING THE CONSTRUCTION SITE.
10. DESIGN STABILIZED ENTRANCE/EXIT TO SUPPORT HEAVIEST VEHICLES AND EQUIPMENT THAT WILL USE IT.
11. SELECT CONSTRUCTION ACCESS STABILIZATION (AGGREGATE, ASPHALTIC CONCRETE, CONCRETE) BASED ON THE TYPE OF TRAFFIC AND THE WEATHER AND SITE CONDITIONS. DO NOT USE ASPHALT CONCRETE (AC) GRINDINGS FOR STABILIZED CONSTRUCTION ACCESS/ROADWAY.
12. IF AGGREGATE IS SELECTED, PLAIN CRUSHED AGGREGATE OVER GEOTEXTILE FABRIC TO AT LEAST 12 IN. DEPTH IS REQUIRED. IF GEOTEXTILE FABRIC IS USED, IT MUST BE A GEOTEXTILE ENGINEER. A CRUSHED AGGREGATE GREATER THAN 3 IN. BUT SMALLER THAN 6 IN. SHOULD BE USED.
13. DESIGNATE COMBINATION OR SINGLE PURPOSE ENTRANCES AND EXITS TO THE CONSTRUCTION SITE. IDENTIFY AND MARK ALL ENTRANCES, SUBURROUNDS, AND SUPPLIERS UTILIZE THE STABILIZED CONSTRUCTION ACCESS.
14. IMPLEMENT SE-7, STREET SWEEPING AND VACUUMING, AS NEEDED.
15. CONSTRUCTION SHOULD BE USED TO BE USED FOR MORE THAN A TWO-WEEK PERIOD SHOULD HAVE STABILIZED CONSTRUCTION ENTRANCE/EXIT BMPs.
16. INSPECT AND MAINTAIN BMPs. BMPs ARE REQUIRED TO BE USED TO THE COMMENCEMENT OF ASSOCIATED ACTIVITIES, WHILE ACTIVITIES ASSOCIATED WITH THE BMPs ARE UNDER WAY. INSPECT WEEKLY DURING THE RAINY SEASON AND OF TWO-WEEK INTERVALS IN THE NON-RAIN SEASON TO VERIFY THE BMP IS MAINTAINED.
17. INSPECT LOCAL ROADS ADJACENT TO THE SITE DAILY. SWEEP OR VACUUM TO REMOVE VISIBLY ACCUMULATED SEDIMENT.
18. CHECK AGGREGATE, SEPARATE AND DISPOSE OF SEDIMENT IF CONSTRUCTION ENTRANCE/EXIT IS CLOGGED WITH SEDIMENT.
19. KEEP ALL TEMPORARY ROADWAY DITCHES CLEAR.
20. CHECK FOR DAMAGE AND REPAIR AS NEEDED.
21. REPLACE GRAVEL MATERIAL WHEN SURFACE VOIDS ARE VISIBLE.
22. REMOVE ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS WITHIN 24 HOURS. REMOVE GRAVEL AND FILTER.



The image contains two sets of technical drawings for concrete washout facilities, labeled 'PLAN' and 'SECTION A-A' for the 'Below Grade' type, and 'PLAN' and 'SECTION B-B' for the 'Above Grade' type.

**Below Grade Type (Left):**

- PLAN:** A square layout with a minimum side length of 10 feet. It shows a central square area labeled 'A' surrounded by a berm. The perimeter is lined with 10 mil plastic lining. A lath and flagging are shown on all sides. A sandbag is placed at the corner of the berm. The height of the berm is labeled 'VARIES'.
- SECTION A-A:** A cross-section showing the 10 mil plastic lining, a sandbag, and a berm. The height of the berm is labeled '3'.

**Above Grade Type (Right):**

- PLAN:** A square layout with a minimum side length of 10 feet. It shows a central square area labeled 'B' surrounded by a berm. The perimeter is lined with 10 mil plastic lining. A straw bale is placed at the corner of the berm. The height of the berm is labeled 'VARIES'.
- SECTION B-B:** A cross-section showing the 10 mil plastic lining, a straw bale, and a berm. The height of the berm is labeled '3'.

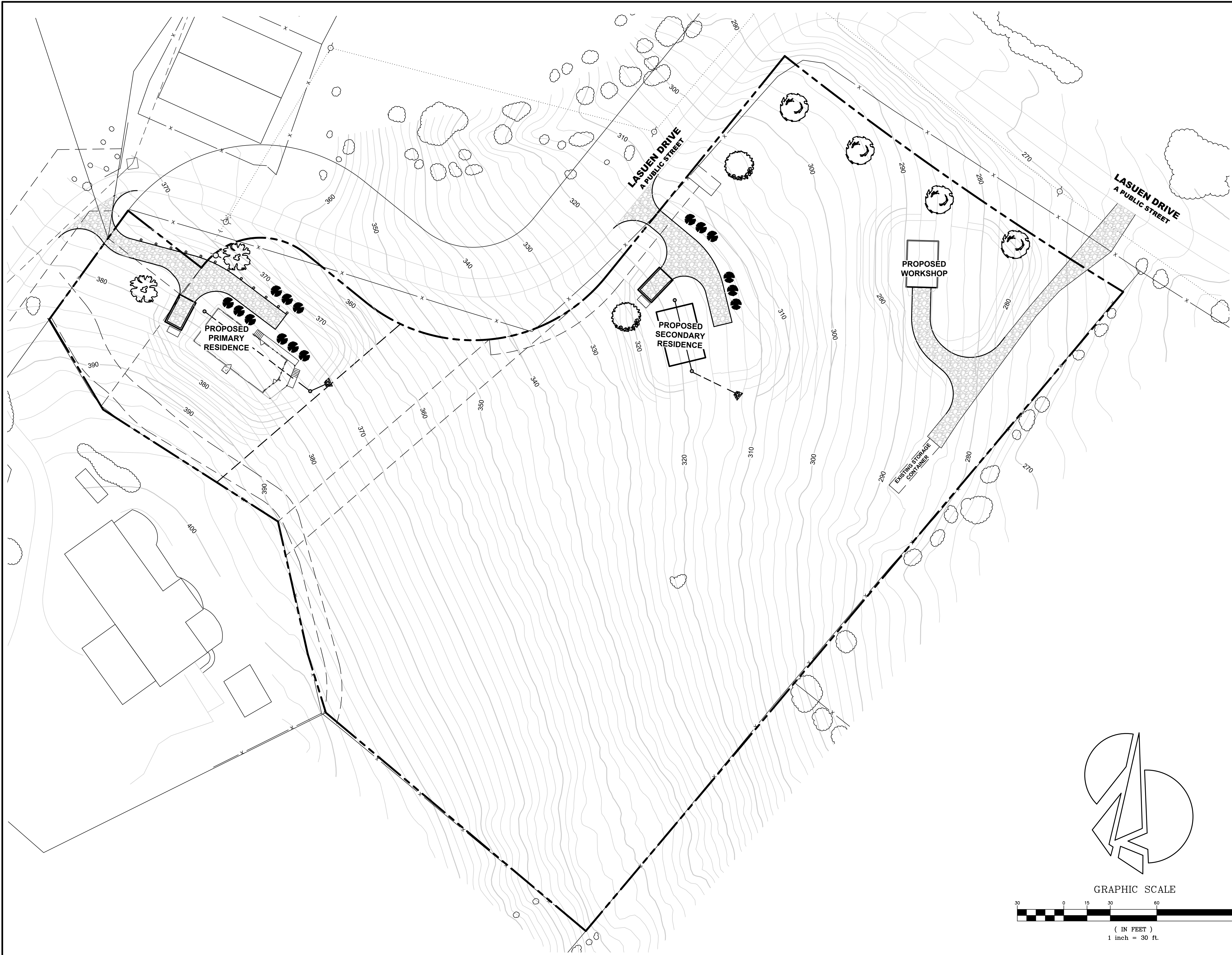
**Concrete Washout Sign Details:**

- Concrete Washout Sign Detail (Left):** A sign with a minimum height of 6 feet, made of plywood (48" x 24") painted white. It is supported by two 3" x 3" x 8' wood posts. The sign is secured with 0.5" lag screws. The sign is labeled 'CONCRETE WASHOUT'.
- Concrete Washout Sign Detail (Right):** A sign with a minimum height of 6 feet, made of plywood (48" x 24") painted white. It is supported by two 3" x 3" x 8' wood posts. The sign is secured with 0.5" lag screws. The sign is labeled 'CONCRETE WASHOUT'.

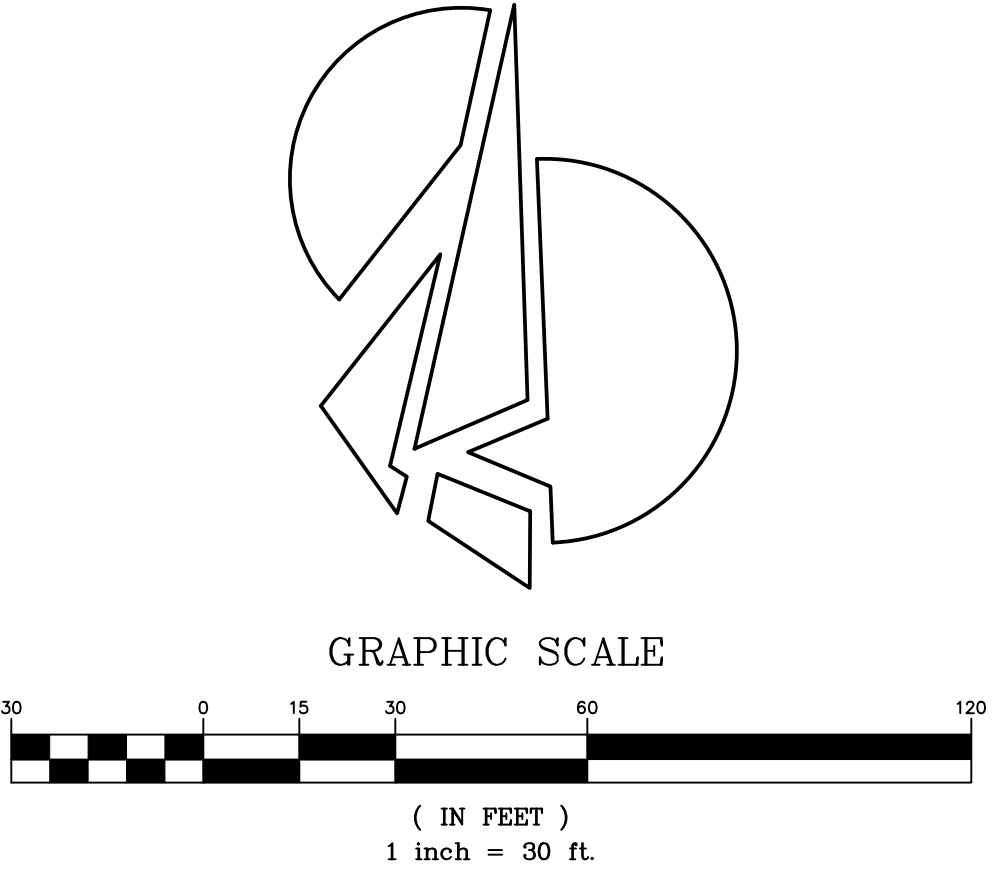
**Notes:**

1. ACTUAL LAYOUT DETERMINED IN FIELD.
2. THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 30 FT. OF THE TEMPORARY CONCRETE WASHOUT FACILITY.



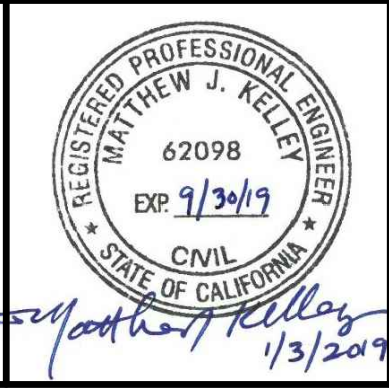


PLANT LEGEND	
	LAVENDER GROSSO "ENGLISH LAVENDER" - 1 GAL.
	MORUS RUBRA "RED MULBERRY" - 5 GAL.
	MALUS SYLVESTRIS "CRABAPPLE" - 5 GAL.
	CORNUS FLORIDA "FLOWERING DOGWOOD" - 5 GAL.



BY	DATE	REVISIONS	APPR

**KELLEY**  
**ENGINEERING & SURVEYING**  
400 PARK CENTER DRIVE, SUITE #4  
HOLLISTER, CA 95023  
OFFICE (831) 636-1104 FAX (831) 636-1837



DATE:	DECEMBER 2018
SCALE:	1" = 30'
DESIGNED:	MJK, TJK
DRAWN:	TJK
JOB No.:	18031

**MCGOVERT**  
**PRELIMINARY LANDSCAPE PLAN**  
**502 LASUEN DRIVE, SAN JUAN BAUTISTA**

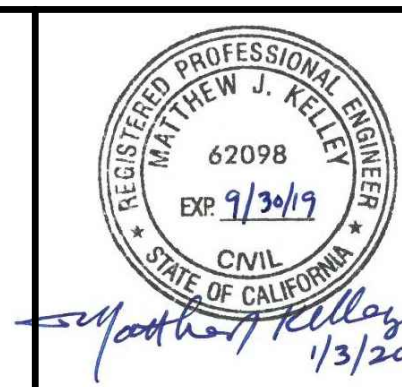
**SHEET**  
**6**  
OF 7





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△	BY	DATE	REVISIONS	APPR	

**KELLEY**  
**ENGINEERING & SURVEYING**  
400 PARK CENTER DRIVE, SUITE #4  
HOLLISTER, CA 95023  
OFFICE (831) 636-1104 FAX (831) 636-1837



DATE:	DECEMBER 2018
SCALE:	1" = 30'
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DRAWN:	TJK
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**MCGOVERT**  
**HIGHWAY 156 VIEWSHED EXHIBIT**  
**502 LASUEN DRIVE, SAN JUAN BAUTISTA**

**SHEET**  
**7**  
**OF 7**



## CITY OF SAN JUAN BAUTISTA

### PLANNING COMMISSION

#### STAFF REPORT

**DATE:** February 5, 2019

**PROJECT:** Conditional Use Permit Request for Hacienda de Leal to allow the on-site sale of Alcoholic Beverages and Caterer's Permit

#### **DISCUSSION:**

This item is being brought forward as requested by the Hacienda de Leal located at 410 The Alameda. The APN is 002-520-013. This application was received by Planning Staff by mail. The Zoning Classification on site is Commercial (C). A Conditional Use Permit is required in order to sell alcoholic beverages onsite in that zoning district.

This case was presented to Planning Commission as a discussion item at their hearing on December 4, 2018. Since that time, staff has received a request for a Conditional Use Permit Application for the sale of alcoholic beverages.

After research and due diligence, there was an approval on December 4, 2014 by Resolution 2014-30. This resolution included the approval of the preparation of an environmental document. Discussion on a liquor license application took place at the Planning Commission hearing on March 3, 2015. It was directed to the applicant to apply for a Conditional Use Permit, Environmental Review, and Site and Design Review per the minutes. That was the last time this property was discussed at Planning Commission based on records found by staff.

At that time, the proposal was to expand the subject property by including a banquet hall and other uses. At this time, that proposal no longer applies. There is not a proposal for expansion with this application. There is an existing outdoor patio and an internal café inside the lobby. Outdoor dining as an accessory use to a restaurant requires a Conditional Use Permit. There is no record a Conditional Use Permit has been applied for that use.

**ACTION:**

Staff has no recommendation. It is recommended that a Conditional Use Permit be filed with the City and approved before the ABC license is signed off by the City bringing the site into compliance.

**Attached:**

Resolution 2019-XX

Site Photos

ABC License Application Form

ABC License Types

Resolution 2014-30

March 3, 2015 Meeting Minutes

## RESOLUTION 2019 - xx

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SAN JUAN BAUTISTA APPROVING A CONDITIONAL USE PERMIT REQUEST FOR AN EXISTING HOTEL (HACIENDA DE LEAL) TO SELL ALCOHOLIC BEVERAGES ONSITE AT 410 THE ALAMEDA IN SAN JUAN BAUTISTA.  
APN 002-520-013**

**WHEREAS**, the Planning Commission conducted a public hearing on February 5, 2019 to review the Conditional Use Permit Request, and

**WHEREAS**, the Planning Commission received oral comments, written correspondence, electronic communication and public testimony at the public hearing, and

**WHEREAS**, the Planning Commission reviewed the staff report for the project, and

**WHEREAS**, the Planning Commission has considered the recommendation from City Staff and makes the following finding to approve the Conditional Use Permit request.

1. That the proposed Conditional Use Permit request is compatible with the existing use and that it does not have any impacts on the character of the property and buildings.

**NOW, THEREFORE, BE IT RESOLVED**, that the Planning Commission of the City of San Juan Bautista hereby approves the Conditional Use Permit request for 410 The Alameda subject to the following conditions.

1. This approval is contingent on the approval of an additional Conditional Use Permit to allow the outdoor dining area as an accessory use to the existing restaurant.

**PASSED AND ADOPTED** by the Planning Commission of the City of San Juan Bautista at a regular meeting held on February 5, 2019 by the following vote:

**AYES:**

**NOES:**

**ABSENT:**

**ABSTAIN:**



---

Scott Freels, Chairperson

ATTEST:

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Trish Paetz, Deputy City Clerk











Department of Alcoholic Beverage Control

State of California

## APPLICATION FOR ALCOHOLIC BEVERAGE LICENSE(S)

AB 620

TO: Department of Alcoholic Beverage Control  
1137 WESTRIDGE PARKWAY  
SALINAS, CA 93907  
(831) 755-1990

File Number: 597564  
Receipt Number: 2539740  
Geographical Code: 3502  
Copies Mailed Date: November 15, 2018  
Issued Date:

DISTRICT SERVING LOCATION: SALINASFirst Owner: HACIENDA DE LEAL LLCName of Business: HACIENDA DE LEAL

Location of Business: 410 THE ALAMEDA  
SAN JUAN BAUTISTA, CA 95045-9547

County: SAN BENITOIs Premise inside city limits? Yes Census Tract 0002.00

Mailing Address: 300 MARANATHA DR  
(If different from HOLLISTER, CA 95023  
premises address)

Type of license(s): 47, 58

Transferor's license name:

Dropping Partner: Yes      No   X  

License Type	Transaction Type	Fee Type	Master	Dup	Date	Fee
47 - On-Sale General Eating	ANNUAL FEE	Pr	Y	0	11-15-18	\$637.00
47 - On-Sale General Eating	OPTIONAL FEES	NA	Y	0	09-10-18	\$15,384.00
58 - Chapter Partner	DUPLICATE SECONDARY	NA	N	1	11-15-18	\$142.00
Total						\$16,163.00

Have you ever been convicted of a felony? No

Have you ever violated any provisions of the Alcoholic Beverage Control Act, or regulations of the  
Department pertaining to the Act? No

Explain any "Yes" answer to the above questions on an attachment which shall be deemed part of this application.

Applicant agrees (a) that any manager employed in an on-sale licensed premises will have all the qualifications of a licensee, and (b) that he will not violate or cause or permit to be violated any of the provisions of the Alcoholic Beverage Control Act.

STATE OF CALIFORNIA County of SAN BENITODate: September 10, 2018

Under penalty of perjury, each person whose signature appears below, certifies and swears: (1) He is an applicant, or one of the applicants, or an executive officer of the applicant corporation, named in the foregoing application, duly authorized to make this application on its behalf; (2) that he has read the foregoing and knows the contents thereof and that each of the above statement therein made are true; (3) that no person other than the applicant or applicant, has any direct or indirect interest in the applicant or applicant's business to be conducted under the license(s) for which this application is made; (4) that the transfer application or proposed transfer is not made to satisfy the payment of a loan or to fulfill an agreement entered into more than ninety (90) days preceding the date on which the transfer application is filed with the Department or to gain or establish a preference to or for any creditor or transferee or to defraud or injure any creditor of transferor; (5) that the transfer application may be withdrawn by either the applicant or the licensee with no resulting liability to the Department.

Effective July 1, 2012, Revenue and Taxation Code Section 7057, authorizes the State Board of Equalization and the Franchise Tax Board to share taxpayer information with Department of Alcoholic Beverage Control. The Department may suspend, revoke, and refuse to issue a license if the licensee's name appears in the 500 largest tax delinquencies list. (Business and Professions Code Section 494.5.)

Applicant Name(s)

Applicant Signature(s)

See 211 Signature Page

HACIENDA DE LEAL LLC



California Department of  
**Alcoholic Beverage Control**

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## List Of License Types

### TEMPORARY LICENSES

License Code	Description of License
30	Temporary Retail Permit (Only in conjunction with a Person to Person transfer)

### NON-PROFIT TEMPORARY LICENSES

License Code	Description of License
31	Special Daily License (Temporary Beer or Wine)
32	Daily Beer
33	Daily Wine
34	Daily Beer and Wine
37	Daily On-Sale General

### OFF-SALE LICENSES

License Code	Description of License
20	Off-Sale Beer and Wine
21	Off-Sale General
85	Limited Off-Sale Retail Wine License

### ON-SALE LICENSES

License Code	Description of License
40	On-Sale Beer
41	On-Sale Beer and Wine for Bona Fide Public Eating Place
42	On-Sale Beer and Wine for Public Premises
43	On-Sale Beer and Wine for Train
44	On-Sale Beer for Fishing Party Boat
45	On-Sale Beer and Wine for Boat
46	On-Sale Beer and Wine for Airplane
47	<u>On-Sale General for Bona Fide Public Eating Place</u>
48	On-Sale General for Public Premises
49	On-Sale General for Seasonal Business
50	On-Sale General for Club
51	Club
52	Veterans' Club
53	On-Sale General for Train
54	On-Sale General for Boat
55	On-Sale General for Airplane
56	On-Sale General for Vessel of more than 1,000 tons burden
57	Special On-Sale General
58	<u>Caterer's Permit</u>
59	On-Sale Beer and Wine for Seasonal Business
60	On-Sale Beer for Seasonal Business
61	On-Sale Beer for Public Premises
62	On-Sale General Bona Fide Public Eating Place Intermittent Dockside Vessel
63	On-Sale Beer and Wine for Hospital
64	Special On-Sale General Theatre
65	Special On-Sale Beer and Wine, Symphony
66	Controlled Access Cabinet Permit
67	Bed and Breakfast Inn
68	Portable Bar
69	Special On-Sale Beer & Wine Theater
70	On-Sale General Restrictive Service
71	Special On-Sale General License
75	On-Sale General Brew-Pub

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[Precedential Decisions](#)  
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[Public Records Requests](#)  
[TRACE Hotline \(law enforcement only\)](#)



### Licensing

[ABC Licensing](#)  
[License Types](#)



**RESOLUTION 2014-30**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SAN  
JUAN BAUTISTA APPROVING THE PREPARATION OF A MITIGATED  
NEGATIVE DECLARATION FOR THE SITE PLAN AND DESIGN REVIEW FOR  
HACIENDA de LEAL PROJECT (DR 2014-1201) AT 410 THE ALAMEDA  
(APN 002-520-013)**

**WHEREAS**, the Planning Commission of the City of San Juan Bautista conducted a public hearing on December 4, 2014 for an application by Hacienda de Leal to construct a 11,250 square feet banquet building and a 3,928 square feet two story multiple personal care building at 410 The Alameda, and

**WHEREAS**, the Planning Commission received oral testimony, written comments, electronic communication, public input, staff report and information from the California Environmental Quality Act, and

**WHEREAS**, the Planning Commission reviewed the staff report, Initial Study and Mandatory Findings of Significance within the Initial Study that identified potential significant impacts of the proposed project, and

**WHEREAS**, the Planning Commission reviewed and considered the specific areas that could have a potential significant impact to the immediate area and would require some type of mitigation that would reduce the impacts to less than significant, and

**WHEREAS**, the Planning Commission determined that together with the oral testimony, public input, and information on the initial study and elements of the California Environmental Quality Act and, in light of the whole record before the Planning Commission, the proposed project will require a mitigated negative declaration to address the areas of potential impact and proposed mitigations to reduce the impacts to less than significant.

**NOW, THEREFORE BE IT RESOLVED** that the Planning Commission of the City of San Juan Bautista hereby finds that a mitigated negative declaration is required for processing the Site Plan and Design Review application for the Hacienda de Leal.

1. That the Planning Commission has independently reviewed and analyzed the initial study and checklist for the project.



2. That the Planning Commission determines that a mitigated negative declaration is necessary for compliance with the California Environmental Quality Act.
3. That the Planning Commission requires the applicant to prepare a mitigated negative declaration for the Hacienda de Leal proposed project addressing the impacts enumerated in the staff report and specified by the Planning Commission.

**PASSED AND ADOPTED BY** the Planning Commission of the City of San Juan Bautista on December 4, 2014 by the following vote.

**AYES:**            **Garratt, Gularte, Guibert, Medeiros, Franco**

**NOES:**           **None**

**ABSENT:**        **None**

**ABSTAIN:**       **None**



Vice Chairperson David Medeiros

ATTEST:



Connie Schobert, City Clerk



**CITY OF SAN JUAN BAUTISTA  
PLANNING COMMISSION REGULAR MEETING  
MARCH 3, 2014  
MINUTES**

**1. CALL TO ORDER** – Chairman Franco called the meeting to order at 6:15 p.m.

**B. ROLL CALL**

**Present:** Chairman Franco, Commissioners Medeiros and Guibert

**Absent:** Commissioner Garratt

**Staff Present:** City Manager Grimsley, Deputy City Clerk Paetz, City Clerk Schobert and Asst. Planner Orbach

**2. PUBLIC COMMENT ON ITEMS NOT ON THE AGENDA**

No comments were received.

**3. INFORMAL PROJECT REVIEW**

No projects were presented.

**4. CONSENT ITEMS**

**A. Approve Affidavit of Posting Agenda**

**B. Approve Affidavit of Posting Public Hearing Notice**

**C. Approve Minutes for November 5, 2014 Meeting**

Chairman Franco suggested continuing the minutes as Dave Medeiros was absent from the November 5, 2014 meeting.

A motion was made by Vice Chairman Medeiros and seconded by Commissioner Guibert to approve Items A and B on the consent agenda. The motion passed 3-0-0-1 with Commissioner Garratt absent.

**5. PUBLIC HEARING ITEMS**

**A. Consider a Conditional Use Permit for Hacienda de Leal allowing outdoor dining as an accessory use in the court yard and patio area, Section 11-04-030 (B) SJBMC (Continue public hearing)**

Assistant Planner Orbach presented his findings after considerable investigation into the history of the property. He determined that since the original CUP was authorized in 1978, the parcel was split, and the zoning code over several years is not clear.

Chairman Franco opened the public hearing. Bob Lund (SJB Mayor and Resident) was concerned with the zoning for this property. There was no other public comment.

Chairman Franco closed the public hearing.



The applicant, Frank Leal, was present and insisted the Commission authorize his request for a liquor license tonight as he would not be coming back before the Planning Commission. Chairman Franco informed the applicant that a conditional use permit is required in order to have a restaurant on the property, and that involves CEQA and an initial study. No action was taken.

**B. Consider Resolution 2015-17 Adopting a Notice of Exemption for a Minor Lot Division and Construct a New City Water Well No. 4, 150 ft. north of San Juan-Hollister Road and 1,100 ft. west of the intersection of Mission Vineyard Road and San Juan-Hollister Road**

City Manager Grimsley presented a report stating the project consists of a minor lot division to create a 0.224 acre parcel of land lying northerly and easterly of the west property line of APN 002-550-005 for the drilling and construction of a new water well No. 4.

A motion was made by Vice Chairman Medeiros and seconded by Commissioner Guibert to adopt Resolution 2015-17 adopting a notice of exemption for a minor lot division and construct a new city water well no. 4. The motion passed 3-0-0-1 with Commissioner Garratt absent.

**C. Consider Resolution 2015-18 Adopting a Notice of Exemption for a Minor Lot Division and Replacement of Existing City Water Well No. 2, 350 ft. north of San Juan-Hollister Road and 1,100 ft. east of the intersection of the Alameda and San Juan-Hollister Road.**

City Manager Grimsley presented a report stating the project is to construct a new water well at the site of the City's existing Well No. 2 which is substandard, situated on an 11.98 acre parcel of land (APN 012-130-031) lying northerly of San Juan-Hollister Road approximately 1,100 feet easterly of the intersection of The Alameda and San Juan-Hollister Road. The existing well is located on a 20' x 20' site and has a substandard annular sanitary seal of 36 feet. The new well site will have a minimum site of 100' x 100'. The well will be constructed to a depth of 500 feet with an annular sanitary seal of approximately 225 feet.

A motion was made by Vice Chairman Medeiros and seconded by Commissioner Guibert to adopt Resolution 2015-18 adopting a notice of exemption for a minor lot division and replacement of existing City water Well No. 2. The motion passed 3-0-0-1 with Commissioner Garratt absent.

**6. ACTION ITEMS**

**A. Consider Request for a Sign Permit for Golden Wheel Antiques at 407 Third Street. Applicant: Tony Cornaggia**

The applicant was not present and there was no recommendation from the Historic Resources Board. This item was continued.



### **B. Harvey's Lockup Compliance List Review**

City Manager Grimsley presented an itemized checklist from the Conditional Use Permit for Harvey's Lockup stating they need to install a sidewalk along Third Street. Property owner Steve Adair was present and stated installation of a sidewalk is planned for this spring or summer. This fire sprinkler system for the Monterey Street side and Second Street side will begin this month. During public comment Eric Gredassoff (SJB Resident) spoke in support of the sidewalk installation, but was concerned with the number of cats on the property in the past three to four months, and with trailers hooked up with power to the building. Mr. Adair explained that he allowed a contractor to use electricity, but then was asked to disconnect it.

### **C. Review Landscaping Plan for Brewery Project**

City Manager Grimsley provided a list and description of approved City trees dating back to 1998, and suggested a new list be developed. During public comment Mirijana Tomas (SJB Resident) encouraged the commission to take time to approve the trees for the project as the city already has many trees that are messy and dangerous. Georgana Gularte (SJB Resident) suggested including trees on the list that are not susceptible to mistletoe. Karen Gredassoff (SJB Resident) commented that pepper trees are messy and have shallow roots.

## **7. DISCUSSION ITEMS**

### **A. General Plan EIR Update**

Assistant Planner Orbach reported on a public meeting held February 18 to solicit feedback from the community regarding Draft #4 of the 2035 General Plan and its supplemental background report. The meeting also served as a scoping meeting for the EIR that is the environmental impact report. During public comment John Freeman (SJB Resident) encouraged developments provide the conduit for high speed fiber optics and for hooking up to purple water line.

### **B. Wayfinding Update**

Assistant Planner Orbach reported there was nothing new to report but advised Wayfinding should be consistent throughout the county.

### **C. Secondary Unit: SJBMC Section 11-04-050 (B) (9)**

City Manager Grimsley reported he has reviewed Municipal Code Section 11-04-050 (B)(9), suggested it be amended, and asked for direction. During public comment Mirijana Tomas (SJB Resident) encouraged the building of secondary housing units to provide affordable housing for retirees and students. A question and answer period followed. No action was taken.

### **D. Fencing: SJBMC Section 11-04-070 (J)**

City Manager Grimsley provided Commissioners with Section 11-04-070 (J). A question and answer period followed. No action was taken.



**E. Minimum Lot Size for Affordable Housing Need**

City Manager Grimsley provided a report. A question and answer period followed. No action was taken.

**F. Strategic Planning Committee Report**

Commissioner Medeiros provided a report on the projects the Strategic Planning Committee has been working on.

**8. FUTURE AGENDA ITEMS**

Assistant Planner Orbach will be bringing back a matrix in the zoning code dealing with conditional use permits.

**8. COMMENTS**

**A. Planning Commissioners**

No comments were received.

**B. City Manager/Asst. Planner**

City Manager Grimsley announced Assistant Planner Matt Leal will not be coming back to the City as he has taken a position with CalTrans in San Luis Obispo.

**9. ADJOURN**

The meeting adjourned at 8:02 p.m.

  
Chairman Ernest J. Franco

ATTEST:

  
Trish Paetz, Secretary





## CITY OF SAN JUAN BAUTISTA PLANNING COMMISSION STAFF REPORT

AGENDA TITLE: Approve Replacing Rancho Vista Development Roundabout at Lavagnino Drive and First Street with a 3-way Stop Controlled Intersection.

MEETING DATE: February 5, 2019

DEPARTMENT HEAD: City Manager

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**RECOMMENDED ACTION:** Approve replacing the Rancho Vista Roundabout with 3-way stop intersection and utilize the remainder funds for other critical capital improvements within the City of San Juan Bautista.

**BACKGROUND INFORMATION:** The Rancho Vista Development project includes a design of a roundabout at the intersection of Lavagnino and San Juan Highway (First Street). The roundabout was designed back in 2017 and approved by the previous City Manager/City Engineer in June 2017. The roundabout shown on these approved plans does not take into account the truck traffic received along First Street. Although truck traffic along First Street is prohibited and the City has posted signage warning commercial trucks over 3 tons to re-route away from the City, many trucks simply ignore these signs. Therefore, the City Manager and City Engineer requested that Meritage Homes redesign the roundabout to allow for truck traffic along this roadway to allow safe passage through the roundabout without getting “stuck”, as these trucks can be as much as 65 feet in length, and create a safety hazard for other drivers and bicyclists. Refer to Attachments 1A-1C for the truck routes and diagram classifying truck sizes.

Meritage Homes contracted with Advanced Mobility Group (AMG) to analyze the roundabout and provide alternative traffic control at the subject intersection. The Traffic Analysis is included in Attachment 2 for your reference.

Operational analysis included the following three traffic control options.

1. Roundabout
2. All-Way Stop Control
3. One-Way Stop Control

It should be noted that each traffic control option results in level of service (LOS) operations with less than 10 seconds delay for vehicles.

The report indicates the current design of the roundabout with inscribed circle diameter of approximately 85-feet would only accommodate a SU-30 typical design vehicle (see attachment 1C for vehicle diagram), which would be insufficient for existing truck traffic navigating through



the City and for future traffic conditions. The 2035 San Benito County General Plan identifies San Juan Highway (First Street) as an Arterial road, expected to carry truck traffic, requiring a larger roundabout. AMG's recommendation for a single lane roundabout installed at the intersection is to have an inscribed circle diameter of at least 105 feet to accommodate large truck traffic. The existing approved roundabout inscribed circle diameter would need to be increase by approximately 20 feet (from 85 to 105 feet).

VVH Consulting Engineers provided City staff a revised roundabout concept with a 105 feet inscribed circle diameter (See attachment 3C). The new layout encroaches further into the future development to the east, towards Loyaza's future planned development (Attachment 3A). The Loyaza's strongly oppose the larger roundabout concept and are not willing to dedicate the additional right of way required for the construction of the larger roundabout.

Upon review of these constraints, City staff recommends that the Lavagnino Drive and First Street intersection should operate with an All-Way(3-Way) Stop Control (See attachment 3D). The existing All-Way stop at Ahwahnee Street would be eliminated (the two existing stops would be removed as shown in attachment 3C). City staff would request a cost estimate for each option, the roundabout and All-Way stop, to be submitted by the developer in order to determine the "credit" to the City for not constructing the original roundabout design.

City staff recommends using the credit for the following options:

1. Install Traffic Safety Device (e.g., Radar Speed Sign) on San Juan Highway
2. Welcome entry monument sign.
3. Beautify the Intersection of Lavagnino and First Street by installing colored "stamped" concrete with City Logo.
4. Keep the credit to fund other critical projects in the City.

**FISCAL IMPACT:** Reminder Funding for this resolution would be added for the Capital Improvement Program or other critical community services.

**ATTACHMENTS:**

1. A. 2035 San Benito County General Plan Figure 6-2 Circulation Diagram Hollister and San Juan Bautista Roadway Classifications  
B. San Benito Bikeway and Pedestrian Master Plan Figure D San Juan Bautista Existing and Proposed Bikeways  
C. Vehicle Classifications
2. Advanced Mobility Group "Rancho Vista Project Access Intersection Review Memorandum" Report (dated November 19, 2018).
3. A. Loyaza's Future Planned Development  
B. Roundabout Exhibit (Inscribed Circle of 85 feet)  
C. Revised Roundabout Exhibit (Inscribed Circle of 105 feet )  
D. All-Way Stop Intersection Exhibit (by VVH Consulting Engineers).
4. Draft City Council Resolution



# Attachment 1A

## Rancho Vista Ranch Project

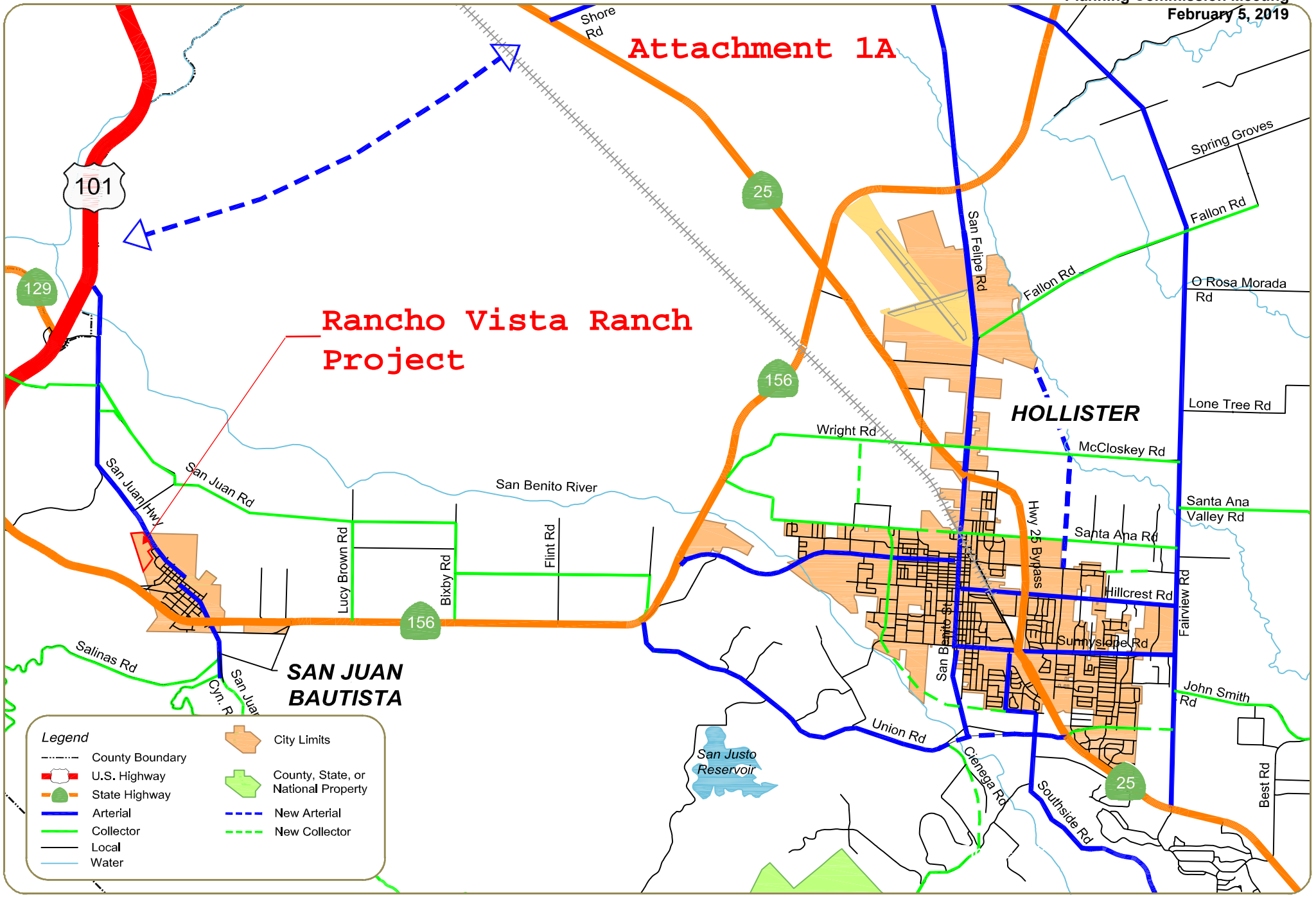
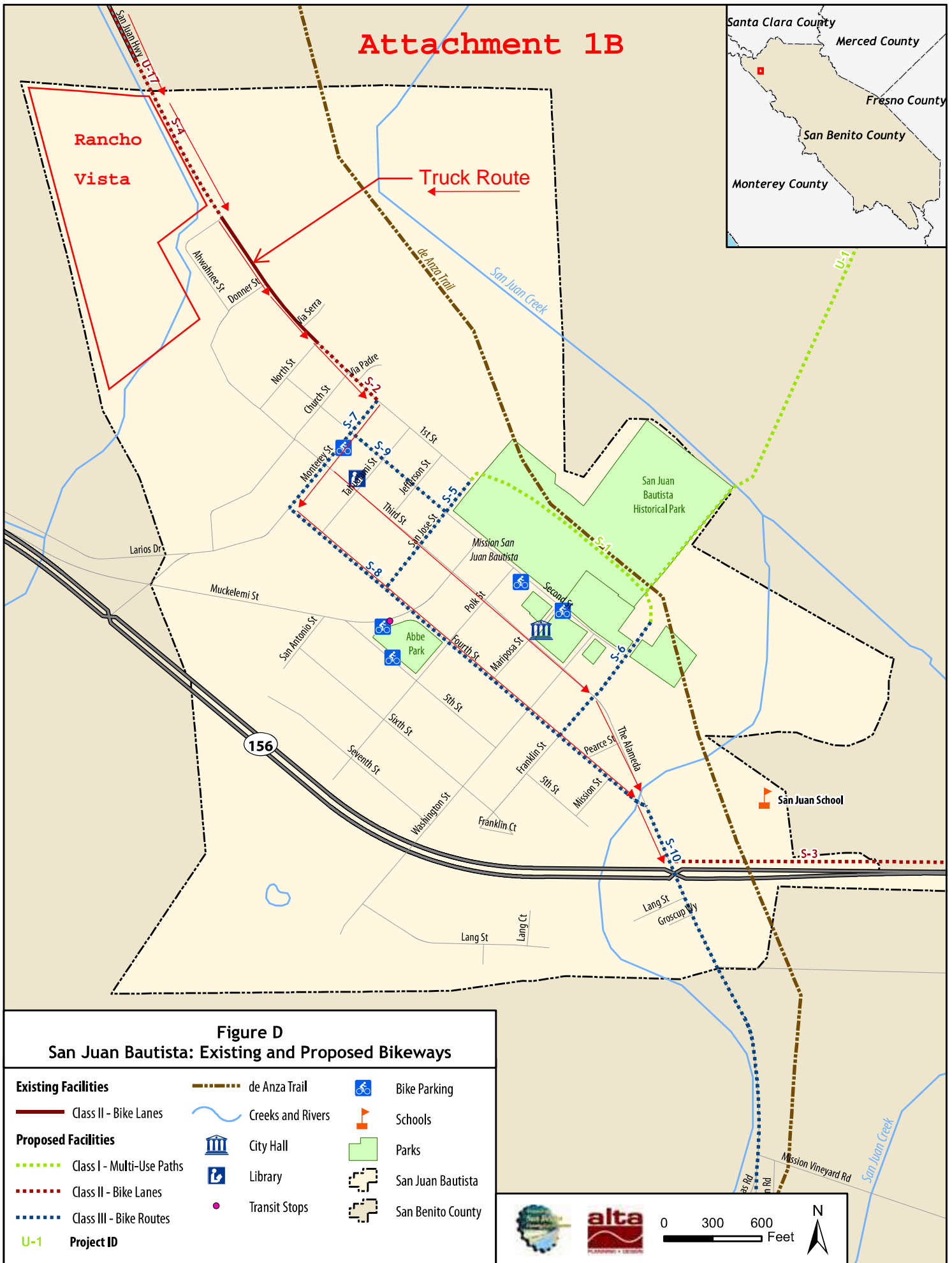


Figure 6-2  
Circulation Diagram (Hollister and San Juan Bautista Roadway Classifications)



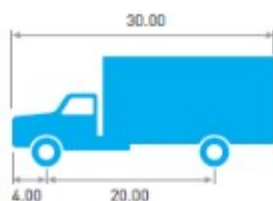
# Attachment 1B



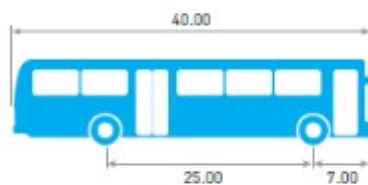


# Attachment 1C

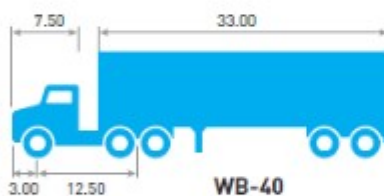
*Last Updated: 9-1-2016*



**SU-30**



**CITY-BUS**



**WB-40**



**WB-50**



**WB-67**



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<b>To:</b> Brian Veitch, PE, QSD/P Senior Civil Engineer  VVH Consulting Engineers 430 10th Street Modesto, CA 95354	<b>From:</b> Joy Bhattacharya, PE, PTOE & Chris Higbee, PE  Advanced Mobility Group 2999 Oak Road Suite 420 Walnut Creek, CA 94597  <b>Date:</b> November 19, 2018
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**Reference: Rancho Vista Project Access Intersection Review Memorandum**

This memorandum presents the review and analysis of alternative traffic control devices and associated design features at the main project access driveway for the Rancho Vista residential development (Project) in San Juan Bautista, CA. A traffic study for the Project was prepared by Hexagon Transportation Consultants, Inc. in April 2014, which provides background information for this analysis and is included in **Appendix A**.

## INTERSECTION OPERATION REVIEW

Operational analysis was performed for the following traffic control devices at the main Project Access driveway (Lavagnino Drive) & First Street intersection:

- Roundabout
- All-Way Stop Control (AWSC)
- One-Way Stop Control (OWSC)

As shown in **Table 1**, each traffic control device would result in level of service (LOS) A operations, representing free-flow conditions with little or no delay.

**Table 1: Peak Hour Intersection LOS**

Lavagnino Drive & First Street Intersection	Control Type	AM Peak		PM Peak	
		Delay (sec)	LOS	Delay (sec)	LOS
	Roundabout	4.4	A	5.1	A
	AWSC	7.6	A	7.9	A
	OWSC	9.5*	A	9.9*	A

Notes: AWSC = All-Way Stop Control

OWSC = One-Way Stop Control

\*Delay = Average control delay in seconds per vehicle for the worst minor street approach

Roundabout and AWSC operations both show acceptable operations with insignificant differences in delay. Though a mini-roundabout is currently proposed, it is recommended that stop control be implemented at Lavagnino Drive as an interim measure due to roundabout design and safety considerations discussed in the following sections. Additionally, OWSC would also provide acceptable LOS A operations with negligible differences in delay from roundabout and AWSC operations. With OWSC, the intersection would operate acceptably with no change in minor street queuing and eliminate stops and queues along First Street, which is assumed to operate at 50 miles per hour (mph).



**Reference: Rancho Vista Project Access Intersection Review Memorandum**

## **INTERSECTION DESIGN REVIEW**

Upon review of the proposed mini-roundabout, it is noted that the 2014 Traffic Study did not analyze a future-year condition. Per Caltrans Highway Design Manual, 6<sup>th</sup> Edition (HDM 6), "Geometric design of new facilities and reconstruction projects should normally be based on estimated traffic 20 years after completion of construction". The Project is being developed west of First Street, but there is a vacant parcel available for development to the east. This parcel, when developed, would add vehicle traffic to the study intersection and could impact performance.

The proposed mini-roundabout has an inscribed circle diameter of ~85 feet, which may be insufficient for future traffic conditions. Additionally, per the San Benito County General Plan, First Street (which becomes Old San Juan Highway north of the Project) is identified as an Arterial, which would be expected to carry truck vehicle traffic. A mini-roundabout only accommodates a SU-30 typical design vehicle.

A single-lane roundabout installed at the study intersection should have an inscribed circle diameter of at least 105 feet to accommodate large trucks. Per HDM 6, single-lane entry roundabout entry speed should not exceed 25 mph. First Street, being assumed with 50 mph operating speeds, would need to be posted with a lower speed limit to ensure the entry speed requirement is adhered to.

Another design feature that should be included with a single-lane roundabout is bicycle accommodation. Though bicycle riders may choose to travel within a roundabout, there should be a Class I shared-use path provided to bypass the circular roadway per HDM 6. The San Benito County Bikeway and Pedestrian Master Plan identifies First Street/Old San Juan Highway to provide Class II bike lanes in each direction. The proposed mini-roundabout would not accommodate bicycle traffic outside the circular roadway and does not identify Class II bike lanes along First Street. To provide Class II bike lanes in each direction on First Street along the Project frontage, additional right-of-way (ROW) may be required.

Stop control is recommend as an interim measure until an appropriate single-lane roundabout can be properly designed to accommodate truck traffic, bicycle activity, and future traffic volumes per HDM 6.

## **INTERSECTION SAFETY REVIEW**

First Street is assumed to have an operating speed of 50 mph. Lavagnino Drive is expected to operate at 25 mph. Due to the requirements of HDM 6, the current mini-roundabout design would not provide a safe entry for vehicles on First Street as a result of the high operating speed. Roundabout installation would require a posted speed limit on First Street that would allow vehicles to safely enter and maneuver through the intersection.

Bicycle facility provisions are necessary to provide a safe environment for riders, who vary greatly in cycling experience, skill and confidence. Experienced riders may feel comfortable riding through a roundabout, but less experienced riders may choose to exit the street and proceed as pedestrians across the roundabout. Providing a separated facility for those unwilling to travel through the roundabout would have a higher level of safety for users.



**Reference: Rancho Vista Project Access Intersection Review Memorandum**

## **CONCLUSION**

Based on the operation, design, and safety review of the proposed mini-roundabout, AMG recommends that the Lavagnino Drive & First Street intersection should operate with OWSC as an interim measure until the currently vacant lot is developed and ROW can be acquired to design and install an appropriate single-lane roundabout. The Project intersection with OWSC will operate at an acceptable LOS A with less than 10 seconds of delay for Lavagnino Drive vehicles.

AMG recommends installing a roundabout at this intersection in the future. Per HDM 6, the single-lane roundabout should be designed to accommodate truck vehicle traffic, bicycle traffic, pedestrian travel, and incorporate expected future traffic growths.

### **Advanced Mobility Group**

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## TRAFFIC STUDY FOR RANCHO VISTA RESIDENTIAL DEVELOPMENT





# HEXAGON TRANSPORTATION CONSULTANTS, INC.

## Memorandum

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To: Bob Fulton, RL Fulton Holding Company, LLC  
From: Robert Del Rio, T.E.  
Date: April 4, 2014  
Subject: Traffic Study for the Proposed Christopher Ranch Residential Development

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Hexagon Transportation Consultants, Inc. has completed this traffic study for the proposed Christopher Ranch residential development in San Juan Bautista, California. The project site, which is currently vacant, is generally located south of 1<sup>st</sup> Street and west of Donner Street (See Figure 1). The project as proposed would consist of up 98 single-family lots. Based on the site plan presented on Figure 2, access to the project would be provided via 1<sup>st</sup> Street and the extension of 3<sup>rd</sup> Street, north of Donner Street.

The purpose of the traffic study is to evaluate the magnitude of traffic to be added to the roadway system by the proposed project and recommend necessary improvements to ensure adequate access to the site. The study consists of a review of existing traffic conditions, trip generation estimates for the proposed project, and projections of the effects that traffic associated with the proposed project would have on the study facilities. The analysis consists of the evaluation of eight existing unsignalized intersections and two future intersections that would provide access to the project site. The key intersections analyzed are:

1. Monterey Street and SR 156
2. Monterey Street and Muckelemi Street
3. Monterey Street and Larios Drive
4. Monterey Street and 3<sup>rd</sup> Street
5. Monterey Street and 1<sup>st</sup> Street
6. Donner Street and 3<sup>rd</sup> Street
7. Donner Street and 1<sup>st</sup> Street
8. Ahwahnee Street and 1<sup>st</sup> Street
9. Project Driveway and 1<sup>st</sup> Street (Future Project Access)
10. Project Driveway and 3<sup>rd</sup> Street Extension (Future Project Access)

The project site and study facilities are shown in Figure 1.

## Existing Traffic Conditions

The existing roadway geometrics and traffic patterns in the study area were measured and observed in the field. The existing traffic conditions on the roadway system and facilities in the vicinity of the project site along with the methodology used in the analysis are discussed below.

### *Existing Roadway Network*

Local access to the project site is currently provided via a private roadway along 1<sup>st</sup> Street, west of Ahwahnee Street. Additionally, a second access point is being proposed, providing secondary access to the project site via 3<sup>rd</sup> Street, north of Donner Street. These facilities, and other facilities providing access to the project site, are described below.



*1<sup>st</sup> Street* is a two-lane east-west roadway that extends south-eastward from the northern City limits to San Jose Street. North of the City limits, 1<sup>st</sup> Street transitions to San Juan Highway, which provides access to and from US 101 and SR 129. First Street currently provides access to the project site via a private roadway located west of Ahwahnee Street. The existing site access along 1<sup>st</sup> Street would become the primary access to the project site for vehicles traveling on 1<sup>st</sup> Street.

*3<sup>rd</sup> Street* is a two-lane east-west roadway that extends between Donner Street and Franklin Street at which point it transitions to The Alameda. Currently, 3<sup>rd</sup> Street runs through central San Juan Bautista and provides access to and from SR 156 via The Alameda. The posted speed limit along 3<sup>rd</sup> Street is 15 mph between North Street and Donner Street and through downtown. Direct access to the project site would be provided with the proposed extension of 3<sup>rd</sup> Street, north of Donner Street. As proposed, 3<sup>rd</sup> Street would be extended from its current terminus point at Donner Street north-eastward into the project site, providing direct access to the proposed housing units, and terminating at a T-intersection just east of the western project site boundary.

*Donner Street* is a two-lane residential roadway that runs between 1<sup>st</sup> Street and 3<sup>rd</sup> Street with no posted speed limit. Donner Street primarily serves residential land uses located south of 1<sup>st</sup> Street.

*Monterey Street* is a two-lane north-south roadway that provides access to and from SR 156. Monterey Street runs in generally a north-south direction between SR 156 and 1<sup>st</sup> Street, with a posted speed limit of 25 MPH.

Sidewalks and bike lanes are provided along both sides of 1<sup>st</sup> Street south of the project site boundary. Sidewalks are provided along both sides of Donner Street between 3<sup>rd</sup> Street and 1<sup>st</sup> Street. With the exception of the south side of a segment located just south of Donner Street, there are currently sidewalks on both sides of 3<sup>rd</sup> Street between Donner Street and Church Street. There also are no sidewalks provided on either side of 3<sup>rd</sup> Street between Church Street and Monterey Street. Sidewalks are provided along 3<sup>rd</sup> Street east of Monterey Street.

### ***Existing Traffic Operations***

Traffic conditions were analyzed for the weekday AM and PM peak hours. The weekday AM peak hour of traffic is generally between 7:00 and 9:00 AM, and the weekday PM peak hour is typically between 4:00 and 6:00 PM. It is during these periods that the most congested traffic conditions occur on a typical weekday. Turning movement volumes at all eight existing study intersections were conducted in March 2014 during the AM and PM peak hours. Figure 3 shows the existing traffic volumes at the study intersections. Peak hour intersection level of service and signal warrant analyses were performed at each of the eight study intersections and are summarized in Table 1.

### **Intersection Level of Service Analysis**

Level of Service is a qualitative description of operating conditions ranging from LOS A, or free-flow conditions with little or no delay, to LOS F, or jammed conditions with excessive delays. Each of the study intersections were analyzed using TRAFFIX software, which is based on the *Highway Capacity Manual* (HCM) 2000 method for computing level of service at intersections. Two-way-stop controlled intersection levels of service are evaluated on the basis of worst approach stop control delay time for all vehicles at the intersection. The City of San Juan Bautista General Plan Transportation Policy T-2 identifies LOS C as the standard for intersections during the weekday peak commute periods.

The level of service analysis under existing conditions indicates that all existing study intersections, with the exception of Monterey Street and SR 156, currently operate at LOS A during both the AM and PM peak hours. The intersection of Monterey Street and SR 156 currently operates at LOS D conditions during both the AM and PM peak hours, based on the approach with the highest delay.

### **Signal Warrant Analysis**

The level of service analysis at unsignalized intersections is supplemented with an assessment of the need for signalization of the intersection. The need for signalization of unsignalized intersections is assessed based on the Peak Hour Volume Warrant (Warrant 3) described in the *California Manual on Uniform Traffic Control Devices for Streets and Highways* (CA MUTCD), Part 4, Highway Traffic Signals,



2010. This method makes no evaluation of intersection level of service, but simply provides an indication whether vehicular peak hour traffic volumes are, or would be, sufficient to justify installation of a traffic signal. Intersections that meet the peak hour warrant are subject to further analysis before determining that a traffic signal is necessary. Additional analysis may include unsignalized level of service analysis and/or operational analysis such as evaluating vehicle queuing and delay. Other options such as traffic control devices, signage, or geometric changes may be preferable based on existing field conditions.

The signal warrants were checked using the existing peak hour traffic volumes. The signal warrant checks indicate that the unsignalized study intersections currently have traffic conditions that fall below the thresholds that warrant signalization.

## Project Traffic Conditions

The effects of project traffic on the study intersections were evaluated based on intersection levels of service analysis and signal warrant checks. Each of the components of the analysis is described in the following sections.

### *Project Description*

The proposed project consists of the construction of up to 98 single-family units within a vacant lot generally located south of 1<sup>st</sup> Street and west of Donner Street. Access to the project site is currently provided via a private roadway that intersects with 1<sup>st</sup> Street, west of Ahwahnee Street. With the development of the proposed project, the private roadway (labeled as Street A on the site plan) would be developed as a two-lane roadway and extended southward to the south project site boundary. A second access point to the project site would be provided with the proposed extension of 3<sup>rd</sup> Street, north of Donner Street.

### *Project Trip Generation, Distribution, and Assignment*

#### Trip Generation

The magnitude of traffic produced by a new development is estimated by applying the size of the project to the applicable trip generation rate contained in the Institute of Transportation Engineers (ITE) Trip Generation Manual, 9<sup>th</sup> Edition, 2012. On the basis of the ITE rates, it is estimated that the proposed project (a 98-unit single-family residential development) would generate 78 AM peak-hour trips (20 inbound trips and 58 outbound trips) and 103 PM peak-hour trips (65 inbound trips and 38 outbound trips). The project trip generation estimates are presented in Table 2.

#### Trip Distribution and Assignment

The trip distribution pattern for the proposed project was estimated based on existing travel patterns on the surrounding roadway system and the locations of complementary land uses. The peak-hour trips generated by the project were assigned to the roadway system in accordance with the trip distribution pattern. Figure 4 shows the proposed trip distribution and assignment for the proposed project.

### *Project Conditions Traffic Operations*

Project impacts are based upon a comparison of project conditions levels of service to those under existing conditions. The City of San Juan Bautista General Plan Transportation Policy T-2 identifies LOS C as the standard for intersections during the weekday peak commute periods.

#### Intersection Level of Service Analysis

Results of the level of service analysis under existing plus project conditions indicate that all of the study intersections, with the exception of the intersection of Monterey Street and SR 156, would continue to operate at acceptable levels of service (LOS B or better) with the addition of project traffic during both the AM and PM peak hours.

The intersection of Monterey Street and SR 156 is projected to continue operating at an unacceptable LOS D conditions during the AM and PM peak hours. However, the addition of project traffic would result



in an increase of no more than 3 seconds during the PM peak hour (less than 1 second during the AM peak hour) to the intersection's worst approach delay and will not cause the peak hour signal warrant to be met, as discussed below.

The poor minor street (southbound Monterey Street) approach levels of service are due to the continuous flow of traffic on SR 156, which can result in less than adequate gaps for left turning traffic. However, the proposed project is not projected to add any traffic to the southbound (Monterey Street) approach at the intersection, approach experiencing the highest delays. The proposed project is projected to add 9 AM and 29 PM peak hour trips to the eastbound left-turn movement on SR 156 to northbound Monterey Street, thus the slight increase in delay from existing conditions.

The levels of service under project conditions are summarized in Table 1.

### **Signal Warrant Analysis**

The results of the peak-hour traffic signal warrant checks under project conditions indicate that the addition of project traffic would not create the need to signalize any of the unsignalized study intersections. Hexagon concludes that the existing stop control at the intersections is appropriate. The results of the peak-hour traffic signal warrant checks are summarized in Table 1.

### **Site Access and On-Site Circulation**

Based on the proposed site plan shown on Figure 2 (by San Benito Engineering & Surveying, Inc. and dated April 1, 2014), the proposed project would be served by two access points: one along 1<sup>st</sup> Street and the second along 3<sup>rd</sup> Street. Access along 1<sup>st</sup> Street is currently being provided via a private roadway that intersects with 1<sup>st</sup> Street west of Ahwahnee Street. With the development of the proposed project, the private roadway (labeled as Street A on the site plan) would be developed as a two-lane roadway and extended southward to the south project site boundary. The existing site access along 1<sup>st</sup> Street would become the primary access for vehicles traveling on 1<sup>st</sup> Street.

A second access point to the project site is being proposed along 3<sup>rd</sup> Street, north of Donner Street. This new access point would provide access to the project site to/from the southeast part of town. As proposed, 3<sup>rd</sup> Street would be extended from its current terminus point at Donner Street north-eastward into the project site, providing direct access to the proposed housing units, and terminating at a T-intersection just east of the western project site boundary.

Within the project site, 3<sup>rd</sup> Street and Street A would be the main roadways providing access to all proposed residential units within the project site. Side residential streets branching off of 3<sup>rd</sup> Street would provide direct access to the majority of the proposed residential units, with three of these streets terminating at cul-de-sacs. In addition, Street A (located approximately in the middle of the project site) and Street F (located along the eastern project site boundary) are shown to extend to the southern project site boundary, providing a connection to potential future development of the adjacent parcels south of the project site.

The site plan does not specify internal roadway widths. Design of all internal access roadways should adhere to City of San Juan Bautista and San Benito County roadway design guidelines and requirements. Adequate emergency vehicle access must be provided to all residential units within the site. Adhering to the recommended roadway design guidelines, the proposed internal roadway layout should provide adequate vehicular access (in particular emergency vehicle access) and on-site circulation, making every proposed residential unit within the project development accessible.

An evaluation of each of the project access point was completed and includes an evaluation of intersection level of service, signal warrants, and sight distance at the 1<sup>st</sup> Street access point. Results of the analysis are described below and summarized in Table 3.

#### **1<sup>st</sup> Street Access**

As shown on the site plan, the 1<sup>st</sup> Street access point is proposed to consist of a roundabout. Although the specific dimensions are not shown on the site plan, for the purpose of this analysis, it was assumed



that this access point would consist of a single-lane roundabout. The analysis of the roundabout was performed using the Synchro software, which is based on the HCM 2010 method for computing level of service at roundabouts. After completion of the proposed project, it is projected that the roundabout at 1<sup>st</sup> Street and Street A (Project Access Road) would operate at LOS A conditions during both the AM and PM peak hours.

Additionally, for informational purposes, the level of service at project site access on 1<sup>st</sup> Street also was checked assuming an unsignalized (all-way stop-controlled) intersection and a traffic signal at this location. The results of the level of service analysis for the Street A/1<sup>st</sup> Street intersection show that the implementation of either an all-way stop-control of a traffic signal would result in LOS A conditions under existing plus project conditions.

The evaluation of sight distance consists of measuring the available sight distance at the proposed project access point along 1<sup>st</sup> Street and comparing it to the minimum required distance based upon standard criteria. The Caltrans Highway Design Manual specifies minimum required sight distances as a function of vehicle speed. The process for determining the adequacy of available sight distance at each driveway location is as follows:

- The minimum stopping sight distance associated with the posted speed limit, using the Caltrans Highway Design Manual, Table 201.1, is calculated
- The available sight distance for each access point is measured out in the field
- The available sight distance is compared to the minimum stopping sight distance to determine if sufficient sight distance is available.

1<sup>st</sup> Street is a two-lane roadway with no posted speed limit. Therefore a speed limit of 50 miles per hour (mph) was assumed. A minimum of 430 feet of sight distance is required for a roadway with travel speeds of 50 mph, based on the Caltrans Guidelines. Based on field observations, more than 500 feet of sight distance is currently provided in the westbound direction (along 1<sup>st</sup> Street) at the proposed project access point. A clear line of site also will be provided to the 1<sup>st</sup> Street and Ahwahnee Street intersection that is located approximately 250 east of the proposed driveway. Therefore, adequate sight distance will be provided at the project access point along 1<sup>st</sup> Street.

### **3<sup>rd</sup> Street Access**

Currently, 3<sup>rd</sup> Street extends from Franklin Street to Donner Street. West of Donner Street, 3<sup>rd</sup> Street becomes an undeveloped access roadway that provides access to existing residential units located east of the project site. With the proposed project, 3<sup>rd</sup> Street would be extended north of Donner Street into the project site, terminating at a T-intersection just east of the western project site boundary. The 3<sup>rd</sup> Street extension would intersect with Street A within the project site. For the purpose of this analysis, it was assumed that the new access point on 3<sup>rd</sup> Street would consist of an all-way stop controlled intersection.

It should be noted that with the extension/development of 3<sup>rd</sup> Street into the project site, a small segment of 3<sup>rd</sup> Street, between Donner Street and the project site, would remain undeveloped. This segment of 3<sup>rd</sup> Street must be developed, either by the proposed project or other potential development, in order to be used as an access route to the proposed project site.

The level of service analysis at the 3<sup>rd</sup> Street project site access indicates that the intersection would operate at a LOS A conditions during both the AM and PM peak hours with implementation of the proposed project.

## **Transit, Bicycle, and Pedestrian Analysis**

Assuming a three percent transit mode share, the project would create no more than 1 new transit rider during each of the peak hours. The City of San Juan Bautista is served by the San Benito County Transit "County Express" bus service. The County Express bus line stops at Abbe Park and Anzar High School. No additional transit facilities are recommended since the demand generated by the proposed project would not justify the implementation of additional transit services.





Sidewalks and bike lanes are provided along both sides of 1<sup>st</sup> Street south of the project site boundary. A sidewalk along the project's frontage on the south side of 1<sup>st</sup> Street should be constructed to provide residents and visitors with a continuous sidewalk between the project site and other surrounding land uses in the area. In addition, roadway improvements at the project access point along 1<sup>st</sup> Street should be designed to provide for the continuation of existing bike lanes south of the project site.

Additionally, it is recommended that sidewalks be provided along both sides of all new streets within the project site and project frontage. Specifically, sidewalks along the future extension of 3<sup>rd</sup> Street should be provided within the project site to provide residents and visitors with a continuous sidewalk between the project site and other surrounding land uses in the area.

## Conclusions

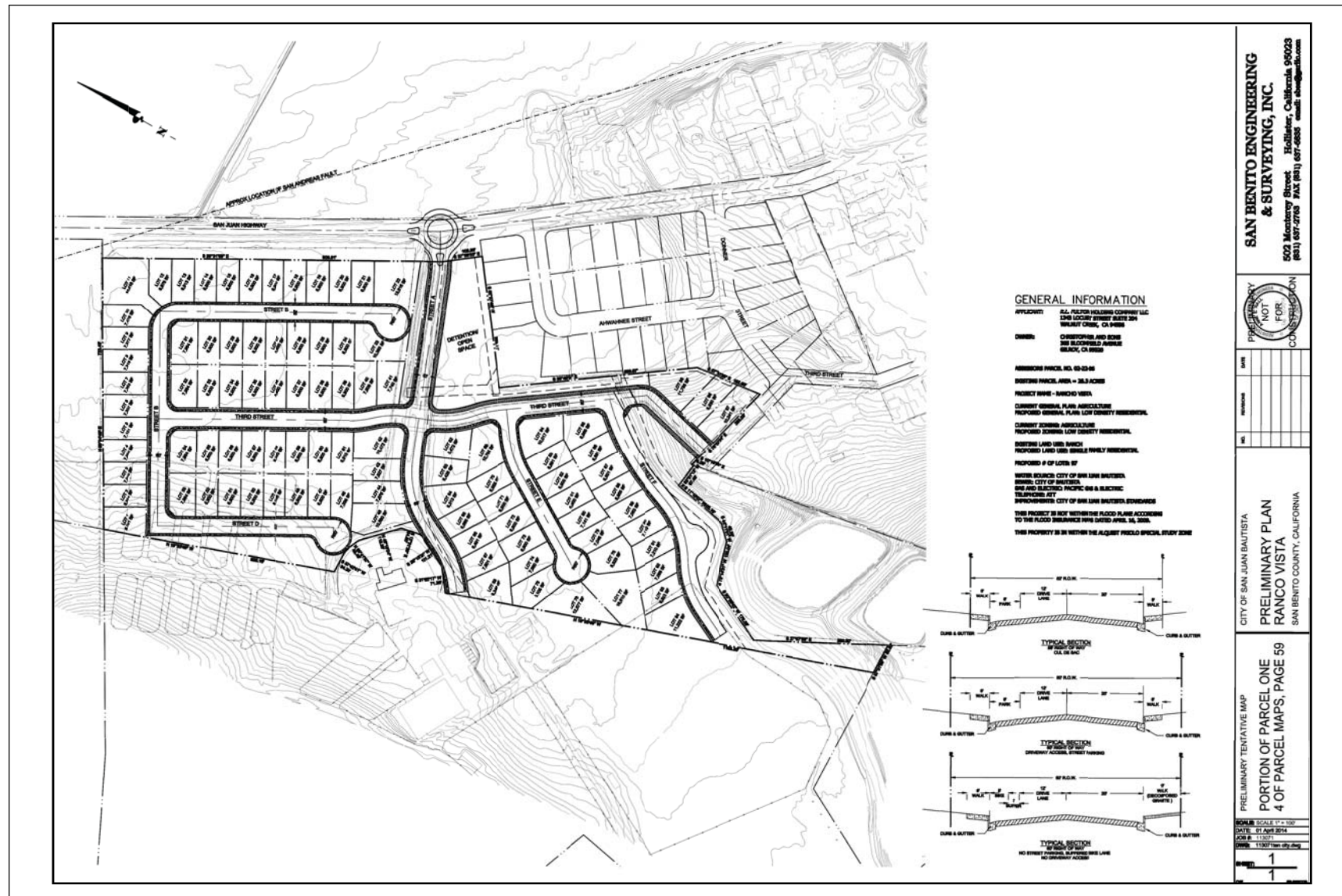
The study included the analysis of AM and PM peak-hour traffic conditions for six unsignalized intersections in the immediate area of the project site. Based on intersection level of service analysis, the project is not projected to have an adverse impact on either of the study intersections. An evaluation of peak hour signal warrants indicated that none of the study intersections would have traffic conditions that warrant signalization with the addition of project traffic. The evaluation of sight distance at the proposed project driveways indicate that adequate sight distance will be provided at the proposed project entrance along 1<sup>st</sup> Street.

A sidewalk along the project's frontage on the south side of 1<sup>st</sup> Street should be constructed to provide residents and visitors with a continuous sidewalk between the project site and other surrounding land uses in the area. In addition, roadway improvements at the project access point along 1<sup>st</sup> Street should be designed to provide for the continuation of existing bike lanes south of the project site. In addition, it is recommended that sidewalks be provided along both sides of all new streets within the project site and project frontage. Specifically, sidewalks along the future extension of 3<sup>rd</sup> Street should be provided within the project site to provide residents and visitors with a continuous sidewalk between the project site and other surrounding land uses in the area.

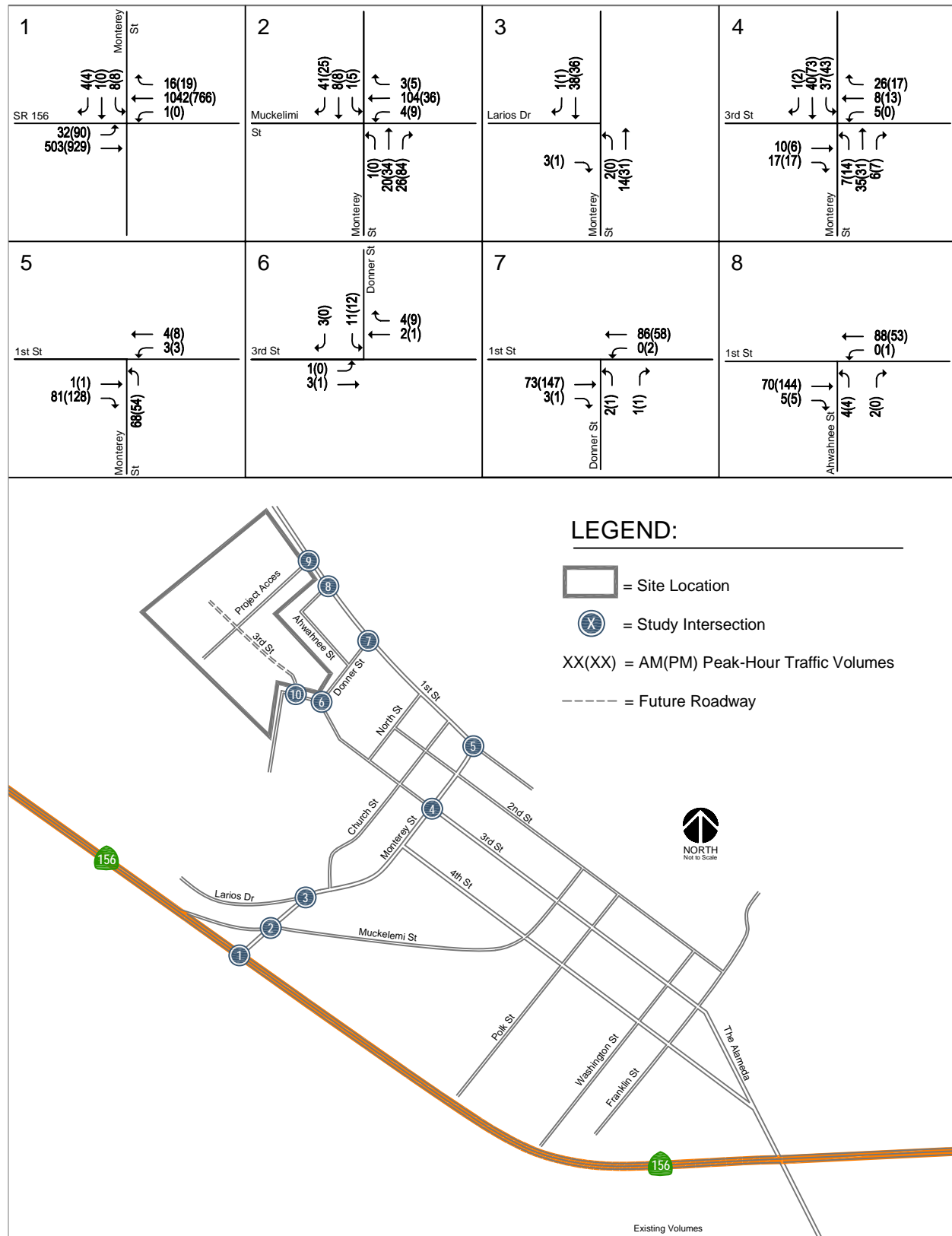






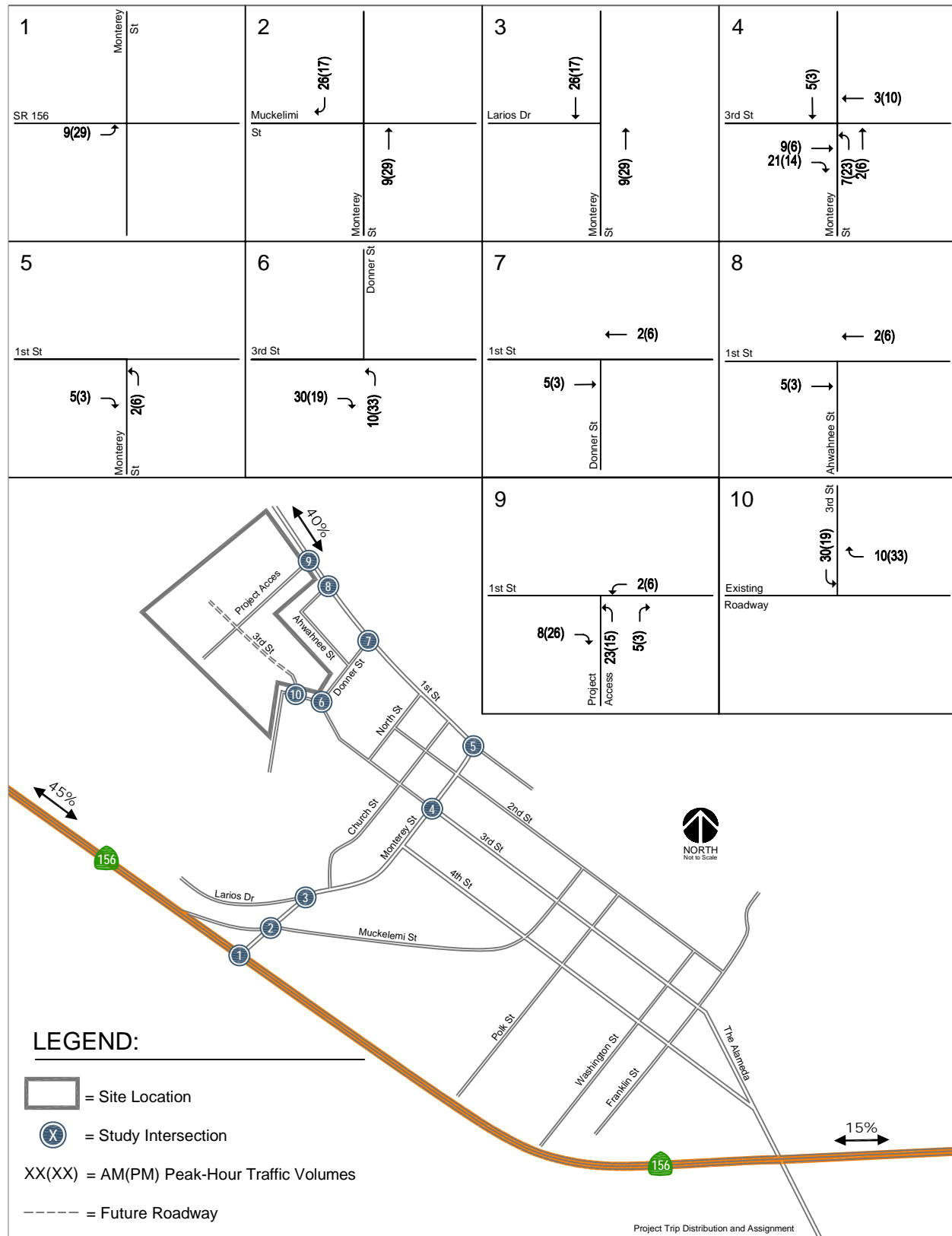
Figure 2  
Site Plan





**Figure 3**  
**Existing Traffic Volumes**





**Figure 4**  
**Project Trip Distributions and Assignments**



**Table 1**  
**Intersection Level of Service and Signal Warrant Summary**

Study Number	Intersection	Existing Control	Peak Hour	Count Date	Existing Conditions				Project Conditions			
					Warrant Met? <sup>1</sup>	Avg Delay <sup>2</sup>	Worst Delay <sup>3</sup>	LOS	Warrant Met? <sup>1</sup>	Avg Delay <sup>2</sup>	Worst Delay <sup>3</sup>	LOS
1	Monterey Street and SR 156	TWSC	AM	03/20/14	No	0.4	27.9	D	No	0.5	28.7	D
			PM	03/20/14	No	0.7	28.5	D	No	0.8	31.4	D
2	Monterey Street and Muckelimi Street	AWSC	AM	03/20/14	No	7.3	--	A	No	7.4	--	A
			PM	03/20/14	No	7.1	--	A	No	7.3	--	A
3	Monterey Street and Larios Drive	OWSC	AM	03/20/14	No	0.7	8.5	A	No	0.4	8.6	A
			PM	03/20/14	No	0.1	8.5	A	No	0.1	8.5	A
4	Monterey Street and 3rd Street	TWSC	AM	03/20/14	No	4.8	9.2	A	No	5.4	9.4	A
			PM	03/20/14	No	4.1	9.5	A	No	4.9	10.2	B
5	Monterey Street and 1st Street	AWSC	AM	03/20/14	No	7.2	--	A	No	7.2	--	A
			PM	03/20/14	No	7.1	--	A	No	7.2	--	A
6	Donner Street and 3rd Street	OWSC	AM	03/20/14	No	5.3	8.5	A	No	3.4	8.7	A
			PM	03/20/14	No	4.5	8.6	A	No	5.2	8.7	A
7	Donner Street and 1st Street	OWSC	AM	03/20/14	No	0.2	9.1	A	No	0.2	9.1	A
			PM	03/20/14	No	0.2	9.3	A	No	0.2	9.3	A
8	Ahwahnee Street and 1st Street	AWSC	AM	03/20/14	No	7.4	--	A	No	7.4	--	A
			PM	03/20/14	No	7.6	--	A	No	7.6	--	A

Intersection control based on existing conditions

- TWSC = two-way stopped controlled intersection
- AWSC = all-way stopped controlled intersection
- OWSC = one-way stopped controlled intersection

<sup>1</sup>Signal warrant analysis based on the Peak Hour Signal Warrant #3, Figure 4C Caltrans MUTCD 2010 Edition.

<sup>2</sup>Whole intersection weighted average control delay expressed in seconds per vehicle.

<sup>3</sup>The worst case delay is normally the time it would take a vehicle on the minor street of an unsignalized intersection to make a left-turn onto the major street, expressed in seconds per vehicle.



**Table 2**  
**Trip Generation Estimates**

Land Use	ITE Code	Size	Daily Trip Rates	Daily Trips	AM Peak Hour						PM Peak Hour					
					Pk-Hr Rate	Splits		Trips		Total	Pk-Hr Rate	Splits		Trips		Total
						In	Out	In	Out			In	Out	In	Out	
Single-Family Detached Housing	210	98 units	10.52	1,031	0.80	25%	75%	20	58	78	1.05	63%	37%	65	38	103
Source: ITE Trip Generation, 9th Edition 2012.																

**Table 3**  
**Site Access Analysis Summary**

Study Number	Intersection	Intersection Control	Peak Hour	Existing Plus Project Conditions		
				Warrant Met? <sup>1</sup>	Avg Delay <sup>2</sup>	LOS
9	Project Driveway and 1st Street	AWSC	AM	No	7.5	A
			PM	No	7.7	A
9	Project Driveway and 1st Street	Signal	AM	No	6.9	A
			PM	No	3.5	A
9	Project Driveway and 1st Street	Roundabout	AM	No	4.0	A
			PM	No	4.5	A
10	3rd Street and Future Access	AWSC	AM	No	7.1	A
			PM	No	6.8	A
Assumed intersection control - AWSC = all-way stopped controlled intersection <sup>1</sup> Signal warrant analysis based on the Peak Hour Signal Warrant #3, Figure 4C Caltrans MUTCD 2010 Edition. <sup>2</sup> Whole intersection weighted average control delay expressed in seconds per vehicle.						



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### ROUNDAABOUT LOS ANALYSIS



# DELAY (CONTROL)

Average control delay per vehicle, or average pedestrian delay (seconds)

 **Site: 101 [First St & Project Access\_AM]**

First St & Project Access

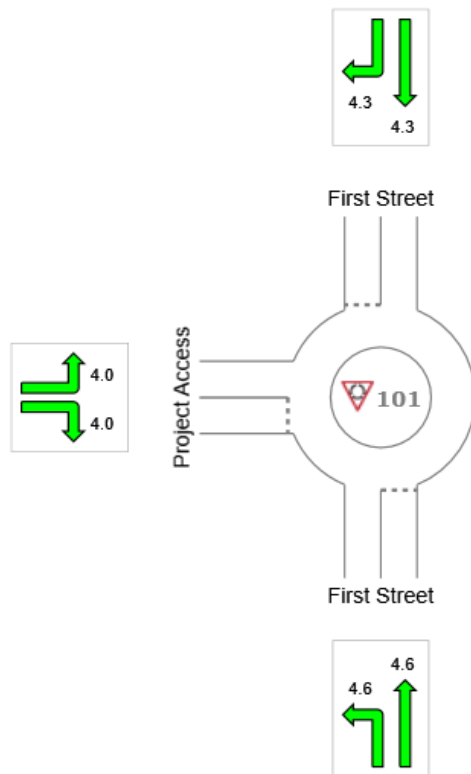
Site Category: (None)

Roundabout

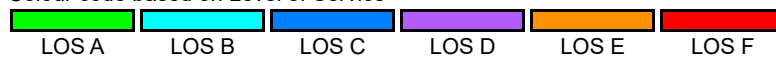
Design Life Analysis (Final Year): Results for 20 years

## All Movement Classes

	Approaches			Intersection
	South	North	West	
Delay (Control)	4.6	4.3	4.0	4.4
LOS	A	A	A	A



Colour code based on Level of Service



Site Level of Service (LOS) Method: Delay & v/c (HCM 2010). Site LOS Method is specified in the Parameter Settings dialog (Site tab). LOS F will result if v/c > 1 irrespective of movement delay value (does not apply for approaches and intersection).

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Roundabout Level of Service Method: Same as Sign Control

HCM Delay Formula option is used. Control Delay does not include Geometric Delay since Exclude Geometric Delay option applies.







# DELAY (CONTROL)

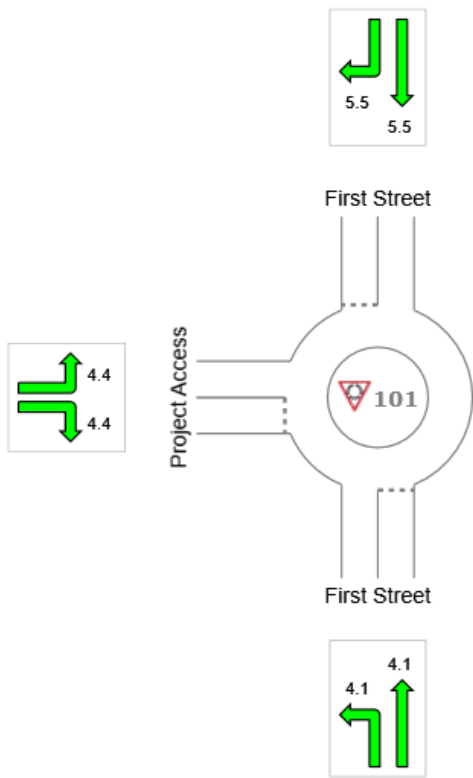
Average control delay per vehicle, or average pedestrian delay (seconds)

 **Site: 101 [First St & Project Access\_PM]**

First St & Project Access  
Site Category: (None)  
Roundabout  
Design Life Analysis (Final Year): Results for 20 years

### All Movement Classes

	Approaches			Intersection
	South	North	West	
Delay (Control)	4.1	5.5	4.4	5.1
LOS	A	A	A	A



Colour code based on Level of Service



Site Level of Service (LOS) Method: Delay & v/c (HCM 2010). Site LOS Method is specified in the Parameter Settings dialog (Site tab).  
LOS F will result if v/c > 1 irrespective of movement delay value (does not apply for approaches and intersection).  
NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).  
Roundabout Level of Service Method: Same as Sign Control  
HCM Delay Formula option is used. Control Delay does not include Geometric Delay since Exclude Geometric Delay option applies.







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### ALL-WAY STOP CONTROL LOS ANALYSIS






# HCM 2010 AWSC

## 1: First St & Project Access

Timing Plan: AM Peak

### Intersection

Intersection Delay, s/veh	7.6
Intersection LOS	A

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	23	5	2	92	75	8
Future Vol, veh/h	23	5	2	92	75	8
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	25	5	2	100	82	9
Number of Lanes	1	0	0	1	1	0

Approach	EB	NB	SB
Opposing Approach		SB	NB
Opposing Lanes	0	1	1
Conflicting Approach Left	SB	EB	
Conflicting Lanes Left	1	1	0
Conflicting Approach Right	NB		EB
Conflicting Lanes Right	1	0	1
HCM Control Delay	7.6	7.6	7.5
HCM LOS	A	A	A

Lane	NBLn1	EBLn1	SBLn1
Vol Left, %	2%	82%	0%
Vol Thru, %	98%	0%	90%
Vol Right, %	0%	18%	10%
Sign Control	Stop	Stop	Stop
Traffic Vol by Lane	94	28	83
LT Vol	2	23	0
Through Vol	92	0	75
RT Vol	0	5	8
Lane Flow Rate	102	30	90
Geometry Grp	1	1	1
Degree of Util (X)	0.115	0.037	0.1
Departure Headway (Hd)	4.059	4.32	4.006
Convergence, Y/N	Yes	Yes	Yes
Cap	881	818	891
Service Time	2.096	2.404	2.047
HCM Lane V/C Ratio	0.116	0.037	0.101
HCM Control Delay	7.6	7.6	7.5
HCM Lane LOS	A	A	A
HCM 95th-tile Q	0.4	0.1	0.3






# HCM 2010 AWSC

## 1: First St & Project Access

Timing Plan: PM Peak

### Intersection

Intersection Delay, s/veh	7.9
Intersection LOS	A

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	15	3	6	57	149	26
Future Vol, veh/h	15	3	6	57	149	26
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	16	3	7	62	162	28
Number of Lanes	1	0	0	1	1	0

Approach	EB	NB	SB
Opposing Approach		SB	NB
Opposing Lanes	0	1	1
Conflicting Approach Left	SB	EB	
Conflicting Lanes Left	1	1	0
Conflicting Approach Right	NB		EB
Conflicting Lanes Right	1	0	1
HCM Control Delay	7.7	7.5	8
HCM LOS	A	A	A

Lane	NBLn1	EBLn1	SBLn1
Vol Left, %	10%	83%	0%
Vol Thru, %	90%	0%	85%
Vol Right, %	0%	17%	15%
Sign Control	Stop	Stop	Stop
Traffic Vol by Lane	63	18	175
LT Vol	6	15	0
Through Vol	57	0	149
RT Vol	0	3	26
Lane Flow Rate	68	20	190
Geometry Grp	1	1	1
Degree of Util (X)	0.079	0.025	0.208
Departure Headway (Hd)	4.129	4.55	3.93
Convergence, Y/N	Yes	Yes	Yes
Cap	863	791	911
Service Time	2.178	2.55	1.961
HCM Lane V/C Ratio	0.079	0.025	0.209
HCM Control Delay	7.5	7.7	8
HCM Lane LOS	A	A	A
HCM 95th-tile Q	0.3	0.1	0.8



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### ONE-WAY STOP CONTROL LOS ANALYSIS



# HCM 2010 TWSC

## 1: First St & Project Access

Timing Plan: AM Peak




Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	23	5	2	92	75	8
Future Vol, veh/h	23	5	2	92	75	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	25	5	2	100	82	9
Major/Minor	Minor2	Major1		Major2		
Conflicting Flow All	191	87	91	0	-	0
Stage 1	87	-	-	-	-	-
Stage 2	104	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	798	971	1504	-	-	-
Stage 1	936	-	-	-	-	-
Stage 2	920	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	797	971	1504	-	-	-
Mov Cap-2 Maneuver	797	-	-	-	-	-
Stage 1	935	-	-	-	-	-
Stage 2	920	-	-	-	-	-
Approach	EB	NB		SB		
HCM Control Delay, s	9.5	0.2		0		
HCM LOS	A					
Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR	
Capacity (veh/h)	1504	-	823	-	-	
HCM Lane V/C Ratio	0.001	-	0.037	-	-	
HCM Control Delay (s)	7.4	0	9.5	-	-	
HCM Lane LOS	A	A	A	-	-	
HCM 95th %tile Q(veh)	0	-	0.1	-	-	



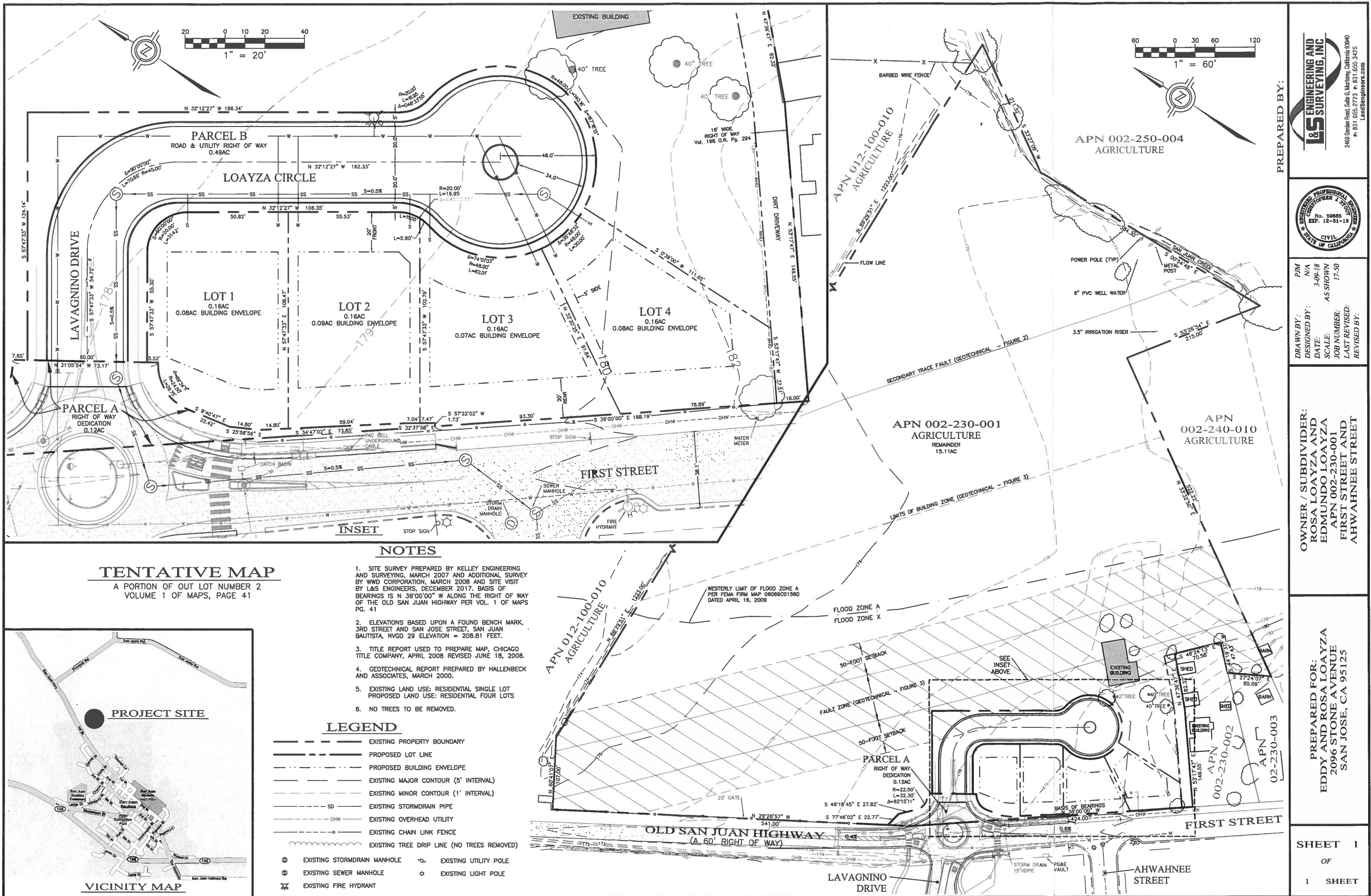
# HCM 2010 TWSC

## 1: First St & Project Access

Timing Plan: PM Peak

Intersection						
Int Delay, s/veh	0.9					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	15	3	6	57	149	26
Future Vol, veh/h	15	3	6	57	149	26
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	16	3	7	62	162	28
Major/Minor	Minor2		Major1		Major2	
Conflicting Flow All	252	176	190	0	-	0
Stage 1	176	-	-	-	-	-
Stage 2	76	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	737	867	1384	-	-	-
Stage 1	855	-	-	-	-	-
Stage 2	947	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	733	867	1384	-	-	-
Mov Cap-2 Maneuver	733	-	-	-	-	-
Stage 1	851	-	-	-	-	-
Stage 2	947	-	-	-	-	-
Approach	EB		NB		SB	
HCM Control Delay, s	9.9		0.7		0	
HCM LOS	A					
Minor Lane/Major Mvmt	NBL		NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1384		-	752	-	-
HCM Lane V/C Ratio	0.005		-	0.026	-	-
HCM Control Delay (s)	7.6		0	9.9	-	-
HCM Lane LOS	A		A	A	-	-
HCM 95th %tile Q(veh)	0		-	0.1	-	-







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GRADING NOTES

1. THIS PLAN HAS BEEN REVIEWED BY THE CITY OF SAN JUAN BAUTISTA AND/OR ITS AUTHORIZED REPRESENTATIVE, BUT SUCH CHECKING AND/OR APPROVAL DOES NOT RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITY TO CORRECT ERRORS, OMISSIONS OR MAKE CHANGES REQUIRED BY CONDITIONS DISCOVERED IN THE FIELD DURING THE COURSE OF CONSTRUCTION. CIVIL ENGINEER & THE CITY OF SAN JUAN BAUTISTA MUST BE INFORMED IN WRITING OF ANY CHANGES MADE DURING CONSTRUCTION PRIOR TO EXECUTION OF THE SAID CHANGE.
2. ALL REVISIONS TO THIS PLAN MUST BE REVIEWED BY THE GOVERNING AGENCY(S) PRIOR TO CONSTRUCTION AND SHALL BE ACCURATELY SHOWN ON REVISED PLANS STAMPED AND SIGNED BY THE APPROPRIATE GOVERNING AGENCY(S) PRIOR TO THE INSTALLATION OF THE IMPROVEMENTS.
3. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISION FOR DUST CONTROL FOR THE WORK. DUST CONTROL MEASURES SHALL BE FULLY AND ADEQUATELY CARRIED OUT ON WEEKDAYS, WEEKENDS, AND HOLIDAYS, AND WHEN NECESSARY, BEFORE AND AFTER NORMAL WORKING HOURS. A DUST CONTROL PLAN APPROVED BY THE MBUAPCD MAY BE REQUIRED AND IS THE CONTRACTOR'S RESPONSIBILITY.
4. DURING CONSTRUCTION OF THE SITE, THE SOIL SHALL BE WATERED IN ORDER TO MINIMIZE THE IMPACTS OF DUST ON THE SURROUNDING AREA.
5. THE CONTRACTOR SHALL NOT PERFORM ANY EXCAVATION OPERATIONS UNTIL ALL UTILITY COMPANIES, STATE, COUNTY, AND CITY AGENCIES HAVE BEEN NOTIFIED AND GIVEN THE OPPORTUNITY TO MARK THEIR FACILITIES IN THE FIELD.
6. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO ASSUME ALL LIABILITY FOR ANY AND ALL DAMAGES WHICH MAY OCCUR DUE TO THE CONTRACTOR'S FAILURE TO PHYSICALLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
7. ALL WORK SHALL BE PER OSHA AND APPLICABLE FEDERAL, STATE AND LOCAL CODE REQUIREMENTS.
8. THE CONTRACTOR SHALL SECURE ALL REQUIRED ENCROACHMENT AND/OR OTHER PERMITS FROM ALL GOVERNING AGENCIES BEFORE THE COMMENCEMENT OF WORK.
9. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD HARMLESS THE OWNER, ARCHITECT, AND ENGINEER FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER, ARCHITECT, OR ENGINEER.
10. EXISTING A.C. PAVING AND/OR CONCRETE SHALL BE CUT TO A NEAT, STRAIGHT LINE (WHEN APPLICABLE). ALL EXPOSED VERTICAL SURFACES SHALL BE TACK COATED BELOW FINISH GRADE LINE WITH ASPHALT EMULSION PRIOR TO PAVING.
11. ALL SURVEY MONUMENTS WITHIN THE AREA OF CONSTRUCTION SHALL BE PRESERVED OR, IF DAMAGED, RESET BY A LICENSED CIVIL ENGINEER OR LICENSED LAND SURVEYOR.
12. ALL GRADING SHALL CONFORM TO CITY OF SAN JUAN BAUTISTA AND SAN BENITO COUNTY GRADING ORDINANCE AND SHALL BE IN ACCORDANCE WITH 2016 CBC APPENDIX J.
13. NO GRADING SHALL BE PERFORMED ONTO ADJACENT PROPERTY WITHOUT WRITTEN PERMISSION OF OWNER.
14. IF REQUIRED BY GOVERNING AGENCY, PROVIDE COMPACTION REPORTS PREPARED BY AN APPROVED TESTING AGENCY.
15. THE CONTRACTOR SHALL OBTAIN A GRADING PERMIT AND ABIDE BY ALL CONDITIONS THEREOF AND PAY PERMIT FEES.
16. CONTRACTOR IS RESPONSIBLE FOR RESETTling EXISTING MONITORING WELLS, MANHOLES, CLEAN-OUTS, VALVE BOXES, UTILITY VAULTS, OR ANY OTHER STRUCTURES TO REMAIN TO FINISH GRADE AS NECESSARY.
17. ALL NEW ASPHALT CONCRETE PAVING AND CONCRETE WORK SHALL MATCH THE EXISTING GRADES AT LIMITS OF WORK. DRAIN CONCRETE SLABS AWAY FROM BUILDINGS.
18. REPLACE EXISTING A.C. PAVING AND/OR CONCRETE AS REQUIRED DUE TO NEW CONSTRUCTION. INSTALL NEW PAVING PER PLAN.
19. JOINT SEALANT SHALL BE APPLIED TO ALL EXPANSION AND ISOLATION JOINTS.
20. THE SITE SHALL BE UNIFORMLY GRADED BETWEEN POINTS WHERE ELEVATIONS ARE GIVEN OR BETWEEN SUCH POINTS AND EXISTING FINISHED GRADES OR CONFORM POINTS.
21. ASPHALT PRIME COAT SHALL BE APPLIED TO EXISTING PAVEMENT PRIOR TO OVERLAY.
22. ASPHALT CONCRETE PAVEMENT SHALL BE PLACED IN TWO OR MORE LIFTS PER CITY OF SAN JUAN BAUTISTA STANDARD SPECIFICATIONS.
23. ASPHALT TACK COAT SHALL BE APPLIED BELOW FINISHED GRADE TO ALL VERTICAL SURFACES IN CONTACT WITH THE ASPHALT PAVEMENT AND TO THE SURFACE OF THE BASE COURSE WHEN FINISH COURSE IS PLACED ON DIFFERENT DAYS. TACK COAT SHALL BE APPLIED AT A RATE BETWEEN 0.05 AND 0.15 GALLONS PER SQUARE YARD.
24. PRIOR TO PLACING AGGREGATE BASE OR PAVEMENT, THE SUBGRADE SHALL BE TREATED WITH A SOILS STERILIZATION TYPE WEED KILLER.
25. SAW-CUTTING OF CONCRETE TO CREATE CONTROL JOINTS SHALL NOT BE ALLOWED.
26. UNLESS SPECIFICALLY NOTED OTHERWISE IN THE PLANS, ALL CONCRETE SIDEWALKS AND PATHS OF TRAVEL ARE TO BE CONSTRUCTED WITH A MAXIMUM CROSS-SLOPE OF 2.00% (0.020 FT/FT), AS REQUIRED TO BE IN COMPLIANCE WITH REGULATIONS FOR ACCESSIBLE PATHWAYS MANDATED BY THE AMERICANS WITH DISABILITIES ACT (ADA).
27. THE GRADING AND PAVING DESIGN SHOWN HEREIN COMPLIES WITH THE CALIFORNIA GREEN BUILDING STANDARDS CODE SECTION 5.106.10 "GRADING AND PAVING". ANY CHANGES TO THESE PLANS MUST BE APPROVED BY THE ENGINEER.
28. FOLLOWING STRIPPING, DEMOLITION, TREE REMOVAL AND FILL REMOVAL THE EXPOSED SUBGRADE IN BUILDING PAD, EXTERIOR FLATWORK AND PAVEMENT AREAS SHOULD BE EXCAVATED/SCARIFIED TO A DEPTH OF AT LEAST 12 INCHES, WORKED UNTIL UNIFORM AND FREE FROM LARGE CLODS, MOISTURE-CONDITIONED AS NECESSARY, AND RECOMPACTED TO A MINIMUM OF 95 PERCENT OF MAXIMUM DENSITY BASED ON ASTM TEST METHOD D1557.
29. PIPELINE TRENCHES SHALL BE EXCAVATED, BACKFILLED AND COMPACTED PER CITY OF SAN JUAN BAUTISTA STANDARD PLANS DETAIL NO E-1-1 & E-1-2
30. LIMITS OF WORK AS SHOWN ARE APPROXIMATE.

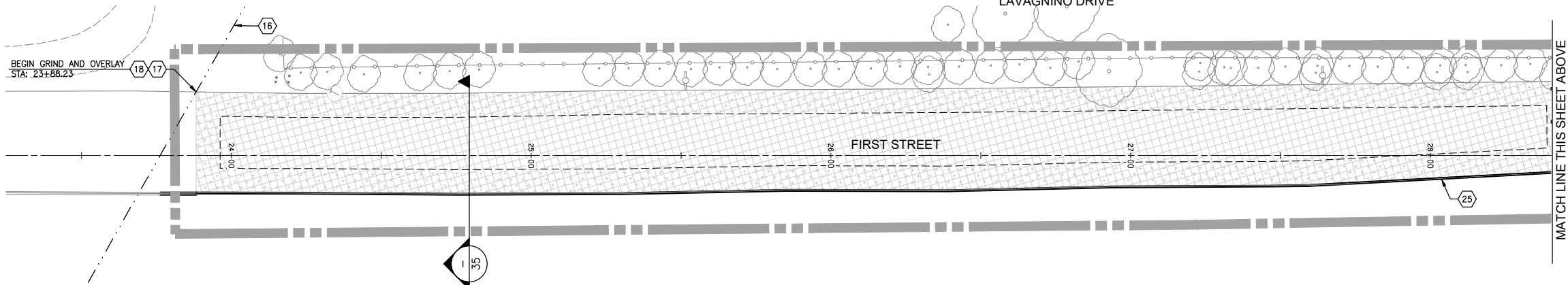
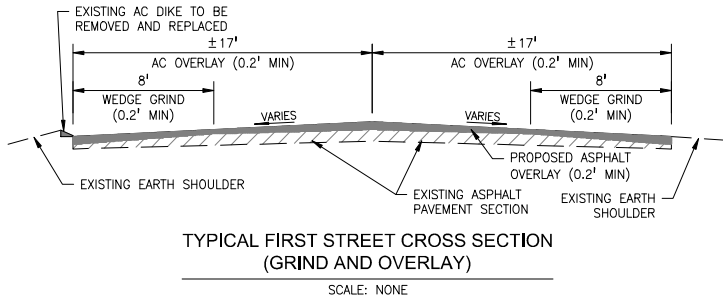
KEY NOTES #

1. MEET AND MATCH EXISTING IMPROVEMENTS.
2. CONSTRUCT 657 LF. VERTICAL CURB AND GUTTER PER CITY OF HOLLISTER STANDARD DETAIL A-8-1.
3. CONSTRUCT 308 LF. TYPE 2 VERTICAL CURB PER COSUB STANDARD DETAIL A-3.
4. CONSTRUCT 157 LF. SPILL CURB & GUTTER PER DETAIL 2, SHEET 40.
5. CONSTRUCT 10,121 SF. PAVEMENT SECTION, T.I. 8, R=5 PER DETAIL 3, SHEET 40.
6. CONSTRUCT 1,471 SF. PCC CONCRETE SIDEWALK PER CITY OF HOLLISTER STANDARD DETAIL A-9-1, OMIT CRUSHED AGGREGATE PER GEOTECHNICAL REPORT RECOMMENDATION.
7. CONSTRUCT 1,256 SF. PCC CONCRETE PER DETAIL 2, SHEET 40.
8. CONSTRUCT CURB RAMP (CASE C) PER DETAIL 4, SHEET 40. (3 PLACES)
9. CONSTRUCT CURB RAMP (CASE F) PER DETAIL 4, SHEET 40. (1 PLACE)
10. CONSTRUCT 55 LF. STREET BARRICADE PER CITY OF HOLLISTER STANDARD DETAIL A-15.
11. REPLACE EXISTING MANHOLE CONE WITH ECCENTRIC CONE PER DETAIL 1, SHEET 40. (1 PLACE)
12. CONSTRUCT 148 LF. 6' HIGH CHAIN LINK FENCE PER DETAIL 5, SHEET 40.
13. CONSTRUCT TYPE A CURB INLET PER CITY OF HOLLISTER STANDARD DETAIL D-3-1 (5 PLACES).
14. CONSTRUCT TYPE 2 ECCENTRIC MANHOLE PER CITY OF HOLLISTER STANDARD DETAIL D-2-1. (3 PLACES)
15. INSTALL METAL FLARED END SECTION PER STATE STANDARD PLAN D94A. (1 PLACE)
16. CITY OF SAN JUAN BAUTISTA/SAN BENITO COUNTY LIMIT LINE.
17. WEDGE GRIND EXISTING PAVEMENT, 8' WIDE MIN. SEE DETAIL THIS SHEET. (0.2'-MIN)
18. ASPHALT PAVEMENT OVERLAY. SEE DETAIL THIS SHEET. (0.2' MIN.)
19. CONSTRUCT 10 LF TYPE A AC DIKE PER DETAIL 6, SHEET 40.
20. CONNECT TO MANHOLE. MANHOLE PROPOSED TO BE CONSTRUCTED WITH IN-TRACT IMPROVEMENTS. SEE SEPARATE PLANS PREPARED BY SAN BENITO ENGINEERING & SURVEYING, INC.
21. FUTURE IMPROVEMENTS SHOWN FOR REFERENCE. NOT A PART.
22. CONSTRUCT 2"x10" REDWOOD HEADER BOARD PER DETAIL 8, SHEET 41.
23. SEE GRADING AND DRAINING PLAN, (SHEET 36) & ROUNDABOUT DIMENSION PLAN (SHEET 37).
24. NOT USED
25. REPLACE EXISTING A.C. DIKE WITH TYPE A DIKE PER DETAIL 6, SHEET 40.
26. CONSTRUCT STORM DRAIN STUB FOR FUTURE IMPROVEMENTS.
27. CONSTRUCT CONCRETE MEDIAN FLATWORK PER DETAIL 9, SHEET 41
28. CONSTRUCT APPROX. 51 LF MASONRY RETAINING WALL. SEE SHEET 32 FOR WALL DETAILS AND SHEET 41 FOR WALL GRADING.

GRADING AND DRAINAGE LEGEND

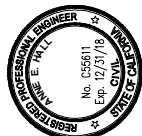
	PROPOSED LIMITS OF CONSTRUCTION		EXISTING TREE
	PROPOSED CHAIN LINK FENCE		EXISTING BRUSH
	PROPOSED CURB, GUTTER		WEDGE GRIND 0.1' MIN. 8' WIDE, 0.2' MIN. ASPHALT PAVEMENT OVERLAY
	PROPOSED VERT. MEDIAN CURB		0.2' MIN. ASPHALT PAVEMENT OVERLAY
	PROPOSED CONCRETE		
	PROPOSED ASPHALT PAVEMENT		
	PROPOSED CATCH BASIN		
	PROPOSED MANHOLE		
	PROPOSED STORM DRAIN & SIZE		
	EXISTING CURB, GUTTER		
	EXISTING STREET LITE		
	EXISTING CONCRETE HEADWALL		
	EXISTING WOOD FENCE		
	EXISTING CHAIN LINK FENCE		
	EXISTING TELECOMM MANHOLE		
	EXISTING STORM DRAIN MANHOLE		
	EXISTING SANITARY SEWER MANHOLE		

ABBREVIATION	DESCRIPTION
BC	BACK OF CURB
P	PAVEMENT
C	CONCRETE
DIA	DIAMETER
E	EAST
EP	EDGE OF PAVEMENT
EX	EXISTING
INV	INVERT
MH	MANHOLE
N	NORTH
S	SOUTH
W	WEST
SD	STORM DRAIN
CB	CATCH BASIN
COSTB	CITY OF SAN JUAN BAUTISTA



SAN BENITO ENGINEERING  
& SURVEYING, INC.

502 Monterey Street  
Hollister, California 95023  
(831) 637-2763 FAX (831) 637-6835 email: sbe@garlic.com



NO.	REVISIONS	DATE
1	GRADING AND DRAINAGE REVISION	05.18

CITY OF SAN JUAN BAUTISTA

RANCHO VISTA

SAN BENITO COUNTY, CALIFORNIA

IMPROVEMENT PLAN

GRADING AND DRAINAGE PLAN

SCALE: SCALE 1" = 20'
DATE: 15 June 2018
JOB #: 1013-0600
DWG: 35_grad.dwg
SHEET: 35
43
OF SHEETS

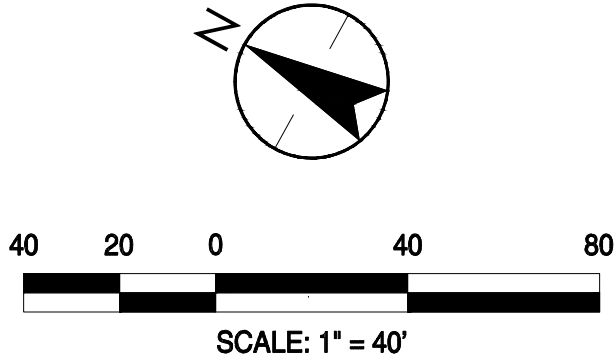
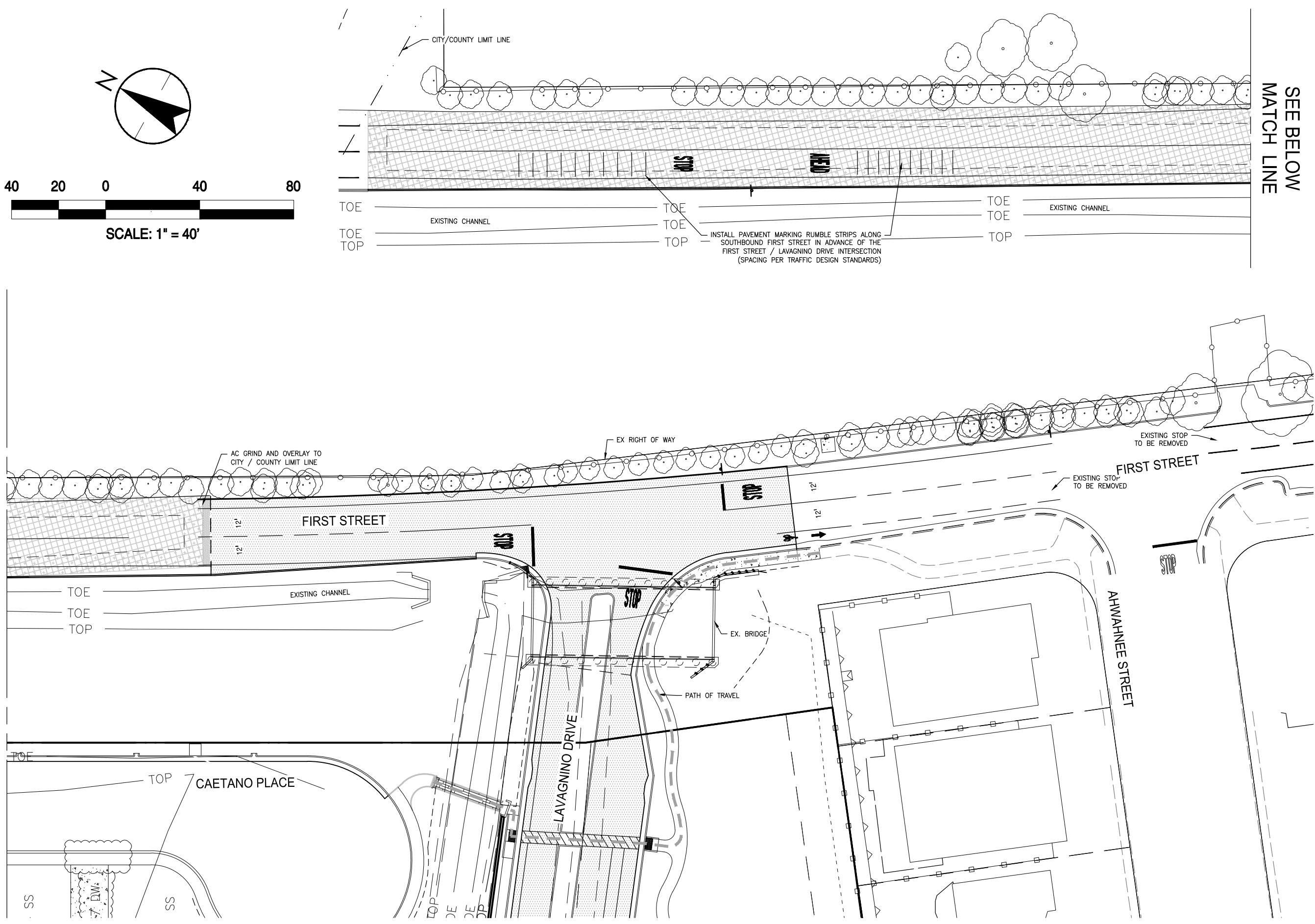






1/15/2019 3:01:04 PM W:\10130600\civil\design\sheet\_files\exhibits\20190114\_3-way\_intersection\20190114\_int\_exh.dwg

SEE ABOVE  
MATCH LINE



SEE BELOW  
MATCH LINE

MERITAGE HOMES  
RANCHO VISTA  
SAN JUAN BATISTA, CALIFORNIA

PROJECT #: 1013-0600  
DRAWN BY: IY  
DATE: 01/14/2019  
DRAWING NO.

1 OF 1

3 - WAY STOP CONTROLLED LAVAGNINO DRIVE / FIRST STREET INTERSECTION EXHIBIT

430 10th Street  
Modesto, CA 95354  
Tel.: 209.568.4477  
Fax: 209.568.4478



Attachment 3D



**RESOLUTION NO. 2019-xx**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SAN JUAN BAUTISTA TO  
APPROVE REPLACING RANCHO VISTA DEVELOPMENT ROUNDABOUT AT  
LAVAGNINO DRIVE AND FIRST STREET WITH A 3-WAY STOP CONTROLLED  
INTERSECTION.**

WHEREAS, City Staff requested and reviewed previously approved roundabout construction drawings, Loayzas Future Planned Development and traffic study prepared by Advanced Mobility Group dated November 19, 2018; and

WHEREAS, City Staff recommends approval of replacing rancho vista development roundabout at Lavagnino Drive and First Street with a 3-way stop controlled intersection; and

WHEREAS, City Staff recommends using the credit for not constructing the roundabout for the construction of a welcome entry monument sign; and

WHEREAS, Reminder funding for this resolution would be added for the Capital Improvement Program or other critical community services; and

NOW THEREFORE, BE IT HEREBY RESOLVED by the City Council of the City of San Juan Bautista that the City Manager is authorized and directed to work with the developer to amend construction drawings to remove the roundabout and construct a 3-Way stop controlled intersection and use funds to construct a welcome entry monument sign.

PASSED AND ADOPTED at a regular meeting of the San Juan Bautista City Council duly held on \_\_\_\_\_ by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk



# CITY OF SAN JUAN BAUTISTA

## PLANNING COMMISSION

### STAFF REPORT

**DATE:** February 5, 2018

**ITEM:** Signage for Monument Signs and Wayfinding Signs

#### **DISCUSSION:**

This item is being brought forward to Commission for discussion of possible signage throughout the city for wayfinding and monument signage and to gather feedback.

There has been interest in having wayfinding signs which would direct tourists and residents around the town, and to sites in town.

Presently, there are two logos used in town. One of them is the City Seal and the other one is the logo. As part of this sign package idea, staff wanted to discuss which logo would be preferred.

Further discussion may be necessary in the future as this is a starting point and an introduction to the idea.

#### **ACTION:**

No action necessary. Staff brought this item to the Planning Commission for introductions and feedback. This item can be brought back to Commission for further review.

#### **Attached:**

Past document listing out city signage  
City Seal  
Logo



## Wayfinding Sign locations in San Juan Bautista

*Rough Draft of sign locations*

Monument Landmark sign: The Alameda or Windmill (replacing City of History sign?)

Landmark Welcome or directional Sign: Monterey St. and Mucheleme St  
Round about hwy 129?

Directional signs:

4<sup>th</sup> and 3<sup>rd</sup>. Historic Downtown, Mission Restaurants, Shopping  
1<sup>st</sup>. and Monterey St. State Park, Mission, Library, Lodging, gas  
4<sup>th</sup> and Mucheleme: @ Public Restrooms ,El Teatro Campesino,  
Historic Downtown & Mission, Lodging  
Monterey and 2<sup>nd</sup>. State Park and Mission

Orientation/Destination signs: @ Public Restrooms @ The Alameda

Historic Downtown Signs with directional arrow: 3<sup>rd</sup> and Monterey  
3<sup>rd</sup> and Mucheleme

Destination signs: Pedestrian Signs/maps (your are here) : @ Plaza State Park & 3<sup>rd</sup> St.?

Pedestrian (map)/Directional sign? @ Mariposa and 3<sup>rd</sup>

Parking Signs: ?

### **Clarify which signs you'll need where**

An overall wayfinding plan will help you clarify what signs you need:

- **Directional signs** point the way.
- **Orientation signs** offer navigation help and reassurance for visitors. Examples include You-Are-Here maps, locators, and kiosks.
- **Destination signs** mark a defined area, such as "Asian Art" or "Teen Central" or "Minneapolis Riverfront."
- **Event-related signs** guide visitors to special events. In many cases, unfortunately, these signs are hastily and carelessly created. A sticky note with "Board Meeting" and an arrow may be functional, but does nothing to enhance your brand.
- **Regulatory signs** maintain desired flow. Examples include "Staff Only" and "Do Not Enter."

More than likely you'll need a combination of all of these types of signs.





**SAN JUAN BAUTISTA**

*The City of History*





**SAN JUAN  
BAUTISTA™**

**EST. 1869**



	Project Name - Current City/Development Projects - Upgrades, Changes, Fixes	Issues/Notes/general info	Status	Permit Issuance	Violations	Comments/Complaints Questions	Likelihood of Completion (Red or Green)
	Building/Planning						
1	Casa Rosa	Casa Rosa was approved by City Council in April of 2017. Building Permits were never issued and demolition began before aprval was given. The property has since been left in a state of disrepair to the current date. The city hired a structural engineer to make recommendation on fixes to the building. The city then reached out to the owner to make required fixes, and the owner could not meet that request in a reasonable amount of time. City staff will be presenting a staff report to begin nuisance abatement. (1/15/19)	Project was approved by HRB as minor alteration (4/4/17). Scope of work indicates major alteration. Work took place without building permits.	Site Design and Review performed by HRB and Planning Commission approved in April of 2017. No building permits applied for or approved. Evidence suggests the City Staff was aware of the work being done at the time. (1/15/19)	Building has been tagged as an unsafe structure. Violations of the Municipal Code include Section 5-8-110 and Section 5-8-190. (1/15/19)	Staff has been reporting as developments have continued, including meeting with the owner and hiring a Structural Engineer consultant. Results of the Structural Engineer report address 3 issues present that must be addressed in order to reopen the public right-of-way. City is beginning the abatement process with a forthcoming presentation to City Council. (1/29/19)	Abatement process to being shortly. After repairs are completed, the right-of-way will eb reopened. Estimated ETA is 3 months till repairs are completed.
2	Fault Line Restaurant	Garage was damaged by a tree fall in 2/17. Project was approved by Commission on 10/2/18 with conditions. Council Approved the zone change for Faultline site and residence at 17 Franklin on 12/18/18. <b>Awaiting documents from property owner for county recording to complete lot merge.</b>	<b>Council has approved the zone change at 11 and 17 Franklin Street. 30-day appeal period and statute of limitations per CEQA expire on 1/22/19.</b>	Permits are required for the garage reconstruction. Permits have been filed, but need more information per building, engineering, and planning before issuance can take place.	demolition was done without permit issuance	Staff is working with the applicant to finish the accessory building and ultimately reopen the restaurant. <b>Lot Merge Docuemnts were created. Applicant is signing them and notarizing them. City Engineer to sign and notarize. Permit review can take place after that date. (1/29/19)</b>	12/18 second Council Reading
3	10 Franklin existing construction, alteration, and demolition	approved per permit issuance by Permit Number 2017149 in 2015. Construction has been ongoing. Building Offical reviewed the plans and permit material was sent to the City Attorney. <b>Meeting was held on 1/9/19 with building, fire, planning, engineering, city manager and the property owner and his legal counsel. Determined the permit is valid. The Zone change is still pending. Awaiting response from discussion with legal counsel and city attorney on CEQA review.</b>	phase 1 for the multi-unit building construction is currently underway.	yes	none	It is anticipated to demolish at least one of the other buildings onsite. Addresses have been assigned. The property is included in the rezoning proposal with 11 and 17 Franklin. The property owner has given the ok to include his property in that change. <b>The second reading of the zone change did not happen because the Native American Tribes need to be included. They expressed concern about burials in or around the site. This property was not included in the second reading of the Zone Change. Building Official reviewed the permit file. Determined the permit is valid. Property owner's legal counsel and City Attorney to discuss CEQA review. (1/15/19)</b>	Property owner is still working on the site. He wants to finish, but a time frame is still undetermined.
4	Harvey's Lockup	progress is being made on the frontage & right-of-way improvements. <b>Interior sprinklers have been installed in one area of the building, but need to cover the entire building per Fire Code. Fire Alarm and smoke dectectors have been installed.</b>	Frontage work is in progress. Fire sprinkler work and prevention system to be completed in all phases. Applicant is ahead of schedule.	No active building permits. Encroachment permit to be pulled as needed.	n/a	Requirments of conditions of approval are in progress. <b>Commission reviewed the case 11/13/18. Staff presented the findings and the applicants provided testimoney. Commission supports to continue working with the business owner to take the necessary steps to resolve the matter and meet the requirements as indicated in the signed agreement. No action was taken. After checking in, they are ahead of schedule in implementing their systems and are moving right along. Staff gave an update to Council and Commission members of the status. Responses were postive. (1/15/19)</b>	Owner is working with his contractor to install heat and smoke detection systems.
5	Rancho Vista Subdivision	42 active permits. 1 application is on hold because of design.	Under Construction	42 active permits. 1 application is on hold because of design.	none	<b>Construction on issued permits is slowly wrapping up. It is expected that we will see more permit applications toward the end of the rainy season. (1/15/19)</b>	To be Determined - Estimate of Early 2019



	Project Name - Current City/Development Projects - Upgrades, Changes, Fixes	Issues/Notes/general info	Status	Permit Issuance	Violations	Comments/Complaints Questions	Likelihood of Completion (Red or Green)
6	Copperleaf Subdivision	20 permits issued as of 6-15-18	Under Construction	Permits have been issued	n/a	Applicant has been granted the deffering of two Impact fees pending agreement with city. Traffic and Park development fee held until approved agreement or Final Inspection on each project. <b>No Occupancies have been issued at this time, temporary occupancies are issued on their sales office.</b> (1/15/19)	To be Determined - Estimate of Winter 2019
7	Building Department activity.	Work continues on both subdivisions while smaller projects take place inside the city. Notable Projects include Midnight Express Warehouse undergoing plan check and Hillside Vista (D'Ambrosia) Model homes are undergoing plan check. (10/26/2018)	n/a	n/a	none	Code Enforcement is making regular rounds to check for work being done without permits and violations of the Municipal Code.	n/a
8	Well 3 and 6 exchange/Minor subdivision on Mission Vineyard and Old San Juan Road	NOE has been recorded and sent to the State for Minor Subdivision.	Well 6 work is moving forward	no permits issued	none	NOE and CEQA worksheet for the Waterboard has been completed and filed at the County Clerk's Office.	August of 2018 for the the completion of the exchange process that includes map recording
9	70 Muckelemi	Applicant wishes to the Chalmers House onsite that is in bad condition. He wishes to develop the site with a mix of uses and housing units.	Staff is reviewing previous documents that have been turned in about the property. Applicatn is preparing for 2/5/19 hearing with informal project review.	no	No violations. There was a notice of nusiance because of the condition of the old chalmers house.	Staff did a field inspection at the site on Monday, July 9, 2018. Discovered the structure is in exceedingly poor condition. <b>Staff met with the property owner on 12/6/18. Staff followed up with the property owner with an overview of the necessary applications to be turned in. Staff discovered a previous Historic Architecture Report and other documents on file done by previous owner. Staff is reviewing it to discover if any of that is still valid (1/15/19).</b>	No-ETA yet.
10	Brewery	Project is showing activity towards deed to perfect and continuation of project towards to completion. Staff met with them and provided a follow up letter to them in December 2018.	no applications yet, working with owner for submittal.	No permits issued	none	<b>Staff is working with the property owner. Met with the property owner on 12/5/18. Went through the case and a revised set of plans. Working on restarting the process. Staff followed up with them and provided an overview of the applications turned in and the items that can be used. Staff left a phone message asking when resubmittal to be turned in. (1/15/19)</b>	no-ETA yet
11	Building Department activity.	Larger Projects: D'Ambrosia Project has it's first 3 Models approved by the Building Plan Check. No permits issued for project. Rancho Vista Subdivision has been Issued multiple Occupancies since the Water Moratorium was lifted. Copperleaf has recied Temporary Occupancy on their Sales office. (1/15/19)	n/a	n/a	none	None at this time. (1/15/19)	n/a
12	Code Enforcement Activity	Code Enforcement is making regular rounds of the city, looking for work taking place without permits, parking violations, and other nuisances. (1/15/19)	n/a	n/a	none	None at this time. (1/15/19)	n/a



	Project Name - Current City/Development Projects - Upgrades, Changes, Fixes	Issues/Notes/general info	Status	Permit Issuance	Violations	Comments/Complaints Questions	Likelihood of Completion (Red or Green)
13	General Plan Timeline	General Plan was updated and adopted in 2/16. The housing element is being reviewed. We need to do so per State requirement. <b>Staff is working towards executing a contract with EMC Planning Group to assist with Housing element Update.</b>	n/a	n/a	n/a	Staff is working on getting the Housing Element squared away for the State. <b>Staff is working towards executing a contract with EMC Planning Group to assist with Housing element Update to meet State Requirements (1/15/19).</b>	n/a
New Development							
1	Hillside Vistas	First Phase Approved for first 8 lots. Master set is under review for design. No construction has started. Phase 2 for the next 19 lots is expected to come in.	Phase 1 completed and anticipated to be built out. Phase 2 to come.	No permits have been issued.	none	<b>Staff has received revised master plan set on 10/26/18. That has been reviewed. No ther activity has taken place. Staff has followed up with them. Plan revisions are taking place. Master set has been reviewed by Building and Planning Staff. (1/15/19)</b>	The developer has met with staff. Eager to continue with the project
2	Loazza (957 First Street - 4 Parcel Minor Subdivision)	Review has started. Waiting for application payment.	CEQA review completed. Staff is starting review.	No permits issued	none	Awating the \$5,000 payment at this time. Roundabout adjacent to Rancho Vista was discussed. More land from Loayza would be needed in order to accommodate Roundabout. Loayza property owners do not support that. Further review is needed.	unknown
3	Gas Station to be located along The Alameda and Hwy 156	staff is working with the applicant to help move the project forward.	can move forward.	no permits have been issued	none	<b>Staff is awaiting project submittal. 40-day appeal period has expired (1/15/19)</b>	Court ruled in favor of the project
6	404 and 406 Third Street - Proposed Vietnamese Bistro and residential unit expansion	The property owners wish to open a new Bistro inside an existing building downtown. They also wish to relocate permanently to town and expand an existing residential unit in the rear of 406 third Street	Minor Alteration approved by Commission	A health permit has been issued previously. Awaiting plan updates to accommodate fire and building requirments.	n/a	<b>Staff met with the applicants and went through the requirments. Fire requirments are currently being worked out between City Staff, applicants, and the fire marshall (1/15/19)</b>	Case was approved by Planning Commission
7	Midnight Express	Approved by Commission Resolution. <b>Plan set has been submitted for permit review. Applicant reduced the size of their project and would like to do it in phases.</b>	Awaiting applicant response.	<b>No permits have been issued.</b>	n/a	Discussion about the development took place and phasing in parts of the development were discussed. <b>The applicant submitted a letter asking for changes to the project. They would like to reduce the intensity of the project scope by reducing the size of the building and do the project in phases. They submitted a letter to City Staff making that formal request. City Staff signed that letter. Their proposal did not appear to need to go back to Planning Commission. Applicant is working on their project in small phases (1/29/19).</b>	staff has issued first round of comments after review.
Infrastructure							
	Traffic Circle	<b>Roundabout is under review. Latest engineering review shows the current roundabout is too small in diamter. Further revie and/or alternatives are necessary.</b>	under review	n/a	n/a	<b>Staff reviewed the roundabout status with Planning Commission on 1/14/19. More information is needed. Staff along with engineering and the developer are anticiapated to attend Planning Commission to present more information.</b>	progress can continue, but final product needs to be determined
	Parking						
	Signage	<b>Monument signs have been discussed. There is an interest in putting in monument signs in gateway locations.</b>	further consideration is needed	n/a	n/a	<b>Staff and San Juan Committee Member met with a sign contractor to discuss possible signs and monument signs. Locations, designs, and logo will need to be reviewed with Planning Commission.</b>	No ETA at this time